

GREAT WESTERN HIGHWAY UPGRADE, MEDLOW BATH

Statement of Heritage Impact (20% Design)



PR147863
3.0
7 July 2021

REPORT

Document status

| Version | Purpose of document | Authored by | Reviewed by | Approved by | Review date |
|---------|-------------------------|------------------|-------------|-------------|-------------|
| 1.0 | Draft for client review | S. van der Linde | S. Kennedy | S. Kennedy | 05.03.2021 |
| 2.0 | Final draft | S. van der Linde | S. Kennedy | S. Kennedy | 03.06.2021 |
| 3.0 | Final | S. van der Linde | S. Kennedy | S. Kennedy | 05.07.2021 |

Approval for issue

S. Kennedy



5 July 2021

This report was prepared by RPS within the terms of RPS' engagement with its client and in direct response to a scope of services. This report is supplied for the sole and specific purpose for use by RPS' client. The report does not account for any changes relating the subject matter of the report, or any legislative or regulatory changes that have occurred since the report was produced and that may affect the report. RPS does not accept any responsibility or liability for loss whatsoever to any third party caused by, related to or arising out of any use or reliance on the report.

Prepared by:

Prepared for:

RPS

Sarah van der Linde
Senior Heritage Consultant

Level 13, 255 Pitt Street
Sydney NSW 2000

T +61 2 8099 3200
E sarah.vanderlinde@rpsgroup.com.au

Transport for NSW

Pete Styles
Project Development Manager

71-79 Pyrmont Bridge Road
Pyrmont NSW 2009

T +61 409 449 258
E pete.styles@transport.nsw.gov.au

EXECUTIVE SUMMARY

Project overview

The proposal is part of the larger Great Western Highway Upgrade Program between Katoomba to Lithgow. The Medlow Bath section of the Great Western Highway is a primary north – south connection between Katoomba and Blackheath. The Great Western Highway provides one of two mountain crossings between Sydney and Lithgow. The overall project seeks to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic.

Proposal overview

Transport for NSW (Transport for NSW) proposes to upgrade approximately 1.2 kilometres of the Great Western Highway at Medlow Bath between Railway Parade and approximately 330m south of Bellevue Crescent (the proposal). This upgrade is part of the Great Western Highway Duplication project between Katoomba and Lithgow which aims to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic.

The Proposal is currently being developed. This report has been prepared to support the early exhibition of the Review of Environmental Factors (REF) in relation to non-Aboriginal (Historic) heritage. Aboriginal cultural heritage has been assessed in a separate report and should be referred to.

Heritage Significance

Three heritage items, and two potential heritage items, are located within the proposal area:

- *Medlow Bath Railway Station Group* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003)
- *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Blue Mountains LEP Item No.MB015)
- *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026)
- *Bus Shelter* (potential heritage item)
- *Advertising sign* (potential heritage item)

Four heritage items, and one potential heritage item, are located adjacent to the proposal area:

- *Hydro Majestic* (Blue Mountains LEP Item No.MB002)
- *Former Post and Telegraph Store* (Blue Mountains LEP Item No.MB008)
- *Urunga* (Blue Mountains LEP Item No.MB017)
- *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP Item No.MB019)
- *Sandstone Railway culvert* (potential heritage item)

Four heritage items are located within the vicinity of the proposal area:

- *Greater Blue Mountains Area* (WHL Reference No. 917, NHL Place No. 105999)
- [Former] *St Luke's Anglican Church* (Blue Mountains LEP Item No. MB010)
- *Horse Trough* (Blue Mountains LEP Item No. MB013)
- *House* (Blue Mountains LEP Item No.MB018)

Summary of heritage impacts

The proposal would have **little to no impact** on the *Greater Blue Mountains Area* (WHL Reference No. 917, NHL Place No. 105999)

REPORT

The proposal would have a **major adverse impact** on *Medlow Bath Railway Station Group* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003).

The proposal may have a **minor to moderate adverse physical impact** and would have a **moderate to major adverse visual impact** on *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002). The proposed alternate design for Bellevue Crescent would have an additional **minor adverse impact** on this heritage item through the reduction of its heritage curtilage and impact on potentially significant pine plantings and archaeological resources.

The proposal would have **little to no physical impact** and a **moderate visual impact** on *Former Post and Telegraph Store* (Blue Mountains LEP 2015 Item No.MB008).

The proposal would have **little to no physical impact** and a **moderate visual impact** on *St Luke's Anglican Church* (Blue Mountains LEP 2015 Item No.MB010).

The proposal would have **little to no impact** on *Horse Trough* (Blue Mountains LEP 2015 Item No.MB013).

The proposal would have a **major adverse impact** on *Avenue of trees (formerly Avenue of Radiata Pines)* (Blue Mountains LEP 2015 Item No.MB015).

The proposal would have **little to no physical impact** and a **moderate to major visual impact** on *Urunga* (Blue Mountains LEP 2015 Item No.MB017).

The proposal would have **little to no impact** on *House* (Blue Mountains LEP 2015 Item No.MB018).

The proposal would have a **little to no physical impact** and a **moderate to major adverse visual impact** on *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019).

The proposal 'preferred option' would have **little to no physical impact** and a **moderate adverse visual impact** on *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP Item No. MB026). However, the proposed 'alternate design' for Bellevue Crescent would have an additional **minor adverse impact** on this heritage item through the reduction of its heritage curtilage.

The proposal would have a **minor to moderate physical impact** and a **moderate adverse visual impact** on *Bus Shelter* (potential heritage item).

The proposal would have a **little to no impact** on *Sandstone railway culvert* (potential heritage item).

The proposal may have a **major adverse impact** on *Advertising sign* (potential heritage item).

Conclusion and Recommendations

The following mitigation measures are recommended to ensure that the proposal would not cause any further impact to heritage items than outlined in this report. Failure to implement these measures may result in additional or inadvertent impact to heritage.

Recommendation 1: Detailed design

The detailed design should be developed and refined in consultation with either a heritage architect or a built heritage consultant. The detailed design should aim to further minimise the impact of the proposal, with particular reference to the pedestrian bridge through the use of appropriate form, proportion and materials. Bulk should be minimised, and new built forms should be clearly separate from existing heritage fabric. Where appropriate, the detailed design should also respond to existing and significant architectural detail, such as the architectural detailing of the station building, or the footbridge. Detailed design should be in accordance with appropriate Sydney Trains and TfNSW guidelines, including:

- *Railway Footbridges Heritage Conservation Strategy 2016*
- *Heritage Platforms Conservation Management Strategy 2015*
- *Heritage Technical Note: Installation of New Electrical and Data Services at Heritage Sites 2017*
- *Station Component Guide 2017*

Recommendation 2: Section 60 Application for Medlow Bath Railway Station (SHR No.01190)

A Section 60 Application would be required for proposed works within the SHR curtilage of Medlow Bath Railway Station. The Application must be granted prior to works commencing.

Recommendation 3: Historic (non-Aboriginal) Archaeological Assessment

An Historic (non-Aboriginal) Archaeological Assessment (HAA) should be undertaken on the Hydro Majestic land proposed for use for the alternate design arrangement for Bellevue Crescent known as Lots 3, 4, 5 and 20 of DP25570. The HAA should be undertaken by a suitably qualified archaeologist in accordance with the *Heritage Act 1977* and the Heritage NSW publication *Assessing Significance of Historical Archaeological Sites and Relics* (2009). The purpose of the HAA is to determine the nature, extent and significance of any archaeological resources associated with the former Glenara Cottage in this area and provide appropriate management recommendations in relation to the proposal.

Recommendation 4: Non-Aboriginal heritage awareness training

- a. Works within the proposal area are being undertaken in an area of heritage significance. Prior to works commencing, contractors shall be briefed as to the sensitive nature of the proposal area and informed of any recommended mitigation measures or controls required
- b. Non-Aboriginal heritage awareness training must be provided for all contractors and personnel prior to commencement of works to outline the identification of potential heritage items and associated procedures to be implemented in the event of the discovery of non-Aboriginal heritage materials, features or deposits (that is, unexpected finds), or the discovery of human remains.

Recommendation 5: Protection of significant fabric

Works should be undertaken with care. To avoid impact to significant fabric during the construction of the proposal, it is recommended:

- a. machinery should be placed with sufficient clearance to significant heritage structures to avoid any inadvertent harm to significant fabric or incidental damage from vibration as per the TfNSW recommended minimum working distances for vibration intensive plant from sensitive receiver (Table 9.1). In particular, care should be taken when working near:
 - *Hydro Majestic's* stone fence
 - *Medlow Bath Railway Station* platform structures, platform edges and footbridge
 - *Former Post and Telegraph Store*
 - *Urunga*
 - *Melbourne House, Cosy Cot and Sheleagh Cottage*, in particular Lot 1 Great Western Highway
 - *Sandstone Railway culvert*
 - Archaeologically sensitive vacant land north of petrol station
- b. Protection of significant fabric – *Hydro Majestic stone fence*
 - i. protective barriers or fencing should be erected between the works corridor boundary and the *Hydro Majestic's* stone fence for the duration of works within the vicinity of this significant fabric to ensure no inadvertent harm occurs
 - ii. machinery and works should be placed with sufficient clearance to significant fabric and associated protective barriers to avoid inadvertent harm from machinery or incidental damage from vibration
 - iii. vibration monitoring of the stone fence should be put in place for the duration of works
- c. Protection of significant fabric – *Sandstone Railway culvert*
 - i. redundancy of the *Sandstone Railway culvert* should not include work to significant fabric

REPORT

- ii. if closure or blocking of the culvert is required, these works should be undertaken in a manner that would not impact significant fabric
- iii. if work to significant fabric is required, this should be undertaken in consultation with either a heritage architect or heritage consultant, and be conducted in a manner that minimises harm as much as practicable
- d. Protection of significant fabric – *bus shelter*
 - i. measures should be put place to protect significant fabric of the *bus shelter* during its proposed removal and relocation
 - ii. relocation position, and details of where and how it will be removed, stored and relocated, should be determined in consultation with Blue Mountains City Council
 - iii. after relocation, conservation of the mural should be undertaken to prevent further loss, or to sympathetically reinstate missing portions
- e. Protection of significant fabric – *advertising sign*
 - i. if removal of the advertising sign is required for the proposal, it should be salvaged and relocated
 - ii. relocation position, and details of where and how it will be removed, stored and relocated, should be determined in consultation with Blue Mountains City Council
 - iii. if removal of the *advertising sign* is not required for the proposal, appropriate measures should be put in place to protect it during proposed works, such as the installation of protective barriers or fencing
- f. Protection of significant fabric – potential archaeological site of former Glenara Cottage
 - i. prior to use as an ancillary facility / stockpile area, the vacant land north of the petrol station should be covered with geotextile, or other suitable protective material, to ensure no inadvertent harm to potential archaeological resources occurs
 - ii. no ground scraping, levelling or landscaping of this area should occur before, during or after the use of the area as an ancillary facility / stockpile area
 - iii. this protection measure may not be required if a HAA does not identify any significant archaeological potential.

Recommendation 6: Protection and management of significant trees

A qualified arborist should be engaged to undertake an Arboricultural Impact Assessment of the proposal area, with a particular focus on trees associated with heritage items, *Hydro Majestic* (Blue Mountains LETP Item No.MB002), *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Item No.MB015) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026) . Management and protection measures recommended in the Arboricultural Impact Assessment should be implemented accordingly to ensure the protection and management of significant trees throughout the implementation of the proposal.

Recommendation 7: Tree replacement

Trees removed as part of the proposal within the heritage curtilage of *Hydro Majestic* (Item No.MB002), *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Item No.MB015) or *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026) should be replaced in a manner that is consistent with, and accurately reflect, the extent, nature and significance of the respective heritage item. The location, species and number of trees to be planted should be determined in consultation with the land owner, Blue Mountains City Council and a qualified arborist with reference to the identified heritage significance of the respective heritage item.

Recommendation 8: Protection and management of moveable heritage

All moveable heritage identified as part of this assessment is to be managed in accordance with a moveable heritage procedure. Moveable heritage identified on *Hydro Majestic* (Blue Mountains LETP Item No.MB002)

REPORT

land should be managed in accordance with Section 6.5, Conserving Moveable Heritage, in the *Hydro Majestic Hotel, Medlow Bath, Conservation Management Plan* (Graham Brooks and Associates 2010).

Recommendation 9: Archival photographic recording

Prior to construction, an archival photographic recording of the heritage items impacted by the proposed works is to be prepared in accordance with the NSW Heritage Division of the Department of Environment and Heritage guidelines titled "*Photographic Recording of Heritage Items using Film or Digital Capture*". The photographic should be prepared by a heritage consultant and must document significant heritage elements and items that will be impacted by the proposed works. The record should also document significant views and vistas as selected by the heritage consultant.

The archival recording should include, but is not limited to, the following:

- *Medlow Bath Railway Station Group* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003)
- *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002)
- *Former Post and Telegraph Store* (Blue Mountains LEP 2015 Item No.MB008)
- *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Blue Mountains LEP 2015 Item No.MB015)
- *Urunga* (Blue Mountains LEP 2015 Item No.MB017)
- *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019)
- *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026)
- *Bus Shelter* (potential heritage item)
- *Sandstone Railway culvert* (potential heritage item)
- *Advertising sign* (potential heritage item)

Recommendation 10: Heritage interpretation

A heritage interpretation plan should be formulated and implemented in accordance with the Heritage NSW, *Interpreting Heritage Places and Items* (Heritage Office (former) 2005) as part of the proposed upgrade of the Great Western Highway. This is to be undertaken with the consent and co-operation of authorised owners or land managers and Blue Mountains City Council.

Heritage interpretation should communicate the history of Medlow Bath, with reference to its identified heritage items, and enable audiences to engage with the significance of these places and the wider Blue Mountains area. It should be integrated into the broader cultural heritage design and heritage interpretation strategy for the overall Great Western Highway Katoomba to Lithgow upgrade project, and pick up themes relevant to the overall Great Western Highway route as well as Medlow Bath.

Recommendation 11: Unexpected finds procedure

In the event that unexpected archaeological resources are identified in the course of the proposal, all work in the affected area should cease, the area should be cordoned off, and Heritage NSW should be notified, in accordance with Section 146 of the *Heritage Act 1977*. The TfNSW (2016) *Unexpected Heritage Finds Guideline* should be adhered to.

Recommendation 12: Further assessment required for any design modification

If the proposed works, or proposal area, are modified to those discussed in this report, additional heritage advice may be required to appropriately manage and mitigate any potential impacts caused by these changes.

Contents

| | | |
|----------|--|-----------|
| 1 | INTRODUCTION | 15 |
| 1.1 | Proposal area | 15 |
| 1.2 | Purpose of this SOHI and approach | 15 |
| 1.2.1 | Project status | 15 |
| 1.3 | Limitations | 16 |
| 1.4 | Authorship | 16 |
| 2 | LEGISLATIVE CONTEXT | 18 |
| 2.1 | World Heritage Convention | 18 |
| 2.1.1 | World Heritage List | 18 |
| 2.2 | <i>Environment Protection and Biodiversity Conservation Act (Cth) 1999</i> | 18 |
| 2.2.1 | National Heritage List | 18 |
| 2.2.2 | Commonwealth Heritage List | 18 |
| 2.3 | <i>Heritage Act (NSW) 1977</i> | 18 |
| 2.3.1 | State Heritage Register | 19 |
| 2.3.2 | Section 170 Heritage and Conservation Register | 19 |
| 2.3.3 | Relics Provisions | 19 |
| 2.3.4 | Conservation Management Plan | 19 |
| 2.4 | <i>Environmental Planning and Assessment Act 1979</i> | 19 |
| 2.4.1 | Blue Mountains Local Environmental Plan 2015 | 20 |
| 2.4.2 | Potential heritage items | 20 |
| 2.4.3 | Blue Mountains Development Control Plan 2015 | 20 |
| 2.5 | Summary | 20 |
| 3 | HISTORICAL CONTEXT | 24 |
| 3.1 | European crossing of the Blue Mountains 1815 | 24 |
| 3.2 | Great Western Highway 1815 onwards | 27 |
| 3.3 | The Main Western Line 1850-1870 | 28 |
| 3.4 | Township of Medlow Bath 1881 onwards | 29 |
| 3.4.1 | Land sales and subdivision 1890-1905 | 29 |
| 3.4.2 | Mark Foy and the Hydro Majestic Hotel 1900 onwards | 38 |
| 3.4.3 | Residential development 1900 onwards | 44 |
| 3.4.4 | Medlow Post and Telegraph Store 1903 onwards | 47 |
| 3.4.5 | St Luke's Anglican Church 1908 onwards | 48 |
| 3.4.6 | Annis and George Bills horse-trough 1930-1940 | 49 |
| 3.5 | Medlow Bath Railway Station | 49 |
| 3.5.1 | Early Development (1881-1940s) | 49 |
| 3.5.2 | Post War (1944) - Present | 50 |
| 3.6 | Aerial image development timeline | 0 |
| 4 | DESCRIPTION AND PHYSICAL EVIDENCE | 5 |
| 4.1 | Landscape setting and features | 5 |
| 4.2 | Proposal area | 7 |
| 4.3 | Potential Heritage Items | 40 |
| 4.3.1 | Bus shelter | 40 |
| 4.3.2 | Sandstone railway culvert | 43 |
| 4.3.3 | Advertising sign | 46 |
| 5 | ASSESSMENT OF ARCHAEOLOGICAL POTENTIAL | 49 |
| 5.1 | Analysis of documentary resources and the proposal area | 49 |

REPORT

| | | |
|----------|--|-----------|
| 5.1.1 | Roads..... | 49 |
| 5.1.2 | Medlow Bath Railway Station | 50 |
| 5.1.3 | Medlow Bath township | 50 |
| 5.2 | Assessment of archaeological potential and archaeological research potential | 53 |
| 6 | ASSESSMENT OF SIGNIFICANCE | 55 |
| 6.1 | Established assessment of heritage significance for statutory heritage items | 55 |
| 6.1.1 | Greater Blue Mountains Area (WHL Reference No. 917, NHL Place No. 105999) | 55 |
| 6.1.2 | Medlow Bath Railway Station Group (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003) | 57 |
| 6.1.3 | Hydro Majestic (Blue Mountains LEP 2015 Item No.MB002)..... | 58 |
| 6.1.4 | Former Post and Telegraph Store (Blue Mountains LEP 2015 Item No.MB008)..... | 59 |
| 6.1.5 | St Luke's Anglican Church (Blue Mountains LEP 2015 Item No.MB010) | 60 |
| 6.1.6 | Horse Trough (Blue Mountains LEP 2015 Item No.MB013) | 60 |
| 6.1.7 | Avenue of Trees (formerly Avenue of Radiata Pines) (Blue Mountains LEP 2015 Item No.MB015)..... | 60 |
| 6.1.8 | Urunga (Blue Mountains LEP 2015 Item No.MB017)..... | 61 |
| 6.1.9 | House (Blue Mountains LEP 2015 Item No.MB018) | 61 |
| 6.1.10 | Melbourne House, Cosy Cot, Sheleagh Cottage (Blue Mountains LEP 2015 Item No.MB019)..... | 62 |
| 6.1.11 | Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic) (Blue Mountains LEP 2015 Item No.MB026) | 62 |
| 6.2 | Assessment of significance of potential heritage items | 63 |
| 6.2.1 | Bus Shelter | 63 |
| 6.2.2 | Sandstone railway culvert..... | 64 |
| 6.2.3 | Advertising sign..... | 64 |
| 7 | THE PROPOSAL | 65 |
| 7.1 | Proposal background | 65 |
| 7.2 | Objectives..... | 65 |
| 7.2.1 | Program Objectives (GWHUP) | 65 |
| 7.2.2 | Project Objectives (Medlow Bath)..... | 65 |
| 7.2.3 | Development criteria | 66 |
| 7.2.4 | Urban design objectives | 67 |
| 7.3 | Description of proposal | 67 |
| 7.3.1 | Proposed alternative intersection at Bellevue Crescent..... | 75 |
| 7.4 | Design | 77 |
| 7.4.1 | Design criteria | 77 |
| 7.4.2 | Engineering constraints | 79 |
| 7.4.3 | Major design features | 79 |
| 7.4.3.2 | Construction of new pedestrian bridge | 85 |
| 7.4.3.3 | Construction of intersections on Great Western Highway | 89 |
| 7.4.3.4 | Construction of bus bays on both sides of Great Western Highway | 89 |
| 7.4.3.5 | Urban design and landscaping features | 89 |
| 7.4.3.6 | Drainage design..... | 89 |
| 7.4.4 | Ancillary facilities..... | 89 |
| 7.4.5 | Construction sedimentation basins..... | 90 |
| 7.5 | Public utility adjustment..... | 90 |
| 7.6 | Property acquisition..... | 92 |
| 8 | HERITAGE IMPACT ASSESSMENT | 93 |
| 8.1 | Summary of heritage impact | 93 |
| 8.2 | Impact assessment discussion | 95 |
| 8.2.1 | Construction of a four lane divided carriage way..... | 95 |
| 8.2.2 | Construction of a new pedestrian bridge | 97 |

REPORT

| | | |
|-----------|---|------------|
| 8.2.3 | Shared use (pedestrian/cyclist) path | 98 |
| 8.2.4 | Retaining wall and traffic barrier construction..... | 99 |
| 8.2.5 | Utility relocation and stormwater drainage upgrade | 99 |
| 8.2.6 | Ancillary facilities..... | 100 |
| 8.2.7 | Sediment basin | 100 |
| 8.2.8 | Landscaping design, materials and finishes..... | 100 |
| 8.3 | Visual impact assessment..... | 101 |
| 8.4 | Summary Statements of Heritage Impact | 106 |
| 8.4.1 | Greater Blue Mountains Area (WHL Reference No. 917, NHL Place No. 105999) | 106 |
| 8.4.2 | Medlow Bath Railway Station Group (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003) | 106 |
| 8.4.3 | Hydro Majestic (Blue Mountains LEP 2015 Item No.MB002)..... | 106 |
| 8.4.4 | Former Post and Telegraph Store (Blue Mountains LEP 2015 Item No.MB008)..... | 106 |
| 8.4.5 | St Luke's Anglican Church (Blue Mountains LEP 2015 Item No.MB010) | 106 |
| 8.4.6 | Horse Trough (Blue Mountains LEP 2015 Item No.MB013) | 107 |
| 8.4.7 | Avenue of trees (formerly <i>Avenue of Radiata Pines</i>) (Blue Mountains LEP 2015 Item No.MB015)..... | 107 |
| 8.4.8 | Urunga (Blue Mountains LEP 2015 Item No.MB017)..... | 107 |
| 8.4.9 | House (Blue Mountains LEP 2015 Item No.MB018). | 107 |
| 8.4.10 | Melbourne House, Cosy Cot, Sheleagh Cottage (Blue Mountains LEP 2015 Item No.MB019)..... | 107 |
| 8.4.11 | Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic) (Blue Mountains LEP 2015 Item No. MB026) | 107 |
| 8.4.12 | Bus Shelter (potential heritage item) | 108 |
| 8.4.13 | Sandstone railway culvert (potential heritage item)..... | 108 |
| 8.4.14 | Advertising sign..... | 108 |
| 8.5 | Impact to archaeological potential..... | 108 |
| 9 | RECOMMENDATIONS AND MITIGATION MEASURES | 109 |
| | Recommendation 1: Detailed design | 109 |
| | Recommendation 2: Section 60 Application for Medlow Bath Railway Station (SHR No.01190) | 109 |
| | Recommendation 3: Historic (non-Aboriginal) Archaeological Assessment..... | 109 |
| | Recommendation 4: Non-Aboriginal heritage awareness training..... | 109 |
| | Recommendation 5: Protection of significant fabric..... | 109 |
| | Recommendation 6: Protection and management of significant trees | 111 |
| | Recommendation 7: Tree replacement..... | 112 |
| | Recommendation 8: Protection and management of moveable heritage | 112 |
| | Recommendation 9: Before and after photographic record | 112 |
| | Recommendation 10: Heritage interpretation | 112 |
| | Recommendation 11: Unexpected finds procedure..... | 113 |
| | Recommendation 12: Further assessment required for any design modification..... | 113 |
| 10 | REFERENCES | 114 |

Tables

| | |
|---|----|
| Table 2.1: Heritage listings in relation to the proposal area | 20 |
| Table 5.1 Levels of archaeological potential | 49 |
| Table 7.1: Medlow Bath addressing problems and benefits..... | 66 |
| Table 7.2: Key design criteria | 77 |
| Table 7.3: Utility adjustment | 91 |
| Table 7.4: Current property acquisitions..... | 92 |

REPORT

| | |
|--|-----|
| Table 8.1: Defining levels of impact..... | 93 |
| Table 8.2: Summary of visual impacts (Spackman Mossop Michaels 2021) | 105 |
| Table 9.1: TfNSW recommended minimum working distances for vibration intensive plant from sensitive receiver | 111 |

Plates

| | |
|---|----|
| Plate 3.1: Blaxland Crossing the Blue Mountains by F.A. Sleaf and J. MacFarlane 1881 (Source: State Library of Victoria item 49349757)..... | 25 |
| Plate 3.2: Convicts building road over the Blue Mountains, New South Wales, 1883 (Source: National Library of Australia object 135505644) | 26 |
| Plate 3.3: William Cox's road over the Blue Mountains descending Mount York 1974 (Source: TfNSW RMS image HO27865)..... | 26 |
| Plate 3.4: Great Western Highway Railway bridge at Medlow Bath around 1970s (Source: TfNSW RMS image HO31604)..... | 28 |
| Plate 3.5: Hargrave's House 1901 (Source: Blue Mountains City Library item 00469) | 39 |
| Plate 3.6: Belgravia, early 1900s (Source: Flickr) | 40 |
| Plate 3.7: Tucker's Cottage (Source: Blue Mountains Association of Cultural Heritage Organisations Inc.) | 40 |
| Plate 3.8: Hydro-Majestic Hotel advertisement (Source: National Library of Australia object 136185419) | 41 |
| Plate 3.9: Hydro Majestic outside casino 1910 (Source: Blue Mountains City Library item 00443) | 41 |
| Plate 3.10: Medlow Bath stables early 1900s (Source: National Library of Australia object 146216810) | 42 |
| Plate 3.11: Mark Foy with his wife, son and guests outside the Belgravia end of the Hydro around 1906 (Source: Smith 2020, 55) | 42 |
| Plate 3.12: The Hydro Majestic from above 1938, showing Avenue of trees running north between the Great Western Highway and the railway (Source: Blue Mountains City Library item 000095) | 43 |
| Plate 3.13: Hydro Majestic after 1922 fire (Source: Blue Mountains City Library item 000823) | 43 |
| Plate 3.14: Medlow Bath from above, around 1930, showing the Hydro Majestic overlooking the valley in the foreground and the limited development of Medlow Bath on the eastern side of the railway in the background and Glenara Cottage within a stand of trees (lower right) (Source: National Library of Australia object 162354898) | 45 |
| Plate 3.15: Medlow Bath from above, around 1930, looking south along the Great Western Highway showing Hydro Majestic to right, township to left and the house and shop at Lot 1 Great Western Highway to lower left (Source: National Library of Australia object 162355018) | 46 |
| Plate 3.16: Medlow Bath from above, around 1930, showing Hydro Majestic to foreground and Medlow Bath township to rear (Source: National Library of Australia object 162355132) | 47 |
| Plate 3.17: <i>Post and Telegraph Office, Former</i> around 1983 (Source: Kaldy 1983, 65) | 48 |
| Plate 3.18: Elizabeth Foy with Mark Foy Jr and unidentified friends on original wooden Medlow Bath station, outside waiting room, December 1901 (Source: Smith 2020, 25) | 51 |
| Plate 3.19: American actress May Yohé, former wife of Lord Francis Hope, is greeted on Medlow Bath Station by Mark Foy around 1906 (Source: Blue Mountains City Library item 000837)..... | 51 |
| Plate 3.20: American actress May Yohé, former wife of Lord Francis Hope, is greeted on Medlow Bath Station by Mark Foy around 1906 (Source: National Library of Australia object 146217717 and Smith 2020, 47))..... | 52 |
| Plate 3.21: Snow at Medlow Bath Station, Blue Mountains, New South Wales around 1935 (Source: National Library of Australia object 141914096) | 53 |
| Plate 3.22: Medlow Bath Railway Station around 1950, showing platform building, signal box and platform plantings (Source: Blue Mountains City Library item 000734)..... | 53 |
| Plate 4.1: View north showing Great Western Highway (centre), Main Western Line (right) and Hydro Majestic Hotel (left) (RPS 2020)..... | 5 |

REPORT

| | |
|--|----|
| Plate 4.2: View south along Railway Parade showing Main Western Line (right) and residential area (left) (RPS 2020) | 6 |
| Plate 4.3: View north showing Medlow Bath Railway Station (left) and residential area (right) (RPS 2020) | 6 |
| Plate 4.4: View south across the rail overbridge at the northern end of town, showing Railway Parade (left) and Great Western Highway (right) (RPS 2020) | 7 |
| Plate 4.5: View east from Medlow Bath Park entrance, showing amenities (RPS 2020) | 9 |
| Plate 4.6: Pathway in Medlow Bath Park on lower slope near drainage channel, looking east (RPS 2020) | 9 |
| Plate 4.7: View west across Medlow Bath Park showing children's play equipment (RPS 2020) | 10 |
| Plate 4.8: View south west across Medlow Bath Park showing Hydro Majestic in background (RPS 2020) | 10 |
| Plate 4.9: View north along Railway Parade showing Medlow Bath Railway Station (left) (RPS 2020) | 11 |
| Plate 4.10: [Former] <i>St Luke's Anglican Church</i> (Blue Mountains LEP Item BM010), looking east (RPS 2020) | 11 |
| Plate 4.11: [Former] <i>St Luke's Anglican Church</i> (Blue Mountains LEP Item BM010), looking east (RPS 2020) | 12 |
| Plate 4.12: <i>Horse Trough</i> (Blue Mountains LEP Item MB013), looking west (RPS 2020) | 12 |
| Plate 4.13: View west across Railway Parade and <i>MB</i> from Park Street (RPS 2020) | 13 |
| Plate 4.14: View north along Railway Parade from Park Street showing <i>Medlow Bath Railway Station</i> (left) and <i>Urunga</i> (right) (RPS 2020) | 13 |
| Plate 4.15: Front façade of <i>Urunga</i> (Blue Mountains LEP Item MB017) as seen from Park Street (RPS 2020) | 14 |
| Plate 4.16: Rear of <i>Urunga</i> (Blue Mountains LEP Item MB017) (RPS 2020) | 14 |
| Plate 4.17: Front façade of <i>House</i> (Blue Mountains LEP Item MB018) at 6-8 Park Street (RPS 2020) | 15 |
| Plate 4.18: Front façade of <i>Former Post and Telegraph Store</i> (Blue Mountains LEP Item MB008) as seen from Railway Parade (RPS 2020) | 15 |
| Plate 4.19: Rear of <i>Former Post and Telegraph Store</i> (Blue Mountains LEP Item MB008) (RPS 2020) | 16 |
| Plate 4.20: Northern end of Railway Parade showing station entrance (left) and rail overpass cross intersection (RPS 2020) | 16 |
| Plate 4.21: View north across rail overpass cross intersection from Railway Parade (RPS 2020) | 17 |
| Plate 4.22: View south west across Great Western Highway from rail overpass cross intersection (RPS 2020) | 17 |
| Plate 4.23: View west across Great Western Highway to Station Street from rail overpass cross intersection (RPS 2020) | 18 |
| Plate 4.24: View south west of rail cutting at northern end of <i>Medlow Bath Railway Station</i> (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003), showing pedestrian overbridge (RPS 2020) | 18 |
| Plate 4.25: View south across <i>Medlow Bath Railway Station</i> (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003) from pedestrian overbridge (RPS 2020) | 19 |
| Plate 4.26: View south east of <i>Medlow Bath Railway Station</i> (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003) buildings from Great Western Highway (RPS 2020) | 19 |
| Plate 4.27: View south along Platform 1 showing platform buildings (RPS 2020) | 20 |
| Plate 4.28: View north east of weatherboard signal room (RPS 2020) | 20 |
| Plate 4.29: South façade of main platform building (RPS 2020) | 21 |
| Plate 4.30: View north east from Platform 1 showing <i>Former Post and Telegraph Store</i> (Blue Mountains LEP Item MB008) (left) and <i>Urunga</i> (Blue Mountains LEP Item MB017)(right) (RPS 2020) | 21 |
| Plate 4.31: View south along Platform 1 (RPS 2020) | 22 |
| Plate 4.32: View west from platform across Great Western Highway to vacant block of land (RPS 2020) | 22 |

REPORT

| | |
|---|----|
| Plate 4.33: View south along platform toward southern station entrance, showing <i>Hydro Majestic</i> (Blue Mountains LEP Item MB002) (right) (RPS 2020)..... | 23 |
| Plate 4.34: Southern level crossing station entrance looking east (RPS 2020)..... | 23 |
| Plate 4.35: View west of shop and residence at Lot 1 Great Western Highway, part of <i>Melbourne House, Cosy Cot, Sheleagh Cottage</i> (Blue Mountains LEP Item MB019) (RPS 2020) | 24 |
| Plate 4.36: View north along Station Street showing <i>Melbourne House</i> (Blue Mountains LEP Item MB019) (RPS 2020)..... | 24 |
| Plate 4.37: <i>Melbourne House</i> (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020) | 25 |
| Plate 4.38: <i>Cosy Cot</i> (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020)..... | 25 |
| Plate 4.39: <i>Sheleagh Cottage</i> (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020) | 26 |
| Plate 4.40: View south along Station Street showing rail overpass cross intersection (left) (RPS 2020)..... | 26 |
| Plate 4.41: View north along Great Western Highway from Foy Avenue at the southern end of the proposal area (RPS 2020) | 27 |
| Plate 4.42: View east across Great Western Highway showing railway compound on eastern side of highway (RPS 2020) | 27 |
| Plate 4.43: View south of western vegetated road reserve embankment between Bellevue Crescent and Foy Avenue (RPS 2020) | 28 |
| Plate 4.44: View east along Bellevue Crescent to intersection with Great Western Highway (RPS 2020) | 28 |
| Plate 4.45: View south along Delmonte Avenue (RPS 2020) | 29 |
| Plate 4.46: View south along Great Western Highway showing road reserve south of <i>Hydro Majestic</i> (RPS 2020)..... | 29 |
| Plate 4.47: View south across vacant lot immediately south of petrol station, showing highway (left) (RPS 2021)..... | 30 |
| Plate 4.48: View north west across vacant lot toward petrol station (RPS 2021) | 30 |
| Plate 4.49: View west across vacant lot showing stands of mature pines to rear (RPS 2021)..... | 31 |
| Plate 4.50: Detail of sandstone block identified in vacant lot fronting Great Western Highway (RPS 2021) | 31 |
| Plate 4.51: View south across vacant lot to rear of petrol station and Hydro Majestic car park, showing stands of mature pines (RPS 2021) | 32 |
| Plate 4.52: View west across vacant lot to rear of petrol station (RPS 2021) | 32 |
| Plate 4.53: View south across vacant lot to rear of petrol station showing rubbish and adjoining residential property (RPS 2021)..... | 33 |
| Plate 4.54: View west across vacant lot to rear of petrol station showing gate to Bellevue Crescent (RPS 2021)..... | 33 |
| Plate 4.55: View east across vacant lot behind petrol station (RPS 2021) | 34 |
| Plate 4.56: Detail of potential moveable cultural heritage located within vacant lot behind petrol station (RPS 2021)..... | 34 |
| Plate 4.57: Detail of potential moveable cultural heritage located within vacant lot behind petrol station (RPS 2021)..... | 35 |
| Plate 4.58: View north along Great Western Highway showing <i>Hydro Majestic</i> (Blue Mountains LEP Item MB002) (left) and railway line (right) (RPS 2020) | 35 |
| Plate 4.59: View north along Great Western Highway showing <i>Hydro Majestic</i> (Blue Mountains LEP Item MB002) and its stone fence (left) and railway line (right) (RPS 2020)..... | 36 |
| Plate 4.60: View south along western road reserve, showing footpath and underground utilities running beside <i>Hydro Majestic</i> (Blue Mountains LEP Item MB002) stone fence (right) (RPS 2020) | 36 |
| Plate 4.61: View west across Great Western Highway to <i>Hydro Majestic</i> (Blue Mountains LEP Item MB002) (RPS 2020) | 37 |
| Plate 4.62: View north along Great Western Highway showing <i>Avenue of trees</i> (Blue Mountains LEP Item MB015) (right) on land between highway and railway (RPS 2020) | 37 |
| Plate 4.63: View north of <i>Avenue of trees</i> (Blue Mountains LEP Item MB015) (RPS 2020) | 38 |

REPORT

| | |
|--|----|
| Plate 4.64: View west of pedestrian refuge crossing across the Great Western Highway from the southern entrance of <i>Medlow Bath Railway Station</i> (RPS 2020) | 38 |
| Plate 4.65: View south along Great Western Highway showing vacant block of land (right) | 39 |
| Plate 4.66: View south of vacant block of land, showing extant weatherboard structure (RPS 2020) | 39 |
| Plate 4.67: View west across vacant block of land (RPS 2020) | 40 |
| Plate 4.68: Bus shelter looking north, showing interior mural (RPS 2020) | 41 |
| Plate 4.69: Bus shelter interior detail (RPS 2020) | 41 |
| Plate 4.70: Bus shelter interior detail (RPS 2020) | 42 |
| Plate 4.71: Bus shelter looking south, showing exterior design (RPS 2020) | 42 |
| Plate 4.72: Bus shelter exterior looking east, showing exterior design (RPS 2020) | 43 |
| Plate 4.73: Sandstone culvert on western side of railway embankment, looking east (RPS 2020) | 44 |
| Plate 4.74: Sandstone culvert on western side of railway embankment showing proximity to Great Western Highway and drain running beneath highway (right), looking south (RPS 2020) | 44 |
| Plate 4.75: Location of sandstone culvert within rail corridor adjacent to Great Western Highway showing modern drain access (right), looking south east (RPS 2020) | 45 |
| Plate 4.76: Sandstone culvert on eastern side of railway embankment, looking west (RPS 2020) | 45 |
| Plate 4.77: Sandstone culvert on eastern side of railway embankment, looking west from Railway Parade (RPS 2020) | 46 |
| Plate 4.78: Rear of Advertising sign, looking east (RPS 2020) | 47 |
| Plate 4.79: Front of Advertising sign, looking north (RPS 2020) | 47 |
| Plate 5.1: Land owned by Mark Foy (Source: Smith 2020, 28) | 51 |
| Plate 5.2: Advertisement for the lease of Mark Foy's Glenara Cottage ("Advertising" Sydney Morning Herald, 29 April 1914, p.3) | 52 |

Figures

| | |
|--|----|
| Figure 1.1: Location of the proposal | 17 |
| Figure 2.1: Heritage listings in relation to the proposal area | 22 |
| Figure 2.2: Heritage listings in relation to the proposal area (detail) | 23 |
| Figure 3.1: Parish of Blackheath 1890 (Source: HLRV) | 30 |
| Figure 3.2: Parish of Kanimbla 1890 (Source: HLRV) | 31 |
| Figure 3.3: Parish of Megalong 1893 (Source: HLRV) | 32 |
| Figure 3.4: Belgravia Estate sales advert 1890 (Source: National Library of Australia object 230300144) | 33 |
| Figure 3.5: Buckingham Park Estate sales advert (Source: National Library of Australia object 230299961) | 34 |
| Figure 3.6: Medlow Bath land sales advert 1905 (Source: National Library of Australia object 230299854) | 35 |
| Figure 3.7: Katoomba crown land sales advert 1895 (Source: National Library of Australia object 230232863) | 36 |
| Figure 3.8: Medlow Grange Estate sales advert (Source: National Library of Australia object 230299739) | 37 |
| Figure 3.9: Plan of Medlow Bath Railway Station, around 1930 (Source: TfNSW) | 54 |
| Figure 3.10: Plan "Medlow Bath extension of platform to 600FT" 1943 (Source: TfNSW) | 55 |
| Figure 3.11: 1943 aerial image of Medlow Bath (Source: Six Maps) | 1 |
| Figure 3.12: 1958 aerial image of Medlow Bath (Source: NSW Department of Customer Service) | 2 |
| Figure 3.13: 1966 aerial image of Medlow Bath (Source: NSW Department of Customer Service) | 3 |
| Figure 3.14: 1994 aerial image of Medlow Bath (Source: NSW Department of Customer Service) | 4 |
| Figure 4.1: Location of potential heritage items in relation to the proposal area | 48 |
| Figure 5.1: Archaeological potential of the proposal area | 54 |
| Figure 7.1: The proposal (plan 1 – southernmost section) (Source: Spackman Mossop Michaels 2021) | 69 |
| Figure 7.2: The proposal (plan 2 – southern section) (Source: Spackman Mossop Michaels 2021) | 70 |
| Figure 7.3: The proposal (plan 3 – middle section) (Source: Spackman Mossop Michaels 2021) | 71 |

REPORT

| | |
|--|-----|
| Figure 7.4: The proposal (plan 4 – northern section) (Source: Spackman Mossop Michaels 2021) | 72 |
| Figure 7.5: The proposal (plan 5 – northernmost section) (Source: Spackman Mossop Michaels 2021) | 73 |
| Figure 7.6: Key features of the proposal (Source: Spackman Mossop Michaels 2021) | 74 |
| Figure 7.7: Proposed alternative intersection at Bellevue Crescent | 75 |
| Figure 7.8: The proposal (plan 6 – alternate Bellevue Crescent design) (Source: Spackman Mossop Michaels 2021) | 76 |
| Figure 7.9: Typical cross section within southern section of proposal area (i.e. just north of Bellevue Crescent) | 81 |
| Figure 7.10: Typical cross section within midpoint of the proposal area (i.e. near Hydro Majestic Hotel) – without right hand turn lane | 82 |
| Figure 7.11: Typical cross section within midpoint of the proposal area (i.e. near Hydro Majestic Hotel) – with right and turn lane | 83 |
| Figure 7.12: Typical cross section within the northern section of proposal area (i.e. near Station Street) | 84 |
| Figure 7.13: Indicative design of the new pedestrian bridge (subject to detailed design) (Source: Spackman Mossop Michaels 2021) | 86 |
| Figure 7.14: Indicative cross section of proposed pedestrian bridge (subject to detailed design) (Source: Spackman Mossop Michaels 2021) | 87 |
| Figure 7.15: Proposed pedestrian bridge crossing detailed plan (Source: Spackman Mossop Michaels 2021) | 88 |
| Figure 8.1: Visual impact assessment viewpoint locations (Spackman Mossop Michaels 2021) | 101 |
| Figure 8.2: Viewpoint 1 visualisation (Spackman Mossop Michaels 2021) | 102 |
| Figure 8.3: Viewpoint 2 visualisation (Spackman Mossop Michaels 2021) | 102 |
| Figure 8.4: Viewpoint 3 visualisation (Spackman Mossop Michaels 2021) | 103 |
| Figure 8.5: Viewpoint 4 visualisation (Spackman Mossop Michaels 2021) | 103 |
| Figure 8.6: Viewpoint 5 visualisation (Spackman Mossop Michaels 2021) | 104 |
| Figure 8.7: Viewpoint 6 visualisation (Spackman Mossop Michaels 2021) | 104 |

Appendices

Appendix A Heritage listings

1 INTRODUCTION

Transport for NSW (TfNSW) proposes to upgrade approximately 1.2 kilometres of the Great Western Highway at Medlow Bath between Railway Parade and approximately 330m south of Bellevue Crescent (the proposal). TfNSW commissioned RPS to prepare a non-Aboriginal heritage assessment to support the Review of Environmental Factors (REF) for the proposal.

This Statement of Heritage Impact (SOHI) assesses the impact of the proposal on the significance of heritage items located within, and adjacent to, the proposal area and is to be read in conjunction with the REF and *Great Western Highway Upgrade Medlow Bath - Urban Design and Landscape Character and Visual Impact Assessment* (Spackman, Mossop & Michaels 2021). It recommends measures to avoid or minimise impact, and any approvals required under the NSW *Heritage Act 1977*.

1.1 Proposal area

The proposal area covers around 1.2 kilometres of the Great Western Highway through Medlow Bath from Foy Avenue to the existing rail overbridge at Railway Parade. It also includes Medlow Bath Station for the installation of a pedestrian bridge, Medlow Bath Park, road reserve and a number of properties along the eastern side of the Great Western Highway to accommodate laydown areas.

The location of the proposal area is shown in Figure 1.1.

1.2 Purpose of this SOHI and approach

The purpose of this SOHI is to assess the impact of the proposal on the significance of heritage items located within, or in the immediate vicinity of, the proposal area. This SOHI assesses the following concept design drawings:

- HW5 – GREAT WESTERN HIGHWAY – UPGRADE PROGRAM, MEDLOW BATH, CIVIL PACKAGE, PREFERRED CONCEPT DESIGN, DETAIL DESIGN AND ENVIRONMENTAL ASSESSMENT, 20% DETAILED DESIGN, date 23 March 2021.
- HW5 – GREAT WESTERN HIGHWAY – UPGRADE PROGRAM, MEDLOW BATH, BRIDGE PACKAGE PREFERRED CONCEPT DESIGN, DETAIL DESIGN AND ENVIRONMENTAL ASSESSMENT, 20% DETAILED DESIGN, date 19 March 2021.
- GWH – MEDLOW BATH SECTION ALTERNATIVE BELLEVUE CRESCENT INTERSECTION, drawing number GWHEMB-MRBJ-MED-CV-SKE-105001_P0, dated 04 May 2021.

This report has been prepared in accordance with the relevant policies of Heritage NSW including NSW Heritage Manual *Statements of Heritage Impacts*, and *Assessing Heritage Significance*. This report uses the method of investigation and analysis established by the Australia ICOMOS (International Council on Monuments and Sites) *Charter for the Conservation of Places of Cultural Significance*, *The Burra Charter 2013* (Burra Charter).

1.2.1 Project status

The proposal is currently being developed and this document has been prepared to assist in project development, in particular to assist in consultation with local Council in accordance with ISEPP requirements. The detailed design is currently 20% complete, with the following details still to be confirmed:

- Final determination of road alignment
- Ancillary facilities including Stockpile areas and laydown sites
- Public utilities adjustment
- Detailed design of pedestrian bridge
- Transport Access Program (TAP) upgrades to Medlow Bath Station

REPORT

This report will require updating and assessment of impacts on completion of the detailed design. An additional SOHI will be prepared to assess the TAP component of the scope. The TAP upgrade of Medlow Bath Railway Station was added as an additional scope to the initial proposal and is yet to be fully determined.

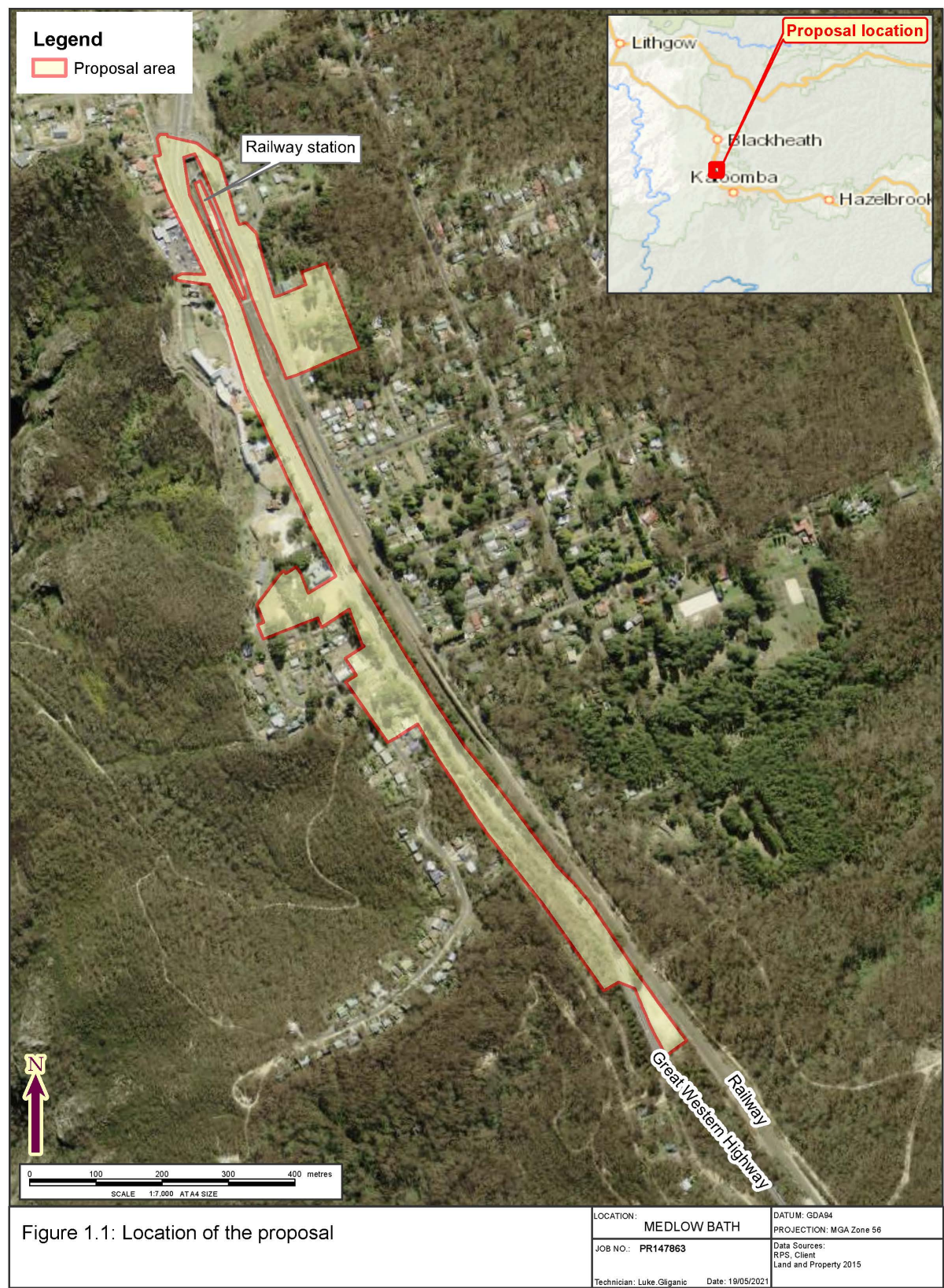
1.3 Limitations

This SOHI is limited to an assessment of non-Aboriginal heritage. Aboriginal Cultural Heritage for this project has been assessed separately by Jacobs in accordance with the (former) RMS *Procedure for Aboriginal Cultural Heritage Consultation and Investigation (PACHCI)*. A PACHCI Stage 2 was prepared in September 2020 and is to be referred to in relation to Aboriginal Cultural Heritage.

1.4 Authorship

Sarah van der Linde (Senior Heritage Consultant) prepared this SOHI with assistance from Luke Gliganic (Heritage Consultant). Susan Kennedy (Heritage Manager) has reviewed this report.

Figure 1.1: Location of the proposal



2 LEGISLATIVE CONTEXT

In NSW, environmental heritage is protected and managed under the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act), the NSW *Heritage Act 1977* and the NSW *Environmental Planning and Assessment Act 1979* (EP&A Act).

2.1 World Heritage Convention

The General Conference of UNESCO adopted the Convention Concerning the Protection of World Cultural and National Heritage (World Heritage Convention) on 16 November 1972, and it came into force on 17 December 1975. The World Heritage Convention aims to promote international cooperation to protect places of outstanding cultural significance.

2.1.1 World Heritage List

The *Greater Blue Mountains Area* is listed the World Heritage List (WHL) (Reference No. 917) and is located within the vicinity of the proposal area.

2.2 *Environment Protection and Biodiversity Conservation Act (Cth) 1999*

The *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act) is the principal environmental Act at a Commonwealth level. It provides for the protection and management of nine matters of national environmental significance as defined in the Act. Matters of national environmental significance include but are not limited to flora, fauna, ecological communities and heritage places of national and international importance.

In addition, the EPBC Act applies to actions with a significant impact on the environment where the actions affect, or are taken on, Commonwealth land, or are carried out by a Commonwealth agency (even if that significant impact is not on one of the nine matters of 'national environmental significance').

The EPBC Act requires approval from the Minister for actions with a significant impact on places included on the National Heritage List or Commonwealth Heritage List.

2.2.1 National Heritage List

The National Heritage List was established under the EPBC Act to protect places of outstanding significance to Australia.

The *Greater Blue Mountains Area* is listed the National Heritage List (NHL) (Place No.105999) and is located within the vicinity of the proposal area.

2.2.2 Commonwealth Heritage List

The Commonwealth Heritage List was established under the EPBC Act to protect places owned and managed by Commonwealth agencies.

There are **no places** on the Commonwealth Heritage List within or near the proposal area.

2.3 *Heritage Act (NSW) 1977*

The NSW *Heritage Act 1977* (the Act) is the principal Act for the management of NSW's environmental heritage. It establishes the State Heritage Register (SHR) and includes provisions for Interim Heritage Orders, Orders to Stop Work and archaeological relics (both on land and underwater within the limits of the State). It also requires government agencies to maintain a Heritage and Conservation Register.

To assist management of NSW's environmental heritage, the Act distinguishes between assets of state and local significance:

REPORT

- state significance refers to significance to the state in relation to the historical, archaeological, architectural, cultural, social, natural or aesthetic value of an item
- local significance refers to significance to an area in relation to the historical, archaeological, architectural, cultural, social, natural or aesthetic value of an item.

Items may be of state and local significance. Items of local significance may or may not be of significance to the state.

2.3.1 State Heritage Register

The State Heritage Register (SHR) identifies places and objects of importance to the whole of NSW.

Medlow Bath Railway Station Group is listed on the State Heritage Register (SHR No.01190) and is located in the proposal area.

2.3.2 Section 170 Heritage and Conservation Register

Section 170 of the Act requires government agencies to establish a Heritage and Conservation Register that identifies all assets of environmental heritage that it owns or occupies. Government agencies are required to provide the NSW Heritage Council notice of any intention to remove an asset from a Section 170 Heritage and Conservation Register, transfer ownership of an asset included on a Section 170 Heritage and Conservation Register, cease to occupy an asset on a Section 170 Heritage and Conservation Register or demolish an item included on a Section 170 Heritage and Conservation Register and assets must be maintained with due diligence in accordance with the *State Agency Heritage Guide* (NSW Heritage Office 2005). Proposals to alter or demolish assets of State significance must be referred to the NSW Heritage Council.

Medlow Bath Railway Station Group is listed on the TfNSW RailCorp Section 170 Heritage and Conservation Register (SHI No.4801011) and is located in the proposal area.

2.3.3 Relics Provisions

Historical archaeological resources or 'relics' are defined by the Act. Section 139 of the Heritage Act protects archaeological 'relics' from being 'exposed, moved, damaged or destroyed' by the disturbance or excavation of land. This protection extends to the situation where a person has 'reasonable cause to suspect' that archaeological remains may be affected by the disturbance or excavation of the land. It applies to all land in New South Wales that is not included on the SHR. A relic is an archaeological deposit, resource or feature that has heritage significance at a local or State level. The definition is no longer based on age. A 'relic' is defined by the Heritage Act as:

Any deposit, object or material evidence:

- (a) which relates to the settlement of the area that comprises New South Wales, not being Aboriginal settlement, and*
- (b) which is of State or Local significance." It should be noted that not all remains that would be considered archaeological are relics under the NSW Heritage Act.*

2.3.4 Conservation Management Plan

Conservation Management Plans relevant to the proposal area include:

- Hydro Majestic Hotel, 52-88 Great Western Highway, Medlow Bath Conservation Management Plan (Graham Brooks & Associates 2010)

2.4 Environmental Planning and Assessment Act 1979

The *Environmental Planning and Assessment Act 1979* (EP&A Act) regulates land-use planning and assessment for NSW. The Proposal is being assessed through a Review of Environmental Factors (REF) under Division 5.1 of the EP&A Act, specifically a development without consent to be determined by TfNSW.

2.4.1 Blue Mountains Local Environmental Plan 2015

The Blue Mountains Local Environmental Plan (LEP) 2015 sets out various planning, development and environmental controls for the Local Government Area (LGA) of the Blue Mountains City Council. Schedule 5 of the LEP identifies heritage items important to the LGA.

The following heritage items listed on the Blue Mountains LEP 2015 are located either in or within the immediate vicinity of the proposal area. Their locations in relation to the proposal area is shown in Figure 2.1.

- *Hydro Majestic* (Item No.MB002)
- *Medlow Bath Railway Station Group* (Item No.MB003)
- *Former Post and Telegraph Store* (Item No.MB008)
- *St Luke's Anglican Church* (Item No.MB010)
- *Horse Trough* (Item No.MB013)
- *Avenue of trees (formerly Avenue of Radiata Pines)* (Item No.MB015)
- *Urunga* (Item No.MB017)
- *House* (Item No.MB018)
- *Melbourne House, Cosy Cot, Sheleagh Cottage* (Item No.MB019)
- *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Conservation Area No. MB026)

2.4.2 Potential heritage items

A REF for the *Great Western Highway – Katoomba to Mt Victoria Road Safety Upgrades* was completed in 2016. The SOHI conducted by Artefact (2015) to support the REF identified *Medlow Bath Bus Shelter* as a potential heritage item. The Bus Shelter is located within the road reserve beside the railway within the proposal area.

2.4.3 Blue Mountains Development Control Plan 2015

The *Blue Mountains Development Control Plan (DCP) 2015* outlines the planning, design and environmental objectives and controls against which Blue Mountains City Council assesses Development Applications (DAs).

The Blue Mountains DCP 2015 requires significant development in the vicinity of a heritage item, heritage conservation area, archaeological site or Aboriginal site to submit a Heritage Impact Statement which addresses potential impacts and mitigation of impacts.

2.5 Summary

The heritage listings in relation to the proposal area are summarised in Table 2.1, with locations shown in Figure 2.1 and Figure 2.2.

Table 2.1: Heritage listings in relation to the proposal area

| Item | ID | Register | Significance | Location |
|--|---------------------------|---|----------------|---------------------------|
| <i>Greater Blue Mountains Area</i> | 917 105999 | World Heritage List National Heritage List | World/National | Vicinity of proposal area |
| <i>Medlow Bath Railway Station Group</i> | 01190 4801011 MB003 | State Heritage Register Section 170 Blue Mountains LEP 2015 | State | Proposal area |
| <i>Hydro Majestic</i> | MB002 | Blue Mountains LEP 2015 | Local | Proposal area |

REPORT

| Item | ID | Register | Significance | Location |
|--|-------|------------------------------|--------------|---------------------------|
| <i>Former Post and Telegraph Store</i> | MB008 | Blue Mountains LEP 2015 | Local | Adjacent to proposal area |
| <i>St Luke's Anglican Church</i> | MB010 | Blue Mountains LEP 2015 | Local | Vicinity of proposal area |
| <i>Horse Trough</i> | MB013 | Blue Mountains LEP 2015 | Local | Vicinity of proposal area |
| <i>Avenue of trees (formerly Avenue of Radiata Pines)</i> | MB015 | Blue Mountains LEP 2015 | Local | Proposal area |
| <i>Urunga</i> | MB017 | Blue Mountains LEP 2015 | Local | Adjacent to proposal area |
| <i>House</i> | MB018 | Blue Mountains LEP 2015 | Local | Vicinity of proposal area |
| <i>Melbourne House, Cosy Cot, Sheleagh Cottage</i> | MB019 | Blue Mountains LEP 2015 | Local | Adjacent to proposal area |
| <i>Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)</i> | MB026 | Blue Mountains LEP 2015 | Local | Proposal area |
| <i>Medlow Bath Bus Shelter</i> | NA | NA – potential heritage item | Local | Proposal area |

REPORT

Figure 2.1: Heritage listings in relation to the proposal area



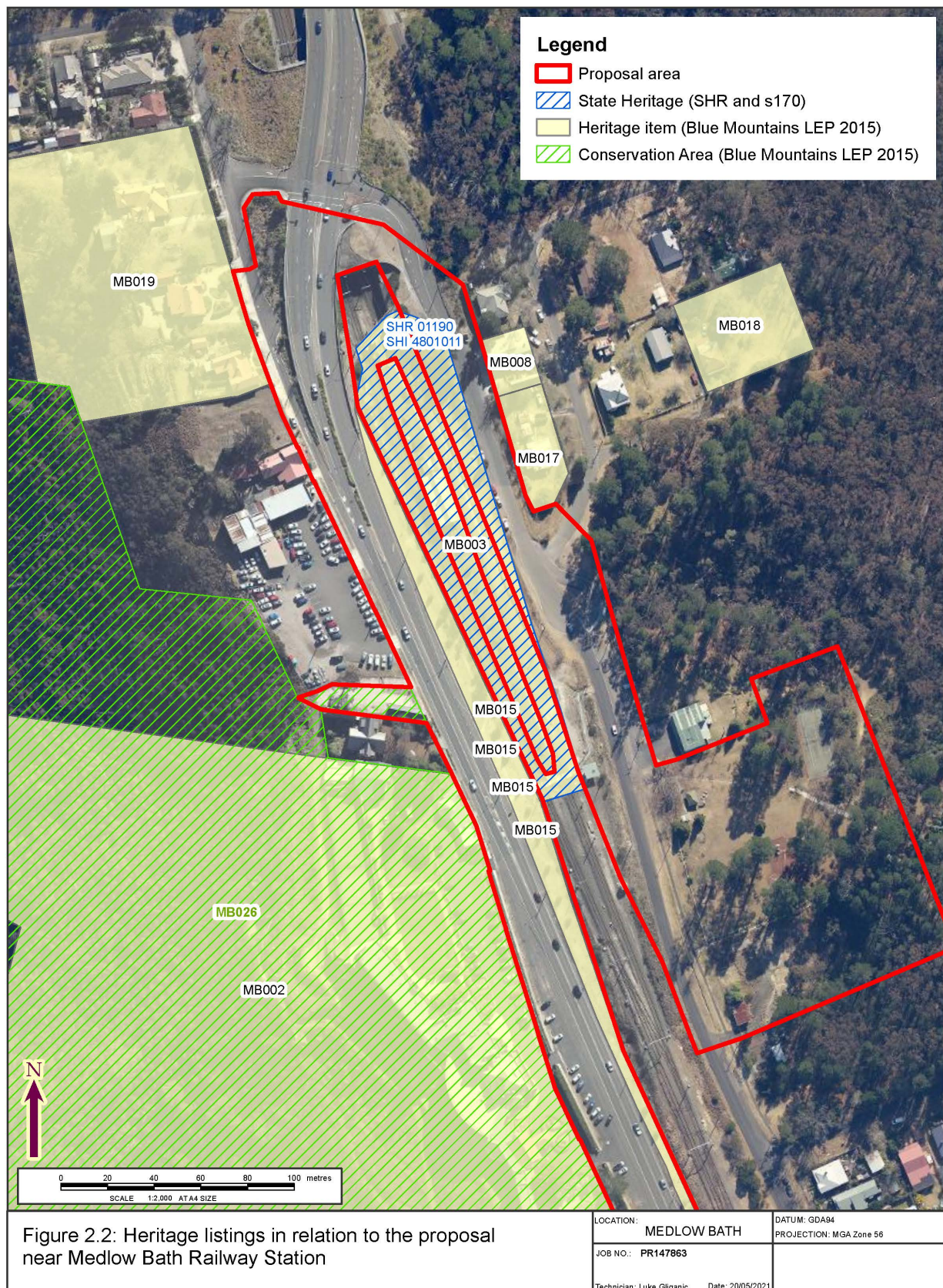
Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig2.1 (v2).mxd
NEW_A4_Portrait 2018 Rev B Produced: NW Reviewed: NW Date: 10/03/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
Level 13, 255 Pitt Street Sydney, 2001
T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 2.2: Heritage listings in relation to the proposal area (detail)



Path: C:\Users\Luke.Gilganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig2.2 (v2).mxd
 NSW_A4_Portrait 2018 Rev-B Produced: NWK/Reviewed: NWK Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T: 02 8270 8300 www.rpsgroup.com.au



3 HISTORICAL CONTEXT

3.1 European crossing of the Blue Mountains 1815

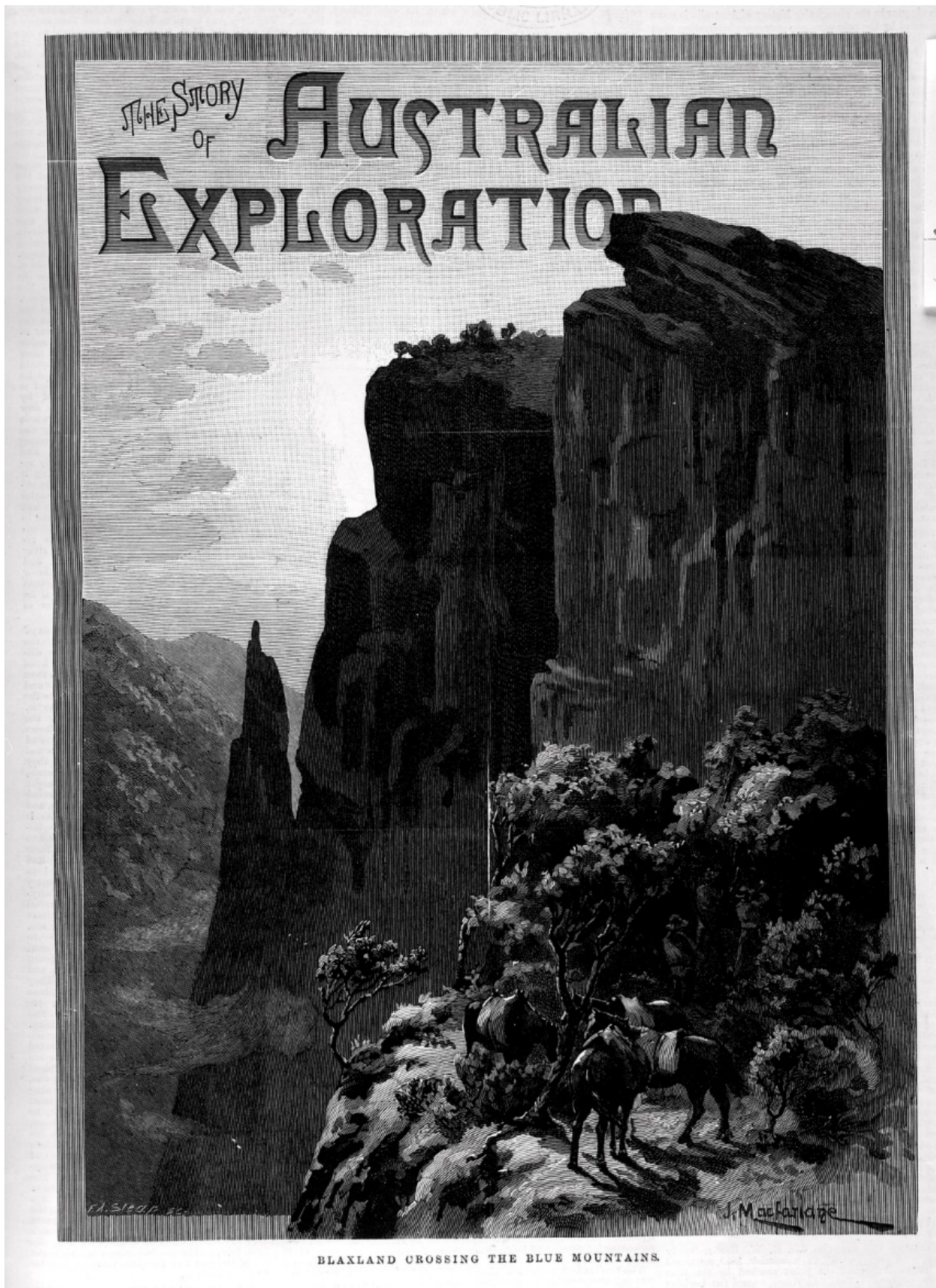
The Blue Mountains were seen as an impenetrable barrier to early European settlers in the Sydney region, halting the westward expansion of the colony. When Governor Macquarie took office in 1810, the colony was suffering drought, insect plagues and exhaustion of the soils around Sydney, making it imperative that new land be found (NMA 2020). Macquarie immediately embarked on an extensive program to raise the level of comfort and prosperity within the colony, with considerable emphasis on roadworks. He established a program of regular road construction to keep pace with exploration and settlement (Department of Main Roads NSW 1976: 10, 13).

By 1813 Gregory Blaxland realised that his sheep and cattle were expanding beyond the resources of his coastal land grant, and that Governor Macquarie would not grant extra lands to large land holders such as himself. In early 1813 Blaxland sought approval for an exploration expedition across the Blue Mountains. On 11 May 1813, Blaxland set out with William Lawson, a trained surveyor, and William Charles Wentworth. The party were assisted by local guide, James Burnes, who was familiar with the bush and Aboriginal pathways, and four convict servants (NMA 2020). The expedition party crossed the mountains in three weeks, utilising the method of traversing ridges instead of looking for a route through the valleys as previous explorers had done (Plate 3.1). The men reported discovering 'enough grass to support the stock of the colony for thirty years' (Conway 1966).

Blaxland, Lawson and Wentworth were rewarded with land grants. Macquarie sent the surveyor William Evans to explore and report on the fertility of the land discovered. Evans completed his task, becoming the first European to cross the Great Dividing Range in the process. Evans reported back that "*twelve men might clear a good Road in three months*", which Macquarie increased to fifty in his proposal to London (Department of Main Roads NSW 1976: 15). Macquarie then had William Cox construct a road suitable for carriages and stock (Plate 3.2 and Plate 3.3). Cox assembled a team of men with various skills, including specially selected convicts who were offered emancipation on the completion of the road as their reward. This arrangement allowed Cox to complete the road in six months. Construction of the road commenced on 18 July 1814. Despite sickness, bad weather and a mountain winter, the road to Bathurst was completed on 14 January 1815. The road was little more than a bush track around 12 feet wide, however, it would become the basis for today's Great Western Highway (Department of Main Roads NSW 1976: 15; NMA 2020).

REPORT

Plate 3.1: Blaxland Crossing the Blue Mountains by F.A. Sleaf and J. MacFarlane 1881 (Source: State Library of Victoria item 49349757)



BLAXLAND CROSSING THE BLUE MOUNTAINS.

REPORT

Plate 3.2: Convicts building road over the Blue Mountains, New South Wales, 1883 (Source: National Library of Australia object 135505644)



Plate 3.3: William Cox's road over the Blue Mountains descending Mount York 1974 (Source: TfNSW RMS image HO27865)



3.2 Great Western Highway 1815 onwards

By 1826, Cox's road had fallen into disrepair and the Colonial Government sought a new line of road through the Blue Mountains offering a reward grant of "*land, cattle, or such other reasonable indulgence as may be preferred*" to any "*free person*" who proposed a better route between Mount York and Bathurst (Ozroads). Archibald Bell had discovered a route in 1823 running north of Cox's. However, the track was rough and extremely steep. The route had little success and was abandoned in 1834. The present day Bell's Line of Road closely follows that created by Bell. Despite efforts to construct a more suitable route, the original Blaxland, Lawson and Wentworth route was adopted as the principle east – west road (Ozroads). An Act (4 William IV, No.II) passed on 28 August 1833 to distinguish main roads from minor roads, declares "*The Western road from Sydney by Parramatta – Emu – Mount Vittoria – and Honeysuckle Hill to Bathurst*" a main road to "*be kept in repair at the public expense*" (Department of Main Roads NSW 1976: 25).

With the discovery of gold in 1851, the road was subjected to immense traffic and deteriorated rapidly. Comprising mostly of horse-drawn and bullock-drawn drays, the constant traffic of these heavy drays broke up the soft stone of the road, producing deep sands through which the vehicles had to plough. Poor drainage of the road caused, where the road was harder, water to wash away surface soil and expose the rocks beneath (Department of Main Roads NSW 1976: 30-31).

Known as the Great Western Road since Mitchell's time, with his vision of three great roads radiating out of Sydney, it is unclear when the road was gazetted. However, the *Main Roads Management Act* (21 Victoria No.8) 1858 refers to "*The Great Western Road – from Sydney, via Parramatta, Penrith, Hartley, and Bathurst, to Wellington, a distance of about 210 miles*" (Department of Main Roads NSW 1976: 41).

The coming of the railway through the Blue Mountains, and Medlow Bath in 1863, saw an increased decline in road traffic. The poor road conditions combined with the dependable, faster, and all weather condition trains saw popularisation of the railway. Roads provided a complementary service, acting as feeder to the railway stations and termini (Department of Main Roads NSW 1976: 42-43). A review of road conditions in 1865 noted that the proposal area section of the Great Western Road between 18 Mile Hollow (Woodford) and Hartley had been ballasted. All efforts were being made to maintain the road as the artery westward into the interior (Department of Main Roads NSW 1976: 47-48).

The Great Western Highway was proclaimed State Highway No. 5 in 1928. The Main Roads Board immediately commenced bituminous sealing of the highway, which was completed by 1939. The alignment of the highway largely follows the original alignment because it traverses ridges as much as possible and avoids gullies, which kept the road dry for horses, carts and carriages. However, early motor vehicles often struggled to climb some of the steep inclines and had to share narrow ridge tops with the railway line, limiting widening of the highway. This was compounded by the establishment of many small settlements at railway stations along the route, which were often located adjacent to the Highway, increasing local traffic conflict. The Department of Main Roads commenced its first deviation of the mountains section of the Great Western Highway at Springwood in 1965 to eliminate two railway underpasses and bypass the congested route through the town centre (Ozroads).

The Department of Main Roads planned reconstruction of the Great Western Highway through the Blue Mountains. Reconstruction commenced in 1979 at Glenbrook, and included duplication and realignment of a railway underpass. The programme was halted in 1985 due to a lack of funds. Works recommenced in Katoomba with the elimination of the last level crossing on the left of the highway. In the late 1990s the highway was expanded to four lanes from Penrith to Katoomba. Upgrades continued in the 2000s at various points along the alignment including duplication, provision of service roads and additional pedestrian bridges. This included a new four lane railway bridge and an improved alignment at Medlow Bath in December 2003 (Plate 3.4) (Ozroads).

REPORT

Plate 3.4: Great Western Highway Railway bridge at Medlow Bath around 1970s (Source: TfNSW RMS image HO31604)



3.3 The Main Western Line 1850-1870

Following the opening of the railway from Sydney to Parramatta in 1850, plans were laid to cross the Blue Mountains and create rail access to the western plains. Extensive exploration revealed that no better route than that found by Blaxland, Lawsons and Wentworth could be found through the Blue Mountains and therefore the railway line would largely follow that of the Great Western Road. The Main Western Line was constructed as a number of phases. The line to Penrith was completed in 1863 and remained the end of the line for a number of years. Penrith was the starting point for coaches over the Blue Mountains. Two Cobb and Co. coaches were available daily to Bathurst (Crowland 1954, 249).

The railway line from Katoomba to Blackheath, through Medlow Bath, opened in 1868. A halt stop was established at Medlow Bath in 1881. The halt was originally known as Brown's Siding because of Brown's sawmill close by in Railway Parade. The name was changed to Medlow in 1883 to avoid confusion with Brown's Siding in Lithgow. The name changed again in 1903 to Medlow Bath with the opening of Mark Foy's Hydro (Heritage NSW 2020a; Kaldy 1983, 7).

3.4 Township of Medlow Bath 1881 onwards

3.4.1 Land sales and subdivision 1890-1905

Little is known about the history of Medlow Bath prior to the arrival of the railway. The Great Western Road and Railway line divided the township in two, with the eastern side of the railway falling in the Parish of Blackheath (Figure 3.1) and the western side of the railway falling into the Parish of Kanimbla (Figure 3.2). The south west portion of the extended township falls within the Parish of Megalong (Figure 3.3). The township seems to have evolved slightly differently within each Parish.

The 1890 map of the Parish of Blackheath reveals that by 1890, Medlow, as it was known at this point in time, had been opened up for Crown land sales to the east of the railway in smaller single figure acreage allotments (Figure 3.1). While the 1890 Parish of Kanimbla and 1893 Parish of Megalong maps reveals that the western side of the Great Western Road has been granted through either promise or purchase in large acreages similar to early land grants across New South Wales (Figure 3.2 and Figure 3.3). The western side of the proposal sits within Thomas and C.G. McGregor's original 50 acres in the north, William Relton's 112 acres through the main section of town, J.H. Neale's 40 acres and H.H. Brown and G.H. Holmes' 40 acres in the south. The Parish maps also indicated that land between the Great Western Road and the Great Western Railway from Emu Plains near Penrith to One Tree Hill at Mount Victoria was reserved in 1878 as Crown Land.

A land sales advert for Belgravia Estate (Figure 3.4) dated January 1890 shows that the grants issued to Thomas and McGregor, and William Relton west of the railway are now owned by T.S. Richardson, and that J.H. Neale's grant "Lauraville" is now the post office and orchard while the eastern side of the railway and road is marked as 'saw mills'. Kaldy (1983, 67) states that Brown's Saw Mills was located on the sites of the two railway cottages in Railway Parade, opposite the station. A Sawmill is further referenced on the 1890 Parish of Blackheath map, where it is noted that Portion 71 immediately adjacent to Medlow Station has special leases to Messrs Bennett and Smith for Sawmill from 1 January 188[0] to 31 December 1893 and seems to be exempt from the Crown land sales occurring at this time. This exemption could explain how *House* (Blue Mountains LEP Item No. MB018) at 6-8 Park Street appears to have been constructed within what seems to have been a reserve area since 1880. *House* is a typical late Victorian weatherboard cottage with bullnose verandah likely to have been constructed between 1890 and 1900, prior to land sales being released in this area.

The land sales advert for Buckingham Park Estate (Figure 3.5) appears to date from the 1890s and again makes reference to the sawmill by the station as being Smith's Sawmill and shows Medlow Bath Park already reserved for public purpose to the south of the saw mills. The property containing Mr Guest's cottage is excluded for sale here but has been subdivided and placed for sale by 1905 as part of the township allotments advertised in the 1905 Medlow Bath land sale advert, which also shows that land north of the railway still remains unsold and available for purchase (Figure 3.6).

The sawmill is no longer referenced on an advert for crown land sales at Katoomba in February 1895 (Figure 3.7), which includes land along the outskirts of Medlow Bath. However, the former sawmill area is still marked as reserved as notified on 8 November 1880 and is surrounded by an area reserved for public purposes, which comprises the present Medlow Bath Park. This area became Crown Reserve 33660 and was dedicated as Medlow Park in 1902 (Blue Mountains City Council 2012: 13). Medlow Park has remained in this location and is an important recreation and memorial area for the community, with many memorial plaques and plantings. The 1895 plan also indicates "Lauraville" in the south of the proposal area to still contain a house with stables and orchard.

H.H. Brown and G.H. Holmes' 40 acres in the south of the proposal area appears to have been subdivided and advertised for sale as "9 *Magnificent Residential Allotments*" around 1900 before Mark Foy's Hydro was established (Figure 3.8).

According to the Medlow Bath subdivision plans held at the State Library of New South Wales, the subdivision and sale of land continued in Medlow Bath through the early twentieth century, particularly with land to the east of the Railway Station in 1914 through to 1922.

Figure 3.1: Parish of Blackheath 1890 (Source: HLRV)



Figure 3.2: Parish of Kanimbla 1890 (Source: HLRV)



Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.2.mxd
 NSW_A4_Portrait 2018 Rev B Produced: NW Reviewed: NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T. 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.3: Parish of Megalong 1893 (Source: HLRV)

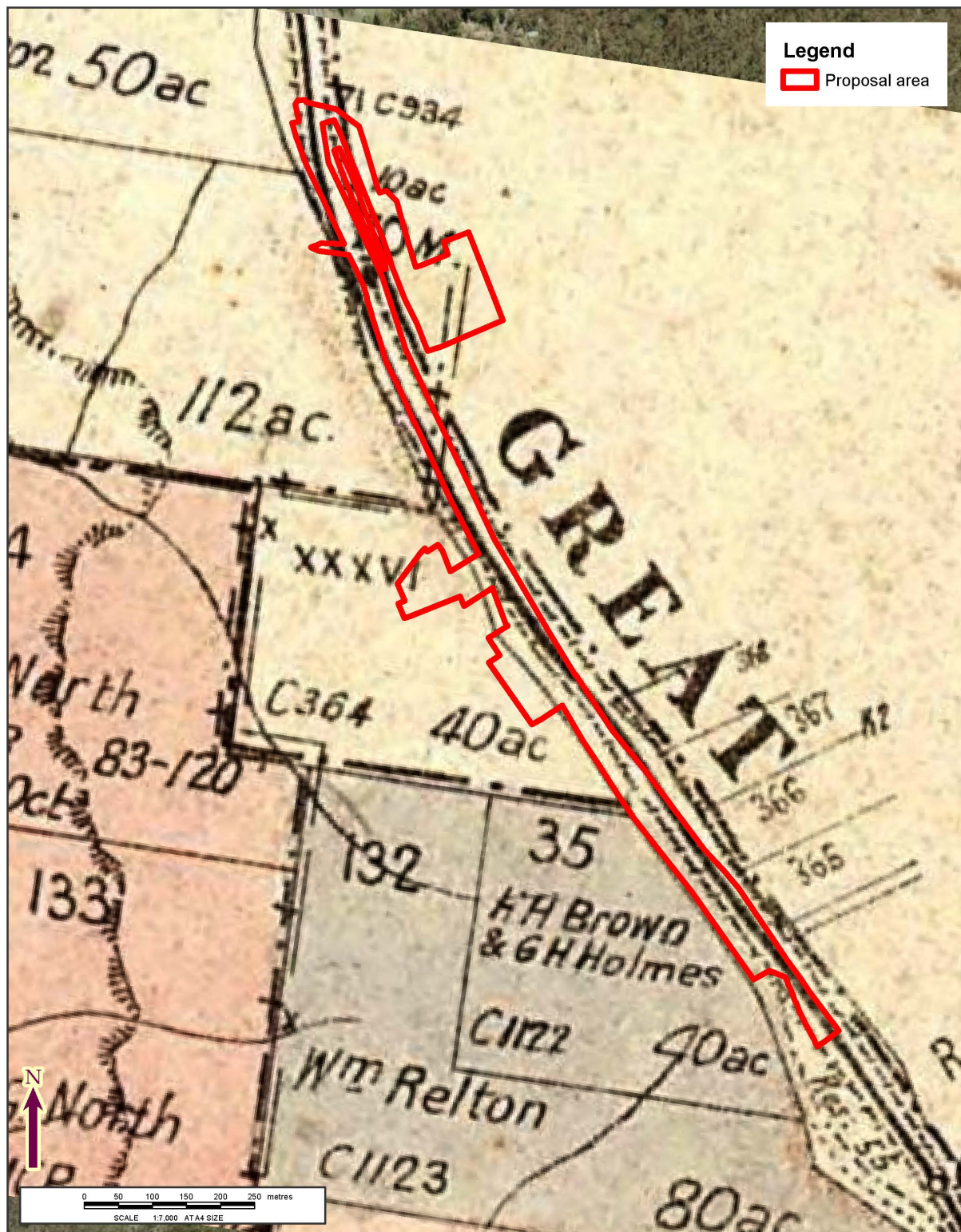


Figure 3.3: Parish of Megalong 1893 (Source: HLRV)

| | | |
|-------------|---------------|-------------------------|
| LOCATION: | MEDLOW BATH | DATUM: GDA84 |
| | | PROJECTION: MGA Zone 56 |
| JOB NO.: | PR147863 | Data Sources: |
| | | RPS, Client |
| | | Land and Property 2015 |
| Technician: | Luke Gliganic | Date: |
| | | 19/05/2021 |

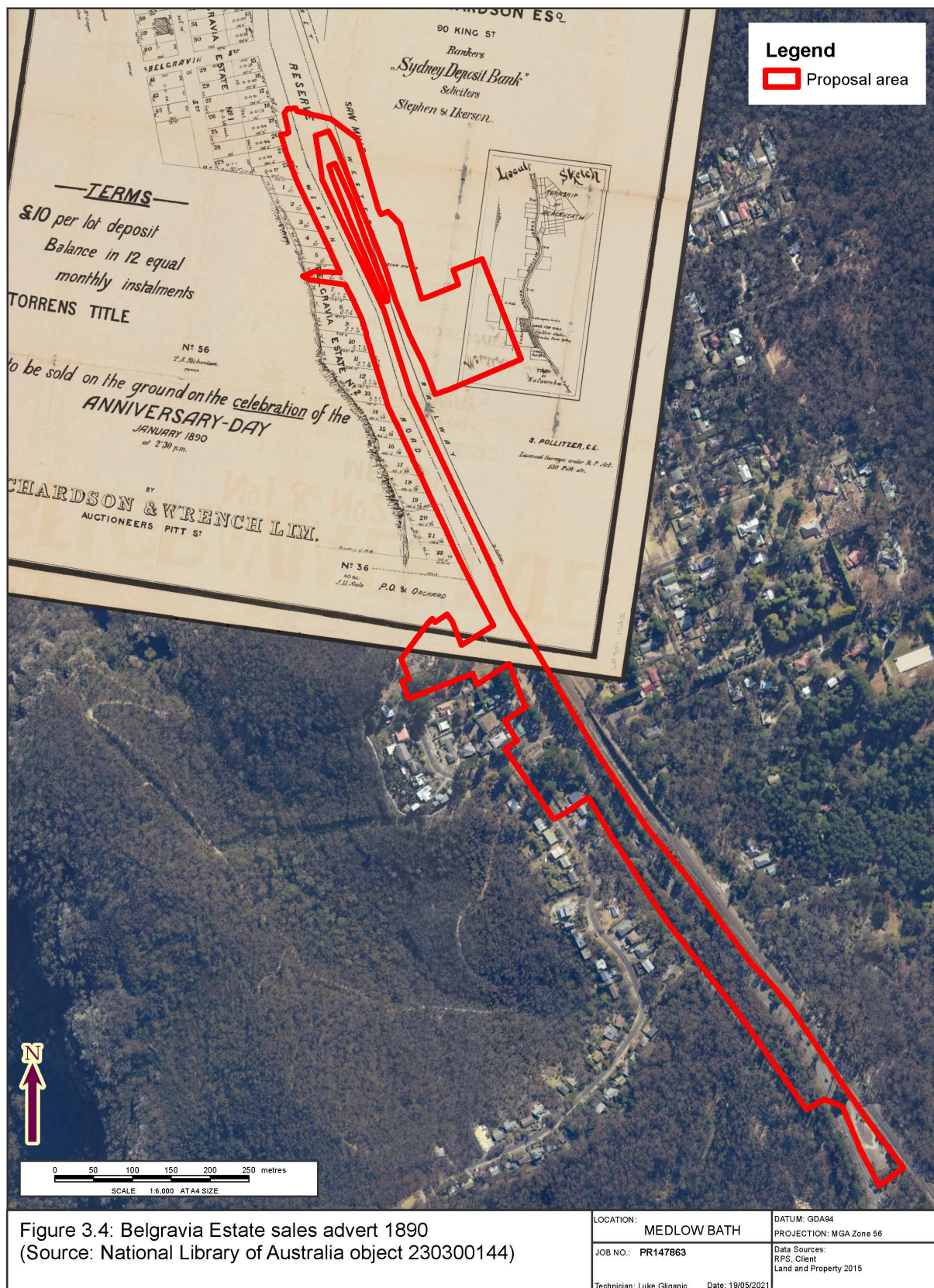
Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.3.mxd
 NEW_A4_Portrait 2018 Rev:B Produced: 19/05/2021

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.4: Belgravia Estate sales advert 1890 (Source: National Library of Australia object 230300144)



Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.4.mxd
NEW_A4_Portrait 2018 Rev-B Produced by RPS Group Pty Ltd Date: 19/05/2021

REPORT

Figure 3.5: Buckingham Park Estate sales advert (Source: National Library of Australia object 230299961)

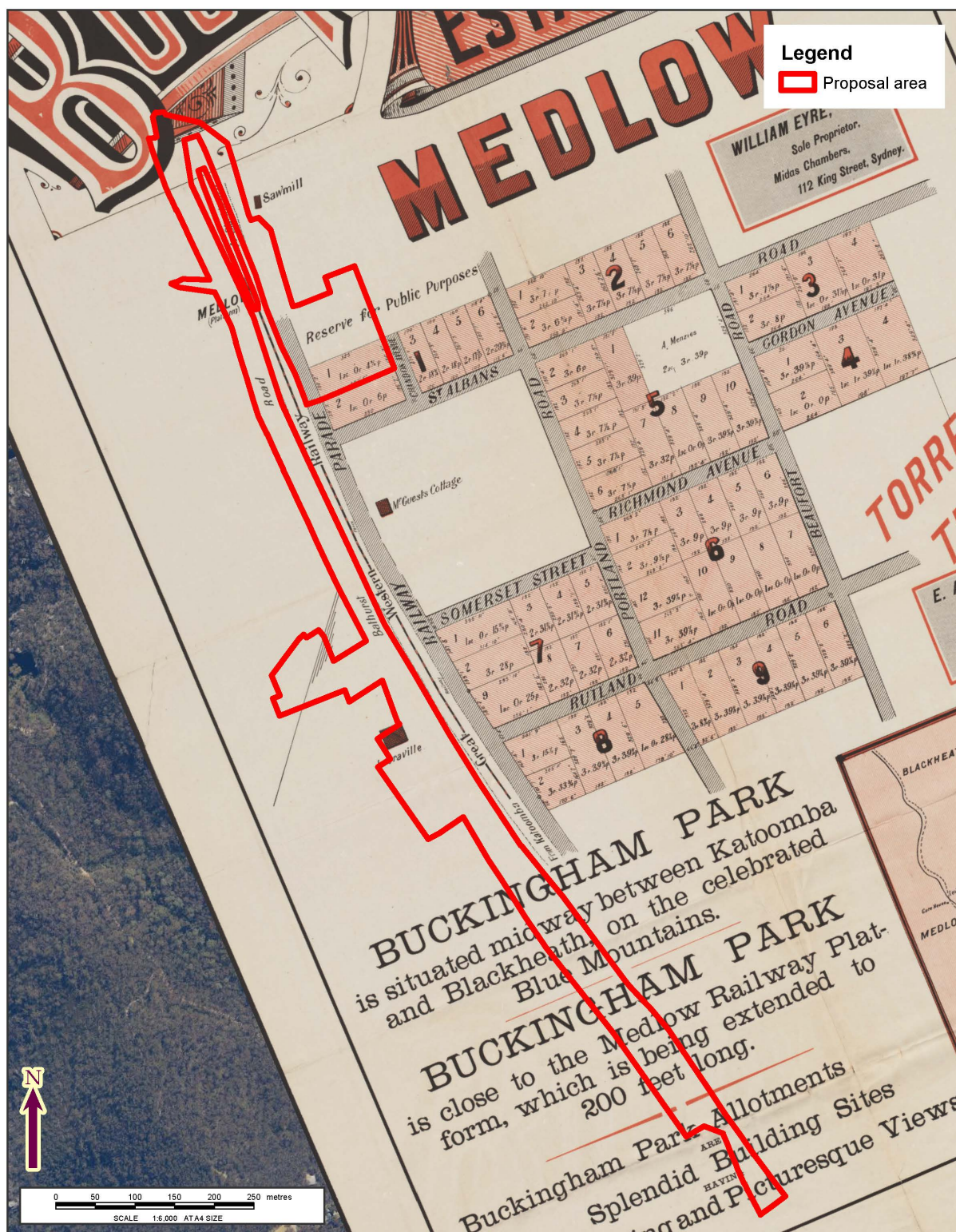


Figure 3.5: Buckingham Park Estate sales advert
 (Source: National Library of Australia object 230299961)

| | |
|---------------------------|---|
| LOCATION: MEDLOW BATH | DATUM: GDA94 PROJECTION: MGA Zone 56 |
| JOB NO.: PR147863 | Data Sources: RPS Client Land and Property 2015 |
| Technician: Luke Gliganic | Date: 19/05/2021 |

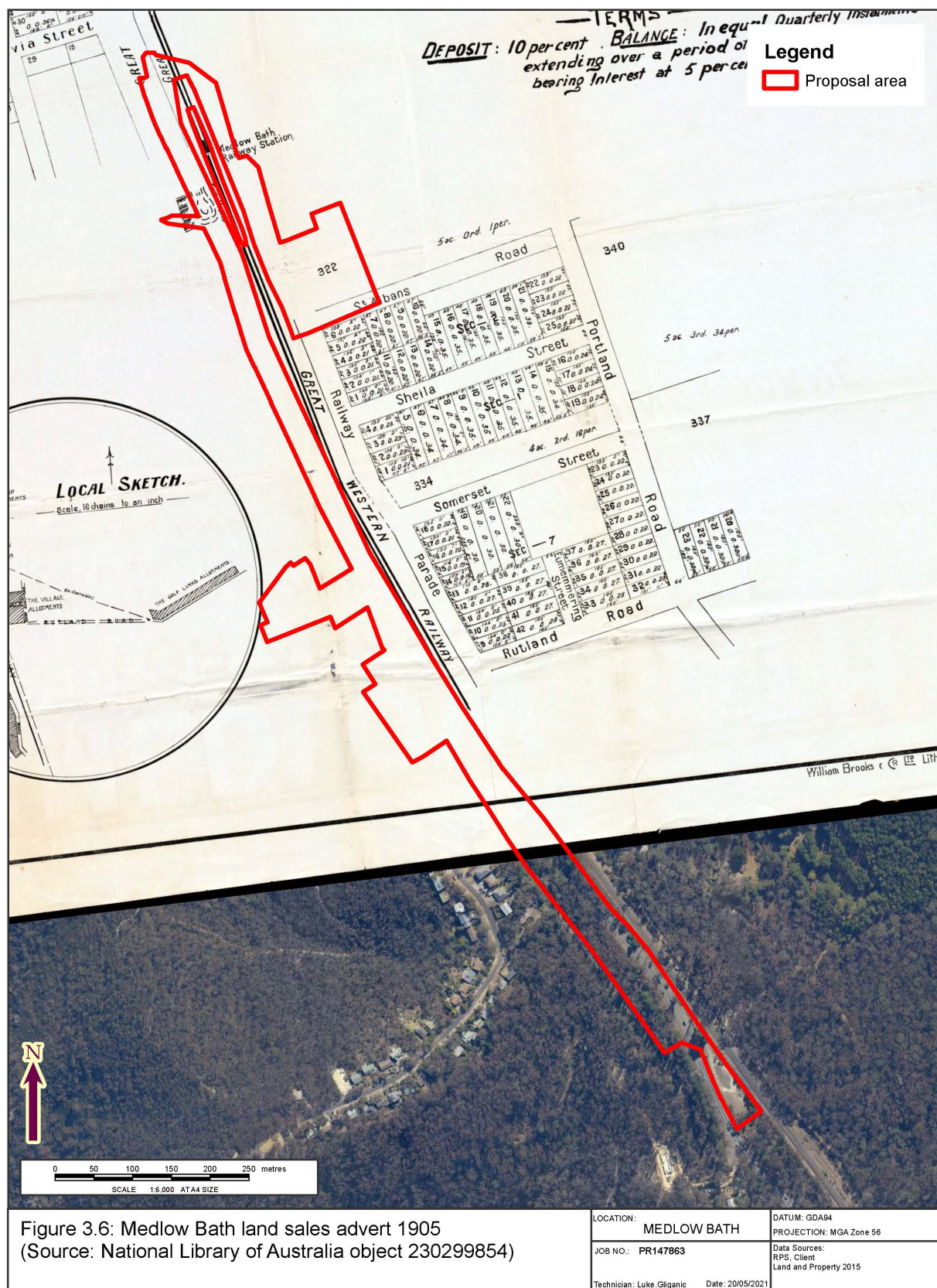
Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.5.mxd
 NEW_A4_Portal 2018 Rev B Produced: 19/05/2021 Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.6: Medlow Bath land sales advert 1905 (Source: National Library of Australia object 230299854)



REPORT

Figure 3.7:Katoomba crown land sales advert 1895 (Source: National Library of Australia object 230232863)

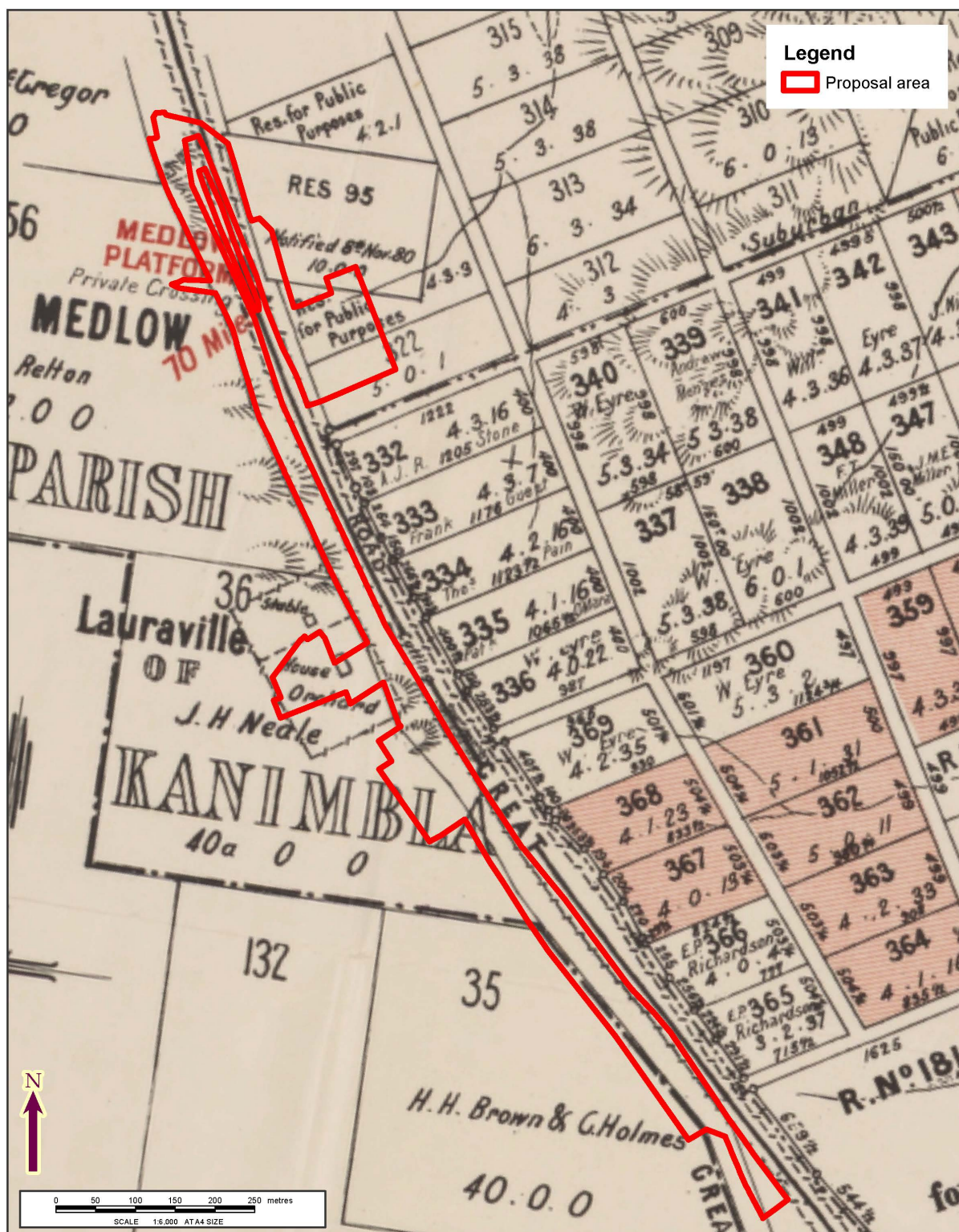


Figure 3.7: Katoomba crown land sales advertisement 1895 (Source: National Library of Australia object 230232863)

| | |
|--|-------------------------|
| LOCATION: MEDLOW BATH | DATUM: GDA94 |
| JOB NO: PR147863 | PROJECTION: MGA Zone 56 |
| Technician: Luke Gilgani | Date: 18/05/2021 |
| Data Sources: RPS, Client Land and Property 2015 | |

Path: C:\Users\Luke.Gilgani\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.6.mxd
NEW_A4_Portrait 2018 Rev-B: Produced NW Reviewed NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
Level 13, 255 Pitt Street Sydney, 2001
T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.8: Medlow Grange Estate sales advert (Source: National Library of Australia object 230299739)

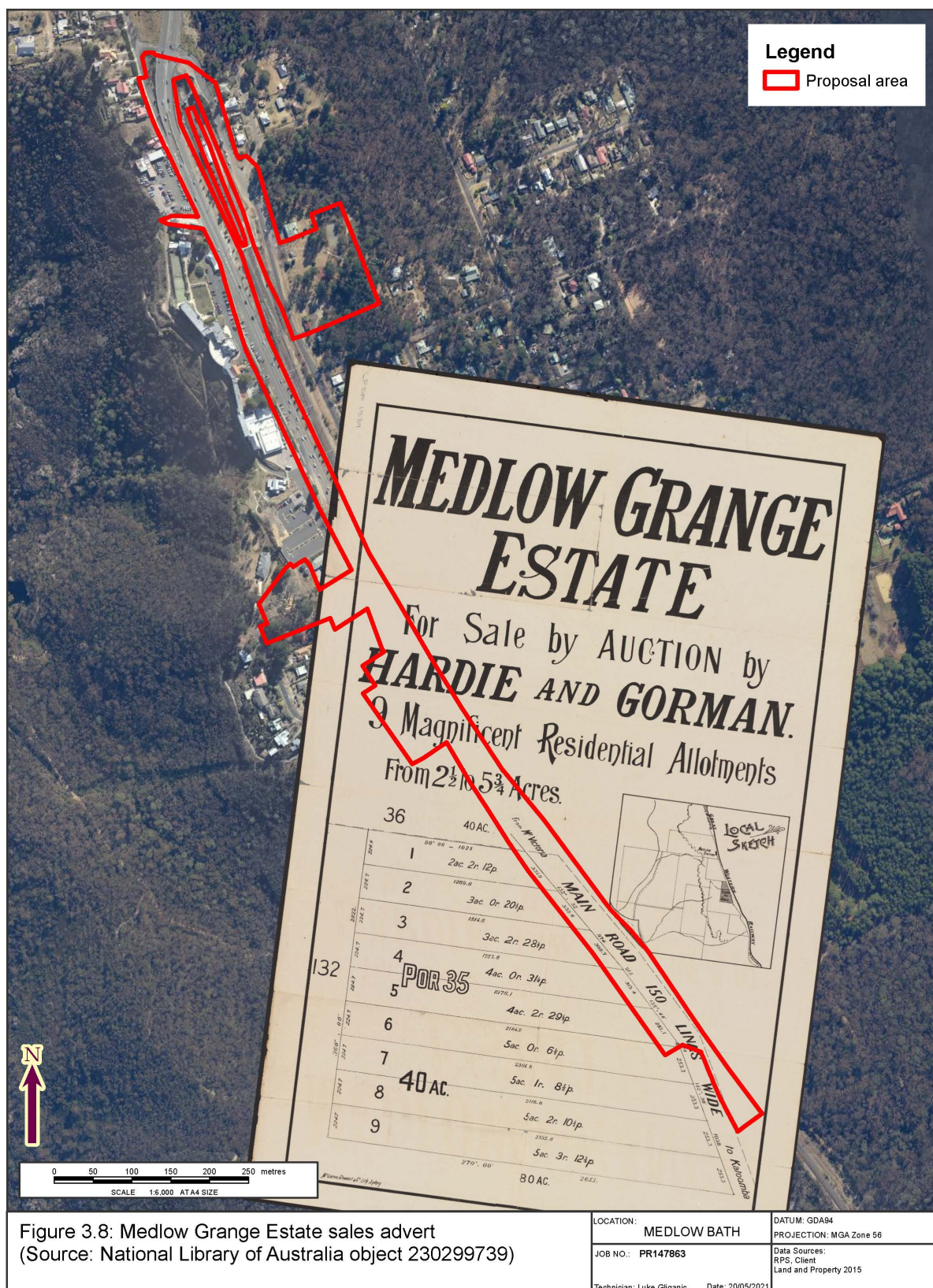


Figure 3.8: Medlow Grange Estate sales advert
 (Source: National Library of Australia object 230299739)

3.4.2 Mark Foy and the Hydro Majestic Hotel 1900 onwards

Mark Foy (1865-1950) was a businessman and sportsman. He was the third son of successful Melbourne draper, Mark Foy. In his youth he was a keen sportsman, having won medals for rifle-shooting, and later establishing the Sydney Flying Squadron to popularise sailing on Port Jackson. He was also interested in motor racing and boxing. After their father's death in 1884, Mark and his brother Francis moved to Sydney and set up a shop in Oxford Street under the style of Mark Foy senior, in memory of their father. The successful business enterprise permitted Mark to pursue his sporting interests and alternate business ventures, including opening a hydropathic resort, the Hydro Majestic Hotel at Medlow Bath (Walsh 1981).

The *Hydro Majestic* (Blue Mountains LEP Item No. MB002) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP Item No. MB026) cross the central portion of the proposal area. The hotel was developed by Mark Foy through bringing together three existing buildings into a complex from 1904 onwards. Foy began buying up land and properties in Medlow around 1900. The three buildings set to become his hydropathic establishment were:

- the country retreat of W.H. Hargraves, registrar in Equity and a trustee of the Australian Museum in Sydney. The single-storeyed house, with elaborate tree and shrub plantings, was bought by Mark Foy in 1901 and developed into the Hargravia section of the Hydro (Plate 3.5).
- the existing Belgravia Hotel to the north of Hargraves' house. The Belgravia had been opened in 1891 by Mr and Mrs Ellis and was acquired by Mark Foy in 1903 (Plate 3.6).
- a cottage owned by Alfred Tucker (Plate 3.7), whose widow later ran the Wonderland Park guesthouse to the north of the gatekeeper's cottage (Heritage NSW 2020b).

On 4 July 1904 Mark Foy opened his hydropathic establishment. Foy employed Dr Georg Bauer of the *Shoeneck* health spa on Lake Lucerne in Switzerland to devise and supervise a program of diets and treatments. The Hydro advertised cures for nervous, alimentary, respiratory and circulatory ailments, but excluded sufferers from infectious diseases such as tuberculosis and those with mental illness (Heritage NSW 2020b; Hydro Majestic 2020). The Hydro soon advertised itself as the "premier tourist resort" (Plate 3.8).

A casino with a prefabricated domed ceiling imported from Chicago was erected between Hargravia and Belgravia and hosted regular performances by international stars (Plate 3.9). A picture gallery several hundred feet long lined with an extensive art collection joined the buildings together, offering superb views over the Megalong Valley (Heritage NSW 2020b; Hydro Majestic 2020; Kaldy 1983, 36). The grounds boasted a croquet lawn, tennis courts, swimming pool and gardens with a series of pathways down the slope to Megalong. A stable of horses provided guests the chance to explore the Megalong (Plate 3.10), while Mark Foy's famous fleet of motor-cars took them on more extended trips such as the Jenolan Caves (Plate 3.11). The kitchens were supplied by Foy's farm in the Megalong valley below, with produce brought up on a flying-fox (Heritage NSW 2020b).

The Hydro's boundary with the Great Western Highway is marked by a fence of rockfaced sandstone piers with turned sandstone balusters and sandstone cappings and taller gateposts of matching design (Heritage NSW 2020b).

Foy's Hydro Majestic capitalised on the growing attraction of the Blue Mountains as a retreat for middle and upper middle classes to escape Sydney. The arrival of the railway in the late nineteenth century opened up the Blue Mountains to holiday makers, fuelling the opening of guest houses and luxury hotels like the Hydro Majestic. To emphasise the Hydro and provide a distinct point of reference for all travellers to the Hydro by both road and rail, Foy planted an avenue of pine trees (*Avenue of trees* (formerly *Avenue of Radiata Pines*) (Blue Mountains LEP Item No. MB015)) around 1904. The single line of pines fall within the proposal area and run between the Great Western Highway and the railway line from the eastern end of the Hydro Majestic estate to the railway bridge and then down Station Street as far as number 33, the former Gatekeeper's cottage (Plate 3.12). The extension of the avenue down Station Street highlighted that *Sheleagh Cottage* (Blue Mountains LEP Item No. MB019) at number 6 was also part of the Foy domain. After a storm in 2011 caused one of the historic trees to fall onto a passenger train, for safety reasons the historic trees forming *Avenue of Radiata Pines* were removed. The trees were replaced with the existing trees by the local Medlow Bath community and Council to retain the character of the township (ABC News 2011, Heritage NSW 2020i). The heritage item, *Avenue of Radiata Pines* was then renamed *Avenue of trees* in 2021.

In 1922 the Belgravia and picture gallery were severely damaged by fire (Plate 3.13). It took fourteen years to rebuild but the lost buildings were replaced and the hotel recovered. During World War II, the Hydro was

REPORT

used as a convalescent hospital for American servicemen. In 1946, a new building was opened with bedrooms on the upper floors and a large lounge downstairs (Hydro Majestic 2020).

In 2008, the restoration of the Hydro Majestic commenced and the hotel reopened in 2014.

Plate 3.5: Hargrave's House 1901 (Source: Blue Mountains City Library item 00469)

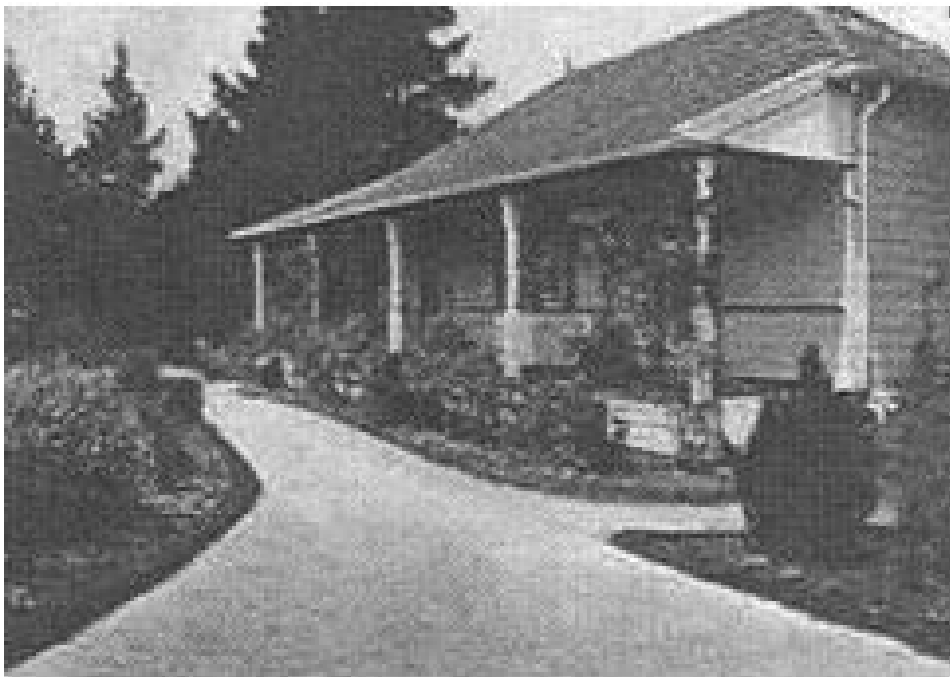


REPORT

Plate 3.6: Belgravia, early 1900s (Source: Flickr)



Plate 3.7: Tucker's Cottage (Source: Blue Mountains Association of Cultural Heritage Organisations Inc.)



REPORT

Plate 3.8: Hydro-Majestic Hotel advertisement (Source: National Library of Australia object 136185419)

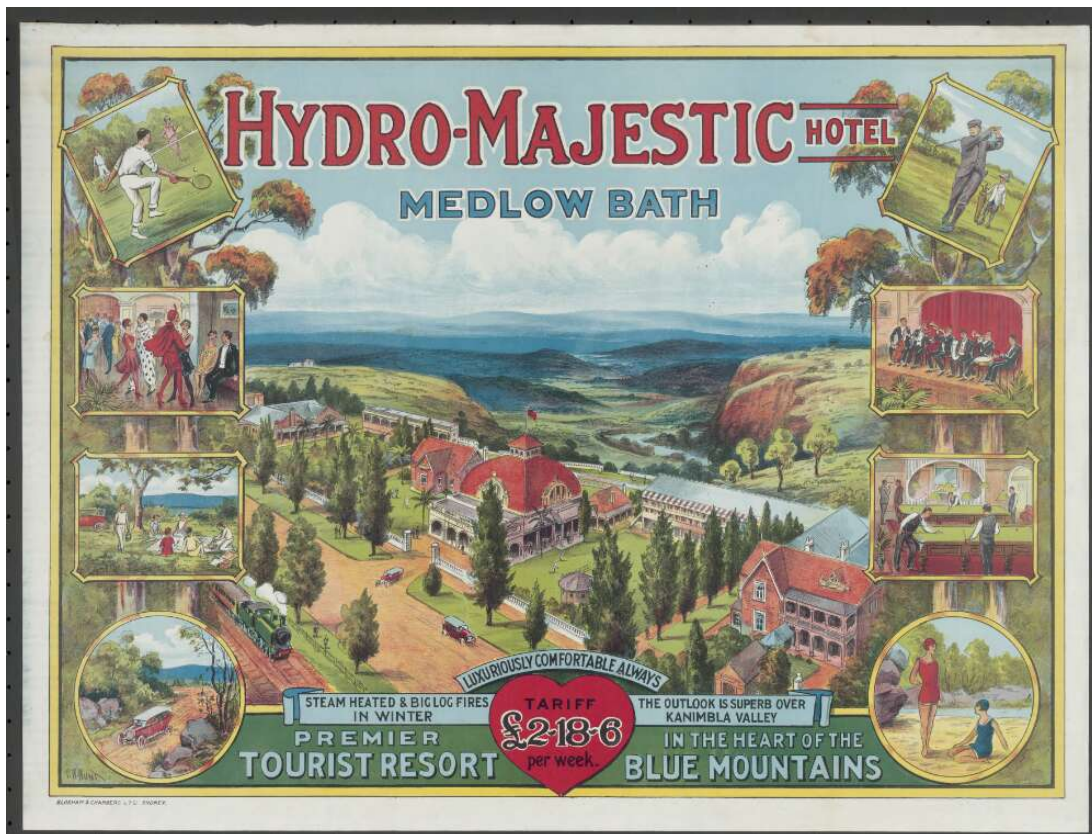


Plate 3.9: Hydro Majestic outside casino 1910 (Source: Blue Mountains City Library item 00443)



REPORT

Plate 3.10: Medlow Bath stables early 1900s (Source: National Library of Australia object 146216810)



Plate 3.11: Mark Foy with his wife, son and guests outside the Belgravia end of the Hydro around 1906 (Source: Smith 2020, 55)



REPORT

Plate 3.12: The Hydro Majestic from above 1938, showing Avenue of trees running north between the Great Western Highway and the railway (Source: Blue Mountains City Library item 000095)



Plate 3.13: Hydro Majestic after 1922 fire (Source: Blue Mountains City Library item 000823)



3.4.3 Residential development 1900 onwards

Following the subdivision and sale of land for the township of Medlow in the early 1900s, construction of houses and holiday retreats commenced. Mark Foy had visions of the town as a 'Garden Suburb'. He began to create major land subdivisions in Medlow Bath that embodied these ideas, however onerous restrictions he placed on purchasers and land meant few sales. After decades spent trying to create his vision, Foy was forced to abandon his 'Garden Suburb' dream (Smith 2020, 43-44).

Aerial imagery from around 1930 shows that the majority of the residential development in Medlow Bath occurred on the eastern side of the railway line, away from the proposal area (Plate 3.14 to Plate 3.16). However, at the northern (westbound) end of the proposal area on the western side of Station Street lie a group of four houses all constructed during the early twentieth century growth of Medlow Bath. Together the houses are listed as heritage item *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP Item No. MB019).

Sheleagh Cottage was the summer residence of Mark Foy and his family. Immediately after Foy purchased what would become the Hydro, he commissioned Sir Herbert Ross to design the summer residence for him. The single storey hipped roof house, *Sheleagh Cottage* (also referred to as Shelaugh Cottage and Shelia Cottage), was named after Foy's third daughter and constructed at 6 Station Street primarily from mahogany between 1901 and 1903 (Heritage NSW 2020i; Kaldy 1983, 37).

The Foy family sold the house to a stonemason named Bagley in around 1958. Bagley added the stone benches, walling and sundial which adorn the house today. In around 1978 the house was purchased by Wilmot-Farden, who sold it to the present owner Mrs Helen Craig in 1995. The outhouse known as 'The Loft' belongs to the Mark Foy period, however the shed constructed from sleepers and known as 'The Pub' is most likely a later addition (Heritage NSW 2020i).

To the south of *Sheleagh Cottage* is *Cosy Cot* at 4 Station Street. The single storey weatherboard cottage was constructed around 1900 to 1910, and was the mountain retreat of Henry Hartley who, with his mother and step father ran a famous chain of bakeries and refreshment rooms in New South Wales and Victoria known for their Sargent's pies. Hartley's health was reduced from his war time experiences and he spent most of his remaining years at *Cosy Cot*. He died there in 1924 after falling from the cliff edge immediately beyond his own grounds. The house has remained in the Sargent family (Heritage NSW 2020i).

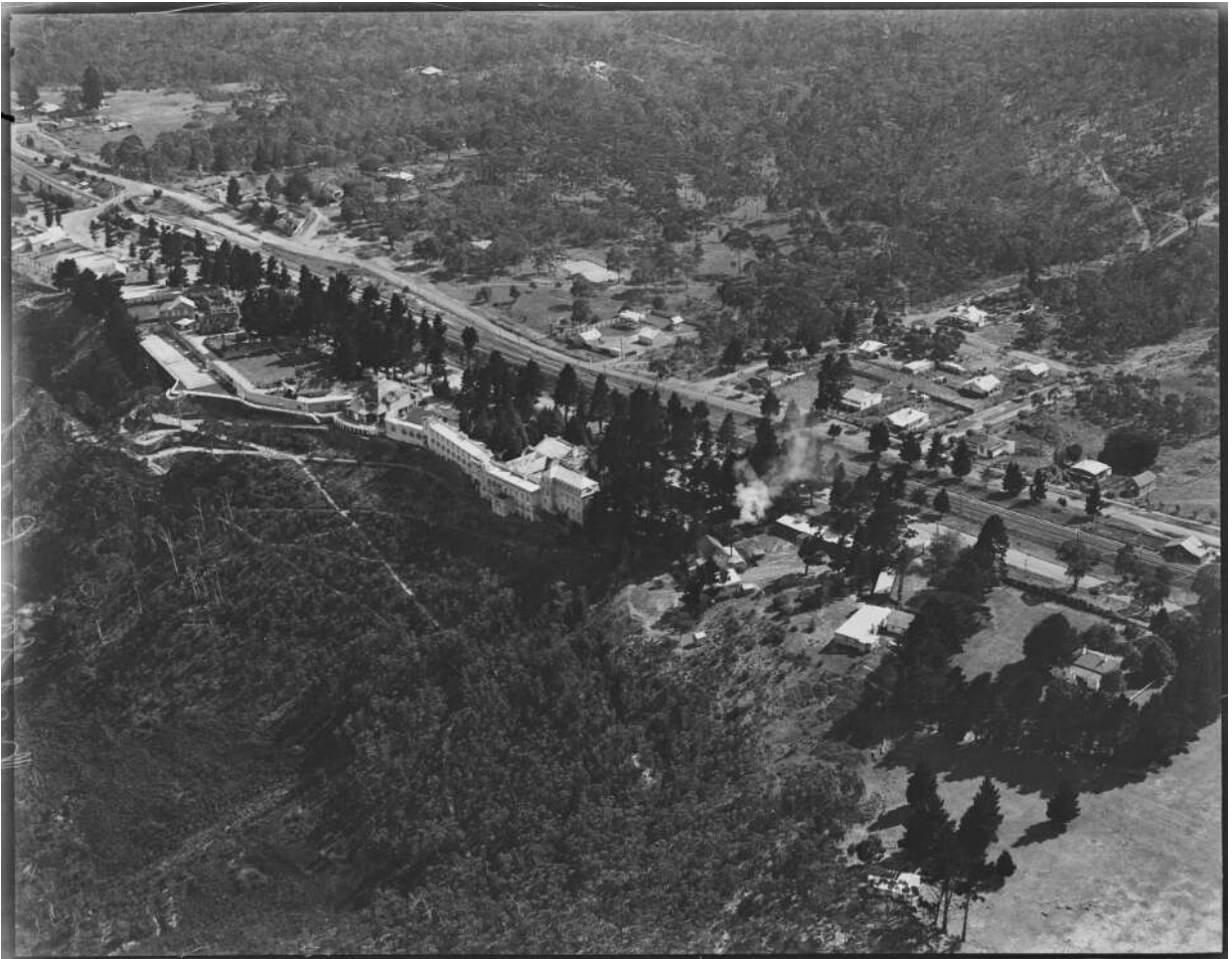
To the south of *Cosy Cot*, lies *Melbourne House* at 2 Station Street. The single storey weatherboard house was built around 1908 by Mr Saunders, who appears to have remained at the property until around 1940. From 1940 onwards, Miss Rose Pritz resided at *Melbourne House*.

On the southern side of the block of vacant land next to *Melbourne House*, opposite the railway station, lies the fourth early twentieth century property at Lot 1 Great Western Highway. It comprises a brick single storey shop with attached single storey residence on the south side (Plate 3.15).

To the immediate south of the Hydro Majestic was Lauraville, which by 1895 was an orchard with a house and stables. Foy leases this 40 acres of land from 1912 and called it Glenara Park and the Glenara Cottage. Foy finally purchased the land in 1927, completing his collection of land along the western side of the Great Western Highway through Medlow Bath (Smith 2020, 28).

REPORT

Plate 3.14: Medlow Bath from above, around 1930, showing the Hydro Majestic overlooking the valley in the foreground and the limited development of Medlow Bath on the eastern side of the railway in the background and Glenara Cottage within a stand of trees (lower right)
(Source: National Library of Australia object 162354898)



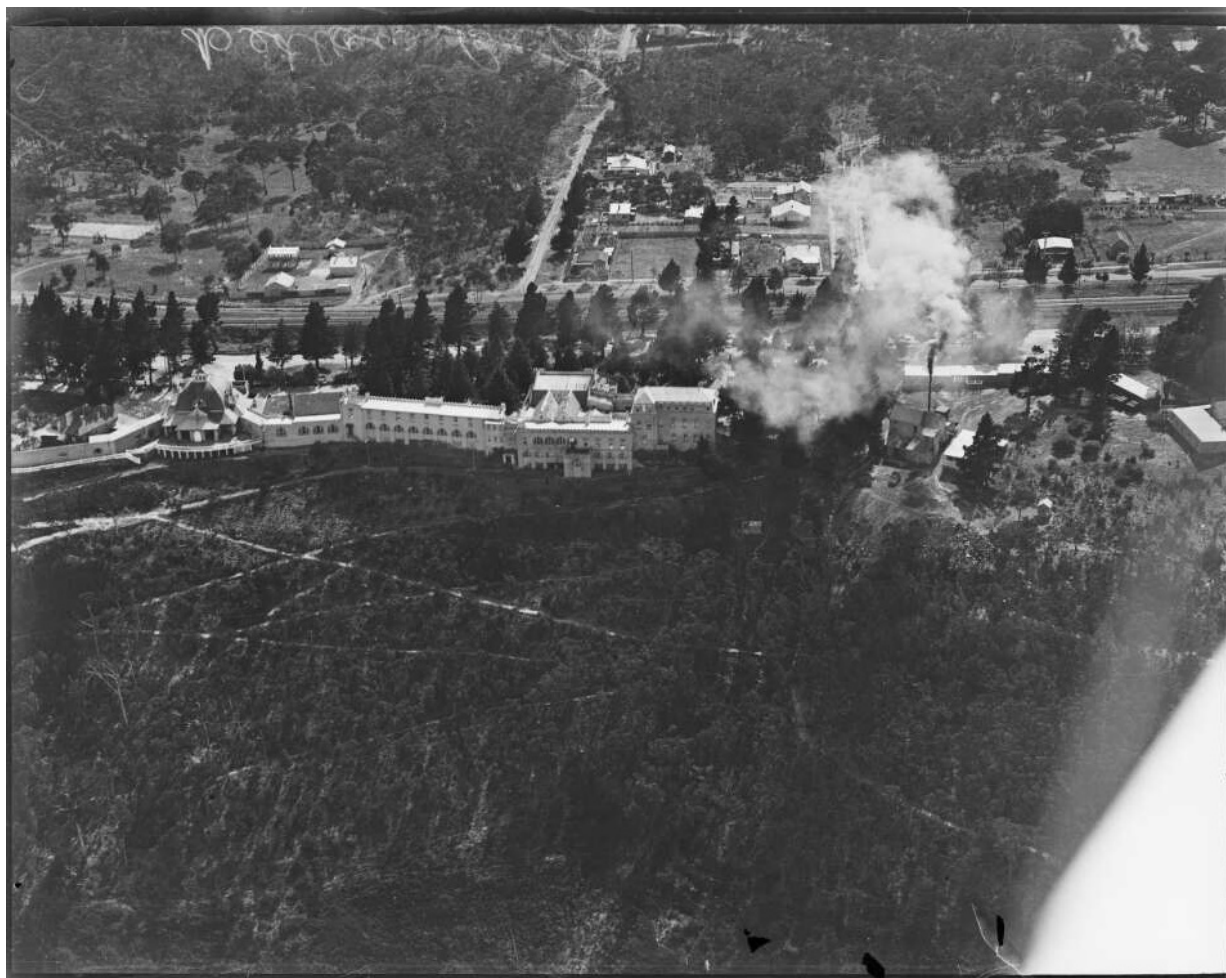
REPORT

Plate 3.15: Medlow Bath from above, around 1930, looking south along the Great Western Highway showing Hydro Majestic to right, township to left and the house and shop at Lot 1 Great Western Highway to lower left (Source: National Library of Australia object 162355018)



REPORT

Plate 3.16: Medlow Bath from above, around 1930, showing Hydro Majestic to foreground and Medlow Bath township to rear (Source: National Library of Australia object 162355132)



3.4.4 Medlow Post and Telegraph Store 1903 onwards

Subdivision plans indicate that the original town post office was operated from J.H. Neale's "*Lauraville*" orchard. However, it seems the first official Medlow post office was operated out of the railway station by Anne Kirkland from 1903 to 1906 and H.E. Munro from 1906 to 1910 (Kaldy 1983, 65). The site of the *Post and Telegraph Store, Former* (Blue Mountains LEP Item No. MB008) to the north east of the proposal area was first purchased by Isabella Jane Smith in 1903. By 1905, Mark Foy had purchased the site from Smith and built a hall to be used for dances for Hydro guests. The hall was built from timber and corrugated iron recycled from the hall in the abandoned shale village of Nellies Glen. The hall was known as 'Flanigan's Hall' because Mrs Flanagan ran a boarding house nearby on the corner of Atlingworth Parade and seems to have managed the hall as well (Heritage NSW 2020d).

In 1910, Foy had a store added to the front of the building, with premises for a post office attached to the northern side. The front extension was given a castellated front where the words "Post Store Telegraph" were written, with "Refreshment" on the right hand side of the building. From around 1910 to 1916 it was operated by Mr Flanagan as the Medlow Bath Post and Telegraph's Office, with one telephone and a small store and refreshment room. From 1916 Mr Tucker became the Post Master and operated the post office out of premises on the southern side of the highway (Kaldy 1983, 49, 65).

After 1918, the building took on a different role under Jack Rice, head chauffeur to Mark Foy. The tourist industry developed by Foy included a steam Serpollet charabanc, a De Dion Bouton and Packard cars to take visitors on day drips from the Hydro to the Jenolan Caves and other nearby sights (Plate 3.11). Flanigan's Hall, now known as Rice's Hall, became the base of this operation between the wars. The onsite petrol browser made the perfect location to commence the vehicle trips with the hall serving refreshments to

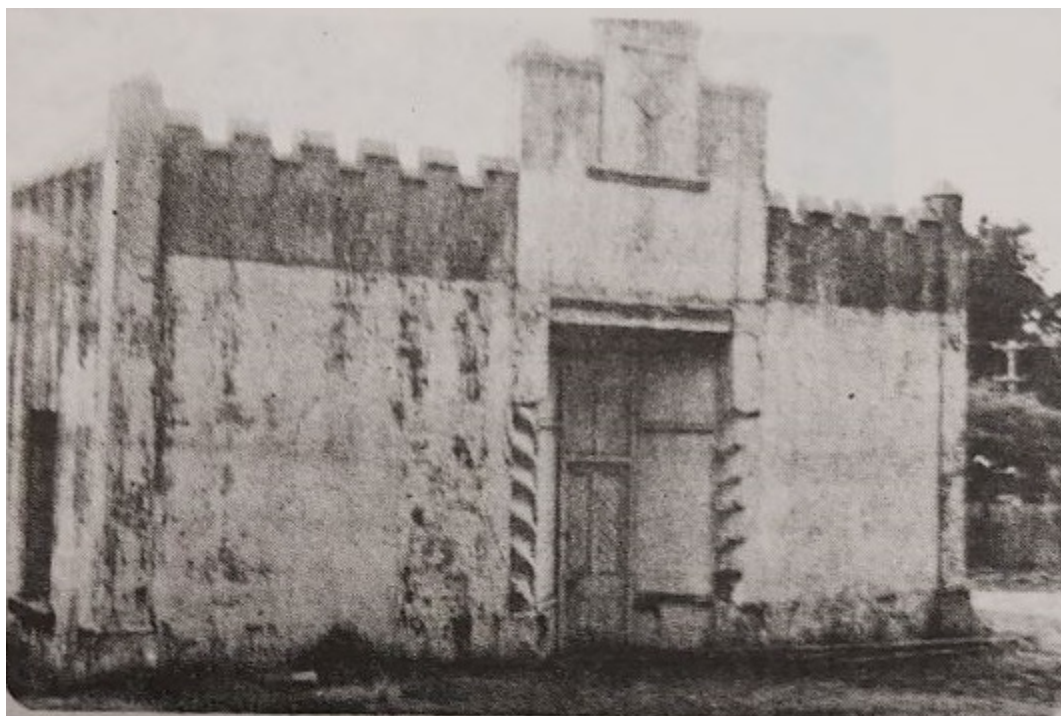
REPORT

the patrons. Also during the inter-war years, the hall was used as a cinema annexe to the Hydro. During World War II it was used by American servicemen for recreation (Heritage NSW 2020d).

After World War II, both the hall and what had been Mrs Flanagan's guesthouse were in need of repair. They were sold off separately by the Hydro. The Roman Catholic church purchased the hall in 1952 and used it as a church and church-hall until 1967. Throughout this period, the hall was also used for a very wide variety of local social activities, including the Progress Association, the library, a polling booth, a Centre for Rural Youth and charity sales. The building deteriorated significantly throughout the following years until it was purchased by Mr W.Boldiston in 1980 who conserved the hall and stall. The post office annexe was demolished in 1986 (Heritage NSW 2020d).

Today, the building is now an antique shop, book-shop and tearoom, run by Peter Koehndorfer under the ownership of Garry Coxen. The highly significant early petrol pump was removed from the front for safety and is secure, with other motoring relics, in the adjacent shed, however the old wooden mile-marker which also stood outside has been stolen (Heritage NSW 2020d).

Plate 3.17: Post and Telegraph Office, Former around 1983 (Source: Kaldy 1983, 65)



3.4.5 St Luke's Anglican Church 1908 onwards

Medlow Bath fell within the Anglican jurisdiction of Blackheath, with ministers from St Aiden's in Blackheath conducting the Anglican church services in Medlow Bath. These were initially conducted in a cave and then a private residence. As the congregations grew, land in Railway Parade to the east of the proposal area was purchased by the church from Tom Pain in 1908, with *St Luke's Anglican Church* (Blue Mountains LEP Item No.MB010) built in 1913. Primary supporters of the church were Captain and Mrs Wade-Browne, who were prominent Anglicans residing in Medlow Bath.

The church was originally built with a nave, chancel and transept, however the chancel was partly destroyed through storm damage in 1920. Repairs to the church saw the transept removed and the chancel rebuilt to join the nave without a transept. This resulted in the church being shortened by some 5 metres and taking on its present simple rectangular shape as a two bay gabled church on an east west axis with an entry vestibule on west end and apsidal chancel at its east end. St Luke's stained glass windows were donated, one at the time of opening in 1913 by 'H.P.P.', the other in memory of a minister, the Reverend J. Read. The church bell is a former ship's bell, recovered from the 'Princess Alexandra' which had been wrecked on the Bellingen bar in northern New South Wales in 1874 (Heritage NSW 2020e).

The church has since been deconsecrated and converted into a one bedroom home.

3.4.6 Annis and George Bills horse-trough 1930-1940

Annis and George Bills were animal lovers who gave considerable sums to animal societies such as the Dowling Street Dogs Home and a shelter for horses in Kingswood/Maroubra. They also donated to the (English) Metropolitan Drinking Fountain and Cattle Troughs Association (Heritage NSW 2020f).

When George died in 1927 he left an estate of £91,000. After various personal bequests, the remainder of his estate was to be used to construct and pay for horse troughs wherever they may be necessary for the relief of horses or other dumb animals in Australasia, the British Isles or any other part of the world subject to the consent of the proper authorities. Their purposed was to prevent cruelty and alleviate the suffering of animals (Heritage NSW 2020f; Merriman 2015).

The troughs were erected between 1930 and 1940, providing employment in the Depression years. Around 700 troughs were erected in Australia, with the majority in Victoria, and around 50 erected overseas, including the UK and Dublin. The troughs were usually of reinforced concrete and were of a standard design. Rare examples were occasionally in granite. The Medlow Bath example, *Horse Trough* (Blue Mountains LEP Item No.MB0013) located east of the proposal area at the corner of Somerset Street and Railway Parade, is a characteristic Bills' trough, with a small trough for small animals such as dogs to the right (Heritage NSW 2020f).

In the early stages of trough supply, each was individually designed and constructed. Later a standard design was adopted, and Rocla concrete products supplied many hundreds of the troughs in Victoria and New South Wales. The troughs were supplied on application to the Bills Trust by Councils. However, the growing use of the motor car and trucks caused a halt in demand for the troughs and none have been installed since World War II (Merriman 2015).

3.5 Medlow Bath Railway Station

3.5.1 Early Development (1881-1940s)

Medlow Bath Railway Station (SHR No.01190, Blue Mountains LEP Item No. MB003) originally comprised a 30 metre long wooden platform and associated structures built in 1881, situated on the country side of the single line. Additions to the station buildings were made in 1899 (Plate 3.18). Duplication of the line in 1902 resulted in the replacement of the original platform with the present platform and the construction of new platform buildings (Heritage NSW 2000a). No plans of the original platform construction have been identified. Early images of the island platform indicate that it had a gravelled surface with concrete deck (Plate 3.19 and Plate 3.20). Similar to other platforms in the Blue Mountains, the platform face was constructed of unreinforced concrete cast in situ (AECOM 2017, 16).

A small timber signal box was also located on the platform. An open interlocking lever frame was erected on the platform in 1909, which appears to have been covered by the existing structure in around 1922. The signal box was taken out of service when the line was electrified in 1957 and was then used as a store. It is a rare example of a separate platform level signal box. An additional siding was built in 1926. Undated plans (Figure 3.9) of the station drawn after the additional siding was added in 1926 show that a waiting shed once stood at the Sydney end of the platform as well as a goods shed on the highway side of the station.

The platform was extended at the Sydney end in 1942 to bring the overall length of the platform to 600 feet. Plans from this extension (Figure 3.10) and aerial imagery from 1943 (Figure 3.11) indicate that the goods shed on the highway side of the station was still present after the platform was extended but the former waiting shed had been demolished to accommodate the platform extension. The plans also indicate that the platform wall was raised and presumably included modifications to the coping (AECOM 2017, 16; Heritage NSW 2000a).

The initial construction of the railway network in NSW required a large number of staff to manage the stations, signalling and other aspects of the operation of the railway (Futurepast Heritage 2013: 4). Railway residences were constructed at nearly every station across NSW, and as many as 300 residences were built throughout the state by the Railways (Futurepast Heritage 2013,12). Purpose-built railway residences were primarily provided for the Station Master and his family, although a number of other types of temporary accommodation were also provided for railway staff. It was common practice at the time of construction for various public offices, including police and post offices, to provide similar accommodation for senior members of staff (Futurepast Heritage 2013, 7).

REPORT

Urunga (Blue Mountains LEP Item No. MB017) is located immediately east of the proposal area. It was built around 1916 to accommodate the station master at Medlow Bath as the station had become busier with the success of Mark Foy's Hydro. The building is a symmetrical gabled cottage fronting Park Street, with a skillion verandah, double hung windows either side of a four panel door with toplight, corrugated steel roof and corbelled brick chimneys and rusticated weatherboard cladding. The gate in its Station Street fence leads directly to Medlow Bath Station. *Urunga* remained a railway house until 1998 when it became a private residence (Heritage NSW 2020b).

The successful rail crossing of the Blue Mountains saw a shift in focus from road travel to rail in the region. This had an extensive impact on the development of Medlow Bath and other towns in the region which were now on the direct train line from Sydney to Bathurst. Not only did it boost residential land subdivision and sales, it opened up Medlow Bath and the greater Blue Mountains area as a holiday retreat for those looking to escape Sydney. The railway line and Medlow Bath station are intimately linked to holiday escapes, guest houses and luxury hotels in the area.

3.5.2 Post War (1944) - Present

The tracks were slewed in 1952 for the insertion of overhead wiring structures in preparation for electrification. The track associated with Platform 1 was slewed again in 1980/1981 and a section of the coping cut back (AEOCOM 2017, 16).

Today, *Medlow Bath Railway Station* (SHR No.01190, Blue Mountains LEP Item No. MB003) (Plate 3.21 and Plate 3.22) is an unattended station and comprises the 1902 Station Building, 1922 signal room, 1902 island platform and 1901 Footbridge.

The Station Building is constructed in the standard 'A8' Federation style. Built of face brick with corrugated metal gabled roof extending as an awning over both platforms. It has six bays with tuckpointed red brickwork and engaged piers between. Other features include rendered and moulded rows of string courses, moulded cornice, timber framed windows and doors with contrasting decorative trims and sills, standard iron brackets over decorative corbels supporting platform awnings, fretted timber work to both ends of awnings and gable ends, timber finials to gable apex, tall corbelled chimneys, timber framed double-hung windows with multi-paned and coloured upper sashes, and timber door openings with multi-paned fanlights with coloured glazing (Heritage NSW 2000a).

Internally, the Station Building comprised a booking office, waiting room, ladies waiting and toilets and male toilets accessed from the south end of the building. The interiors generally feature custom orb corrugated iron ceilings with ceiling roses, fireplaces with no grates, timber floor boards to main rooms and tiling to toilets, beaded dado line and timber bead style moulded cornices. Toilet fittings are modern (Heritage NSW 2020a).

The signal room is a small timber building at the booking office end of the station. Evidence suggests that the weatherboard structure covered an interlocking lever frame, which was originally erected as an open lever frame. After being taken out of service in 1957, it appears to have been used as a store room. It has a steep gabled corrugated metal roof, rusticated timber boarding with small four-paned windows on three elevations, and a four-panelled timber door with timber awning on the south elevation (Heritage NSW 2020a).

The 1902 island platform runs north-south, with the buildings centrally located. The platform is brick faced with a concrete deck and asphalt finish. Two raised round shaped slabs are located to the south of the station building probably for access to the services below the platform. Manicured garden beds along the platform enhance the setting of the station. Modern platform furniture including light fittings, signage, timber bench seating and aluminium palisade fencing at both ends of the platform have been added to the platform. In 2019 the platforms were modified to accommodate the new Intercity Fleet of trains through cutbacks and built outs and recessing of services (Heritage NSW 2020a).

The 1901 footbridge is a standard concrete slab structure supported on original brick abutments and two steel trestles with new stairs to the platform and bridge, with a new concrete deck over the tracks spanning between the Great Western Highway and Railway Parade. The footbridge marks the northern end of the station, while a concrete level crossing marks the southern end of the station. The footbridge was cleaned and repainted and upgraded with a new deck and the addition of metal balustrades in 1994 (Heritage NSW 2020a).

REPORT

Plate 3.18: Elizabeth Foy with Mark Foy Jr and unidentified friends on original wooden Medlow Bath station, outside waiting room, December 1901 (Source: Smith 2020, 25)



Plate 3.19: American actress May Yohé, former wife of Lord Francis Hope, is greeted on Medlow Bath Station by Mark Foy around 1906 (Source: Blue Mountains City Library item 000837)



REPORT

Plate 3.20: American actress May Yohé, former wife of Lord Francis Hope, is greeted on Medlow Bath Station by Mark Foy around 1906 (Source: National Library of Australia object 146217717 and Smith 2020, 47))



REPORT

Plate 3.21: Snow at Medlow Bath Station, Blue Mountains, New South Wales around 1935 (Source: National Library of Australia object 141914096)



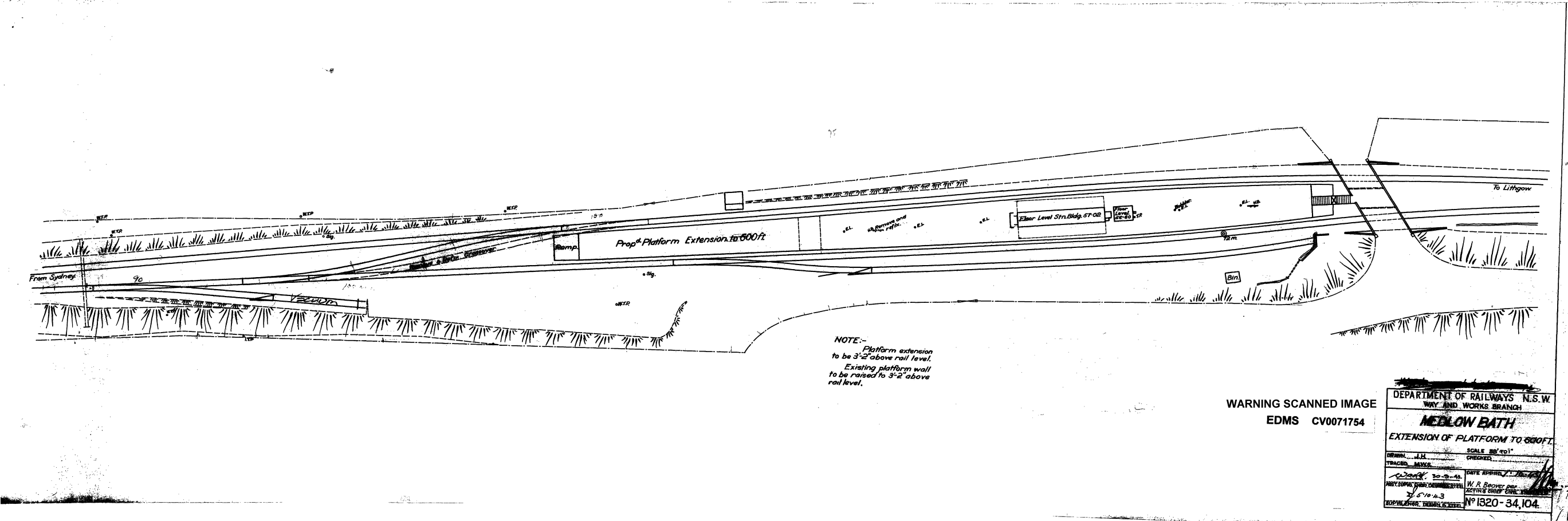
Plate 3.22: Medlow Bath Railway Station around 1950, showing platform building, signal box and platform plantings (Source: Blue Mountains City Library item 000734)



Figure 3.9: Plan of Medlow Bath Railway Station, around 1930 (Source: TfNSW)



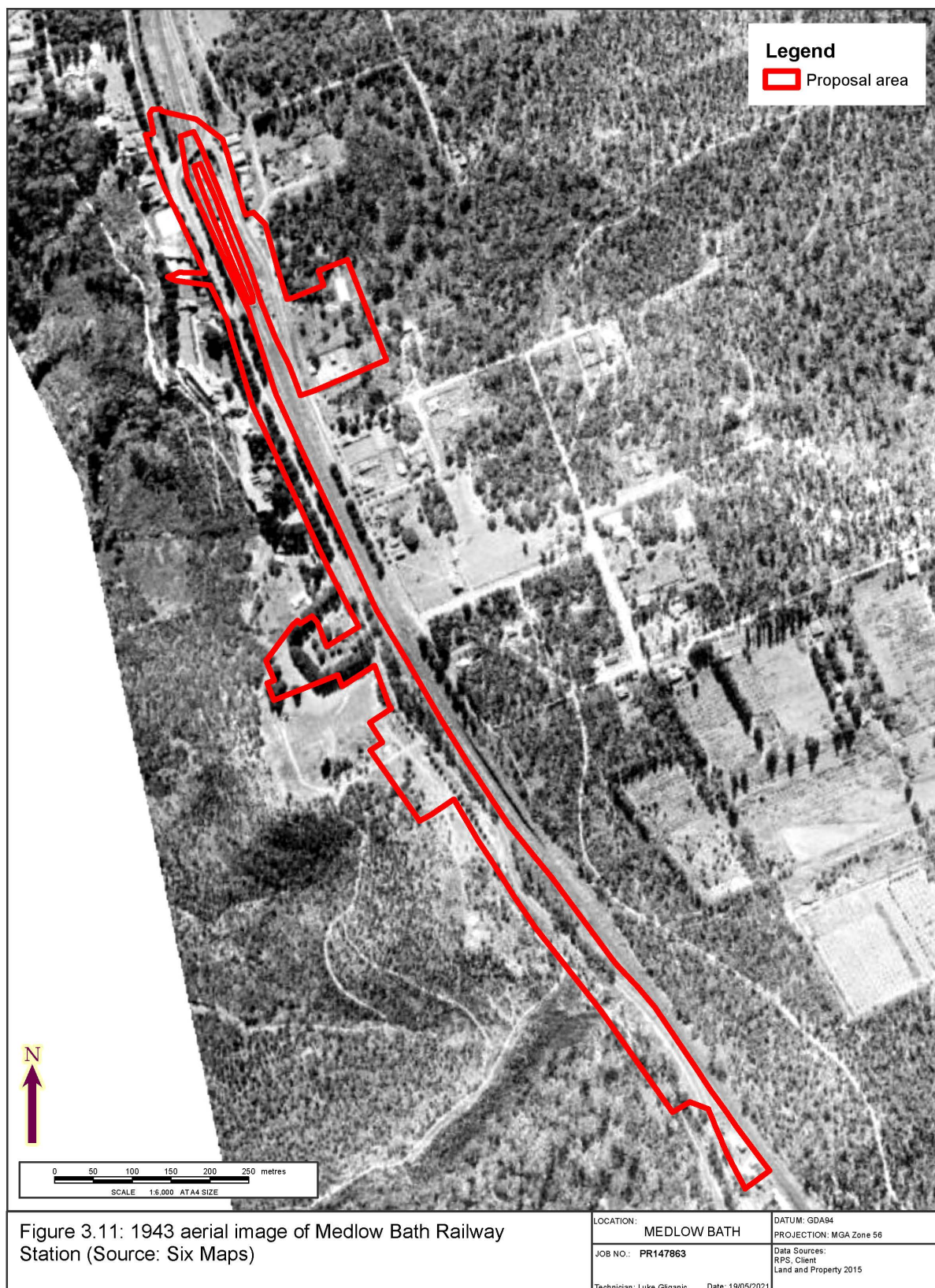
Figure 3.10: Plan “Medlow Bath extension of platform to 600FT” 1943 (Source: TfNSW)



3.6 Aerial image development timeline

The following series of aerial images show the development of Medlow Bath between 1943 and 1994. Much of the township was established by 1958, with little development occurring afterwards. The most noticeable development is that of the land immediately north of the Hydro Majestic and south of the group of cottages referred to as *Melbourne House*, *Cosy Cot*, *Sheleagh Cottage* (Blue Mountains LEP Item No. MB019) between 1966 and 1994. However, the aerial images indicate that Glenara Cottage and its stand of trees is intact in 1943 but by 1958 the south east portion of the block has been cleared of its trees. The building to the north of the block appears to have gone by 1966 and replaced by 1994.

Figure 3.11: 1943 aerial image of Medlow Bath (Source: Six Maps)



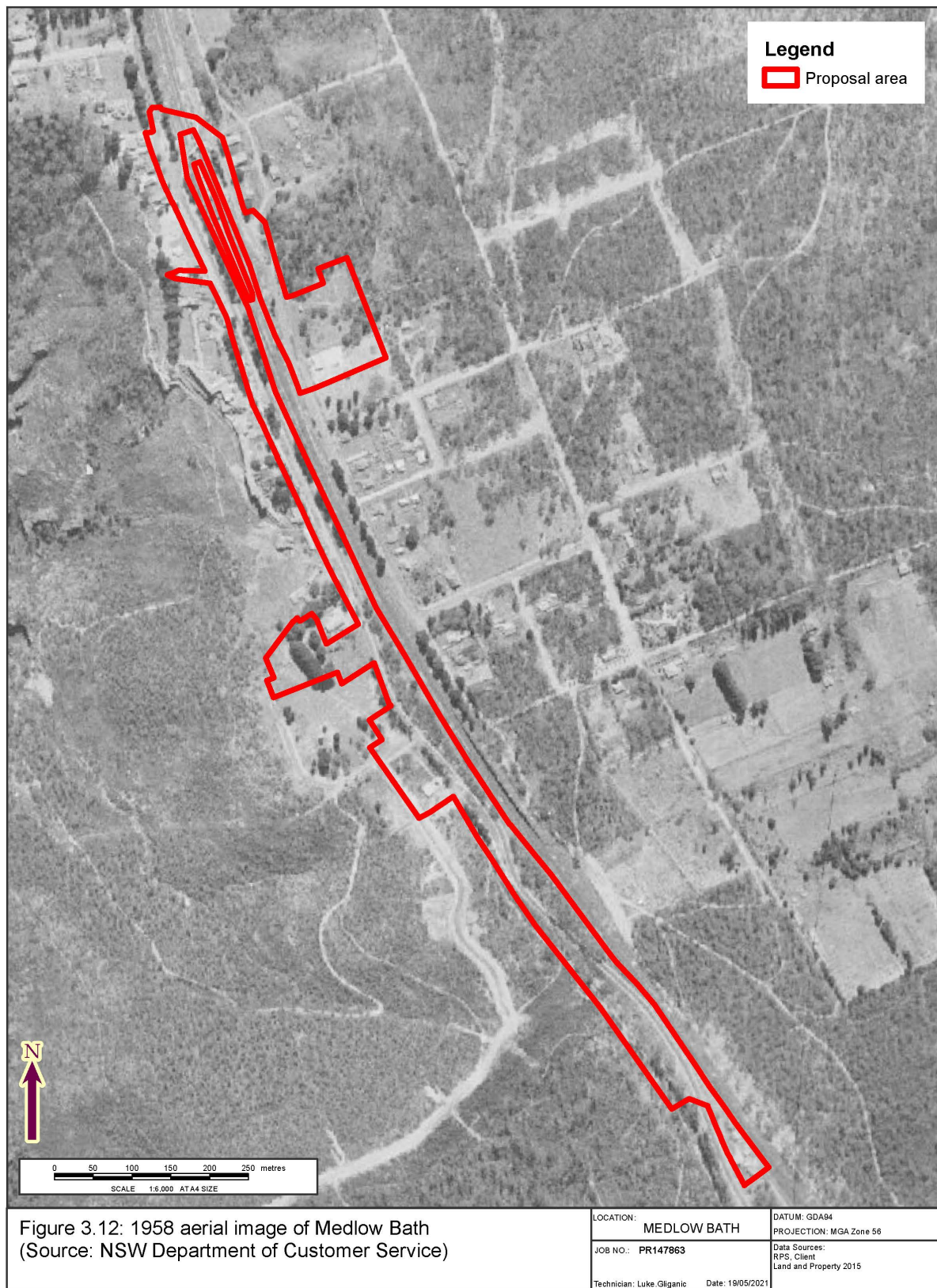
Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.8.mxd
NEW_A4_Portrait 2018 Rev:B Produced: NW Reviewed: NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
Level 13, 255 Pitt Street Sydney, 2001
T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.12: 1958 aerial image of Medlow Bath (Source: NSW Department of Customer Service)



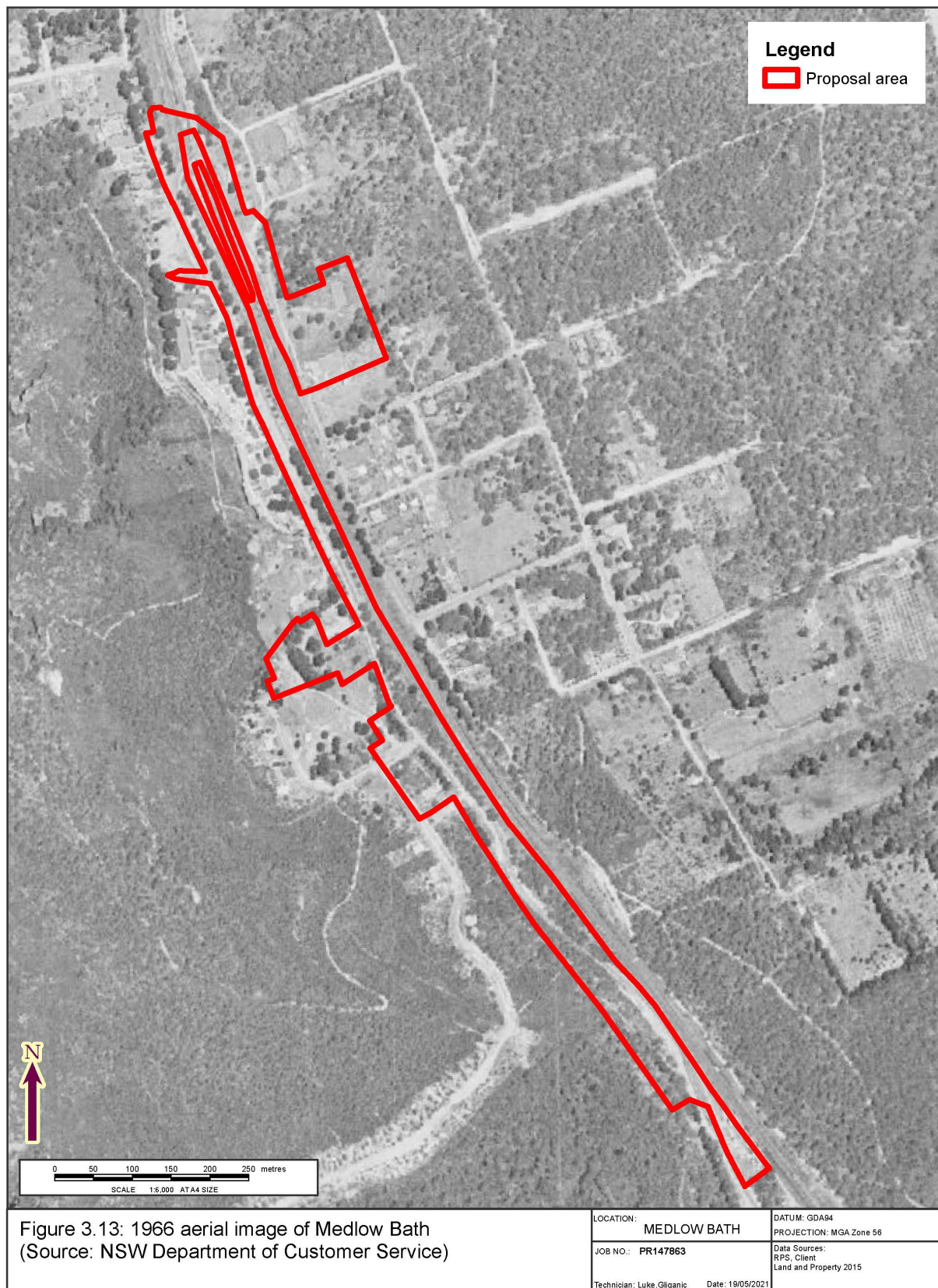
Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.9.mxd
 NSW_A4_Portrait 2018 Rev:B Produced: NW Reviewed: NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T: 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.13: 1966 aerial image of Medlow Bath (Source: NSW Department of Customer Service)



Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.10.mxd
 NSW_A4_Portrait 2018 Rev-B Produced: NW Reviewed: NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T. 02 8270 8300 www.rpsgroup.com.au



REPORT

Figure 3.14: 1994 aerial image of Medlow Bath (Source: NSW Department of Customer Service)



Path: C:\Users\Luke.Gilganić\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig3.11.mxd
NEW_A4_Portrait 2018 Rev-B Produced: RPS Reviewed: N/A Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
Level 13, 255 Pitt Street Sydney, 2001
T: 02 8270 8300 www.rpsgroup.com.au



4 DESCRIPTION AND PHYSICAL EVIDENCE

RPS Senior Heritage Consultant, Sarah van der Linde, inspected the proposal area on 23 December 2020 and 17 May 2021.

This section is based on the inspection and analyses the landscape and setting, existing infrastructure within the proposal area and heritage items within, or immediately adjacent to, the proposal area.

4.1 Landscape setting and features

Set within the Blue Mountains, Medlow Bath is a relatively flat area of land bisected by the Great Western Highway and the Main Western Railway Line. The main township lies on the eastern side of the railway. The Hydro Majestic Hotel occupies most of the western side of the highway, with residential areas north and south. Land on the western side of the highway overlooks Megalong Valley. Medlow Bath Railway Station and associated rail overbridge lie to the north of the township.

Plate 4.1: View north showing Great Western Highway (centre), Main Western Line (right) and Hydro Majestic Hotel (left) (RPS 2020)



REPORT

Plate 4.2: View south along Railway Parade showing Main Western Line (right) and residential area (left) (RPS 2020)



Plate 4.3: View north showing Medlow Bath Railway Station (left) and residential area (right) (RPS 2020)



Plate 4.4: View south across the rail overbridge at the northern end of town, showing Railway Parade (left) and Great Western Highway (right) (RP 2020)



4.2 Proposal area

The eastern section of the proposal area covers Medlow Bath Park and Railway Parade from the park north to the rail overbridge. Medlow Bath Park slopes east towards a drainage channel. It contains manicured lawns, mature plantings, concrete pathways, children's play equipment, tennis court, brick gazebo and associated amenities, including seating, rubbish bins, drinking fountain and toilets. It is surrounded by a post and rail fence (Plate 4.5 to Plate 4.8). Many of the plantings are memorials to former Medlow Bath residents.

Railway Parade is a local asphalted access road, with off street parking areas (Plate 4.9). *St Luke's Anglican Church* (Blue Mountains LEP Item MB010) (Plate 4.10 and Plate 4.11) and *Horse Trough* (Blue Mountains LEP Item MB013) (Plate 4.12) are both located on Railway Parade south of Medlow Bath Park outside of the proposal area. At the northern end of Railway Parade opposite Medlow Bath Railway Station is a T-intersection with Park Street (Plate 4.13 and Plate 4.14). *Urunga* (Blue Mountains LEP Item MB017), the former station masters cottages and now a private residence, lies on the north east corner of this intersection facing Park Street (Plate 4.15 and Plate 4.16). East of *Urunga* at 6-8 Park Street lies *House* (Blue Mountains LEP Item MB018), which as a late Victorian bullnose cottage is a surviving residence of old Medlow before the Hydro made it Medlow Bath (Plate 4.17). North of *Urunga* on Railway Parade is the *Former Post and Telegraph Store* (Blue Mountains LEP Item MB08), which has been converted into a café (Plate 4.18 and Plate 4.19). Railway Parade inclines toward the rail overbridge, which is marked by a cross intersection with the Great Western Highway and Station Street. A pedestrian pathway from Railway Parade leads to Medlow Bath Railway Station (Plate 4.20 to Plate 4.23).

Medlow Bath Railway Station (SHR No.01190, S170 Reference No. 4801011, Blue Mountains LEP Item MB003) can be accessed from both the north and south. The 1901 pedestrian footbridge across the railway cutting at the northern end provides access via stairs to the single island platform. Access at the southern end of the station is via a ramp from a pedestrian level crossing linking the Great Western Highway to Railway Parade. The 1902 main six bay brick platform building is situated toward the centre of the platform, with the small 1922 weatherboard signal room building immediately north. Garden beds with manicured plantings occur along the platform, however these have been heavily modified over the years and differ from the first garden beds established on the platform. The 1902 platform has been extended and resurfaced, with

REPORT

tactile indicators added along the platform edges. The footbridge stairs have been upgraded with new treads, anti-slip guards and hand rails. Opal totems, user information panels, wayfinding signage, seating and additional lighting have also been installed across the station, with some historic light posts still present (Plate 4.24 to Plate 4.34).

Station Street is situated in the north east section of the proposal area. It can be accessed via a one way, single lane slip lane from Great Western Highway or at the traffic light cross intersection with Great Western Highway and Railway Parade on the rail overpass. *Melbourne House*, *Cosy Cot*, *Sheleagh Cottage* (Blue Mountains LEP MB019) are four houses and a vacant block of land located on Station Street between the slip lane entrance and the cross intersection (Plate 4.35 to Plate 4.40). *Melbourne House*, *Cosy Cot*, *Sheleagh Cottage* is located immediately adjacent to the proposal area.

Great Western Highway is characterised as an asphalted road way with a single lane each direction. The wide reserve along its western flank comprises grassed and vegetated areas in the south of the proposal area (Plate 4.41 and Plate 4.42). Between Bellevue Crescent and Foy Avenue, an access track runs between the vegetated area and residential allotments to the west on Delmonte Avenue north to a petrol station, where it joins a formal concrete footpath outside the *Hydro Majestic* (Plate 4.43 to Plate 4.46). Immediately south and east of the petrol station lies vacant land which forms part of the Hydro Majestic estate and forms part of *Hydro Majestic* (Blue Mountains LEP Item MB002) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP Item MB026). The block fronting the Great Western Highway has been cleared of vegetation and shows evidence of a former structure (Plate 4.47 to Plate 4.50). The adjoining block to the rear contains a number of mature pine plantings and appears to currently be used as an informal storage area and rubbish dump (Plate 4.51 to Plate 4.55), with a number of potential moveable cultural heritage items presumably associated with the Hydro Majestic stored there (Plate 4.56 and Plate 4.57). The area does not show evidence of formal walking tracks associated with *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)*.

Through the township, the wide western verge is of gravel and asphalt and acts as parking. A concrete footpath and underground utility services run along the western side of the highway between the road and the *Hydro Majestic* (Blue Mountains LEP Item MB002). The boundary of the *Hydro Majestic* is marked by its stone fence and flag poles and lies within the western edge of the proposal area (Plate 4.58 to Plate 4.61). To the east, the area between the highway and railway line is an undeveloped grassed area on which *Avenue of trees (formerly Avenue of Radiata Pines)* (Blue Mountains LEP Item MB015) is planted. All the original pines here have been removed and replaced (Plate 4.62 and Plate 4.63). A concrete footpath runs between the two station entrances. A pedestrian refuge crossing across the highway is situated at the southern entrance to the railway station (Plate 4.64). North of the *Hydro Majestic* is a residential property, followed by a vacant block of land and then a car sales yard, which extends to Station Street (Plate 4.65 to Plate 4.67).



Plate 4.6: Pathway in Medlow Bath Park on lower slope near drainage channel, looking east (RPS 2020)



Plate 4.7: View west across Medlow Bath Park showing children's play equipment (RPS 2020)



Plate 4.8: View south west across Medlow Bath Park showing Hydro Majestic in background (RPS 2020)



REPORT

Plate 4.9: View north along Railway Parade showing Medlow Bath Railway Station (left) (RPS 2020)



Plate 4.10: [Former] St Luke's Anglican Church (Blue Mountains LEP Item BM010), looking east (RPS 2020)



REPORT

Plate 4.11: [Former] St Luke's Anglican Church (Blue Mountains LEP Item BM010), looking east (RPS 2020)



Plate 4.12: Horse Trough (Blue Mountains LEP Item MB013), looking west (RPS 2020)





Plate 4.14: View north along Railway Parade from Park Street showing *Medlow Bath Railway Station* (left) and *Urunga* (right) (RPS 2020)



REPORT

Plate 4.15: Front façade of *Urunga* (Blue Mountains LEP Item MB017) as seen from Park Street (RPS 2020)



Plate 4.16: Rear of *Urunga* (Blue Mountains LEP Item MB017) (RPS 2020)





Plate 4.18: Front façade of *Former Post and Telegraph Store* (Blue Mountains LEP Item MB008) as seen from Railway Parade (RPS 2020)





Plate 4.20: Northern end of Railway Parade showing station entrance (left) and rail overpass cross intersection (RPS 2020)



Plate 4.21: View north across rail overpass cross intersection from Railway Parade (RPS 2020)



Plate 4.22: View south west across Great Western Highway from rail overpass cross intersection (RPS 2020)



REPORT

Plate 4.23: View west across Great Western Highway to Station Street from rail overpass cross intersection (RPS 2020)



Plate 4.24: View south west of rail cutting at northern end of *Medlow Bath Railway Station* (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003), showing pedestrian overbridge (RPS 2020)



REPORT

Plate 4.25: View south across *Medlow Bath Railway Station* (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003) from pedestrian overbridge (RPS 2020)



Plate 4.26: View south east of *Medlow Bath Railway Station* (SHR No. 01190, S170 Ref No. 4801011, Blue Mountains LEP Item MB003) buildings from Great Western Highway (RPS 2020)



Plate 4.27: View south along Platform 1 showing platform buildings (RPS 2020)



Plate 4.28: View north east of weatherboard signal room (RPS 2020)



Plate 4.29: South façade of main platform building (RPS 2020)



Plate 4.30: View north east from Platform 1 showing *Former Post and Telegraph Store* (Blue Mountains LEP Item MB008) (left) and *Urunga* (Blue Mountains LEP Item MB017)(right) (RPS 2020)



Plate 4.31: View south along Platform 1 (RPS 2020)



Plate 4.32: View west from platform across Great Western Highway to vacant block of land (RPS 2020)



REPORT

Plate 4.33: View south along platform toward southern station entrance, showing *Hydro Majestic* (Blue Mountains LEP Item MB002) (right) (RPS 2020)



Plate 4.34: Southern level crossing station entrance looking east (RPS 2020)



REPORT

Plate 4.35: View west of shop and residence at Lot 1 Great Western Highway, part of *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP Item MB019) (RPS 2020)



Plate 4.36: View north along Station Street showing *Melbourne House* (Blue Mountains LEP Item MB019) (RPS 2020)



REPORT

Plate 4.37: Melbourne House (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020)



Plate 4.38: Cosy Cot (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020)



REPORT

Plate 4.39: *Sheleagh Cottage* (Blue Mountains LEP Item MB019) viewed from Station Street (RPS 2020)



Plate 4.40: View south along Station Street showing rail overpass cross intersection (left) (RPS 2020)



REPORT

Plate 4.41: View north along Great Western Highway from Foy Avenue at the southern end of the proposal area (RPS 2020)



Plate 4.42: View east across Great Western Highway showing railway compound on eastern side of highway (RPS 2020)



REPORT

Plate 4.43: View south of western vegetated road reserve embankment between Bellevue Crescent and Foy Avenue (RPS 2020)



Plate 4.44: View east along Bellevue Crescent to intersection with Great Western Highway (RPS 2020)



REPORT

Plate 4.45: View south along Delmonte Avenue (RPS 2020)



Plate 4.46: View south along Great Western Highway showing road reserve south of *Hydro Majestic* (RPS 2020)



REPORT

Plate 4.47: View south across vacant lot immediately south of petrol station, showing highway (left) (RPS 2021)



Plate 4.48: View north west across vacant lot toward petrol station (RPS 2021)



Plate 4.49: View west across vacant lot showing stands of mature pines to rear (RPS 2021)



Plate 4.50: Detail of sandstone block identified in vacant lot fronting Great Western Highway (RPS 2021)



REPORT

Plate 4.51: View south across vacant lot to rear of petrol station and Hydro Majestic car park, showing stands of mature pines (RPS 2021)



Plate 4.52: View west across vacant lot to rear of petrol station (RPS 2021)



REPORT

Plate 4.53: View south across vacant lot to rear of petrol station showing rubbish and adjoining residential property (RPS 2021)



Plate 4.54: View west across vacant lot to rear of petrol station showing gate to Bellevue Crescent (RPS 2021)





Plate 4.56: Detail of potential moveable cultural heritage located within vacant lot behind petrol station (RPS 2021)



REPORT

Plate 4.57: Detail of potential moveable cultural heritage located within vacant lot behind petrol station (RPS 2021)



Plate 4.58: View north along Great Western Highway showing *Hydro Majestic* (Blue Mountains LEP Item MB002) (left) and railway line (right) (RPS 2020)



REPORT

Plate 4.59: View north along Great Western Highway showing *Hydro Majestic* (Blue Mountains LEP Item MB002) and its stone fence (left) and railway line (right) (RPS 2020)



Plate 4.60: View south along western road reserve, showing footpath and underground utilities running beside *Hydro Majestic* (Blue Mountains LEP Item MB002) stone fence (right) (RPS 2020)



REPORT

Plate 4.61: View west across Great Western Highway to *Hydro Majestic* (Blue Mountains LEP Item MB002) (RPS 2020)



Plate 4.62: View north along Great Western Highway showing *Avenue of trees* (Blue Mountains LEP Item MB015) (right) on land between highway and railway (RPS 2020)



Plate 4.63: View north of Avenue of trees (Blue Mountains LEP Item MB015) (RPS 2020)



Plate 4.64: View west of pedestrian refuge crossing across the Great Western Highway from the southern entrance of Medlow Bath Railway Station (RPS 2020)



Plate 4.65: View south along Great Western Highway showing vacant block of land (right)



Plate 4.66: View south of vacant block of land, showing extant weatherboard structure (RPS 2020)





4.3 Potential Heritage Items

Three potential heritage items were identified during the site inspection:

- Bus shelter
- Sandstone railway culvert
- Hydro Majestic sign

The location of each potential heritage item in relation to the proposal area is show in Figure 4.1.

4.3.1 Bus shelter

The concrete bus shelter is located at the bus stop near the southern entrance to Medlow Bath Railway Station. The bus shelter has been painted inside and out with murals. The interior boasts a mural of an historic view of the Hydro Majestic Hotel and Great Western Road, while the exterior is decorated with foliage motifs and naturalistic scenes (Plate 4.68 to Plate 4.72).

The concrete form of the bus shelter is standard across the Blue Mountains and its murals form part of a wider aesthetic campaign of bus shelter murals across the Blue Mountains. The artwork appears to be unsigned and therefore cannot be attributed to a particular artist.

REPORT

Plate 4.68: Bus shelter looking north, showing interior mural (RPS 2020)



Plate 4.69: Bus shelter interior detail (RPS 2020)



Plate 4.70: Bus shelter interior detail (RPS 2020)



Plate 4.71: Bus shelter looking south, showing exterior design (RPS 2020)





4.3.2 Sandstone railway culvert

The sandstone culvert runs beneath the railway embankment south of Medlow Bath Railway Station, within the proposal area. Visible on both sides of the embankment, the culvert has been recently modified through the addition of a plastic pipe (Plate 4.73 to Plate 4.77). A drain linked to the stone culvert appears to run beneath the Great Western Highway (Plate 4.74). While the section of drain immediately adjacent to the stone culvert was unable to be inspected, as the area falls within the rail corridor, what was visible of the drain and surface infrastructure visible on the Hydro Majestic side of the highway indicates the drain itself to be more modern in design (Plate 4.75).

Plate 4.73: Sandstone culvert on western side of railway embankment, looking east (RPS 2020)



Plate 4.74: Sandstone culvert on western side of railway embankment showing proximity to Great Western Highway and drain running beneath highway (right), looking south (RPS 2020)



REPORT

Plate 4.75: Location of sandstone culvert within rail corridor adjacent to Great Western Highway showing modern drain access (right), looking south east (RPS 2020)



Plate 4.76: Sandstone culvert on eastern side of railway embankment, looking west (RPS 2020)



REPORT

Plate 4.77: Sandstone culvert on eastern side of railway embankment, looking west from Railway Parade (RPS 2020)



4.3.3 Advertising sign

The timber sign advertising the “Majestic Lounge and Public Bar” is located on the western road embankment of the Great Western Highway in the southern section of the proposal area. Overgrown with roadside vegetation, the sign is in a dilapidated condition (Plate 4.78 and Plate 4.79).

Plate 4.78: Rear of Advertising sign, looking east (RPS 2020)



Plate 4.79: Front of Advertising sign, looking north (RPS 2020)



Figure 4.1: Location of potential heritage items in relation to the proposal area

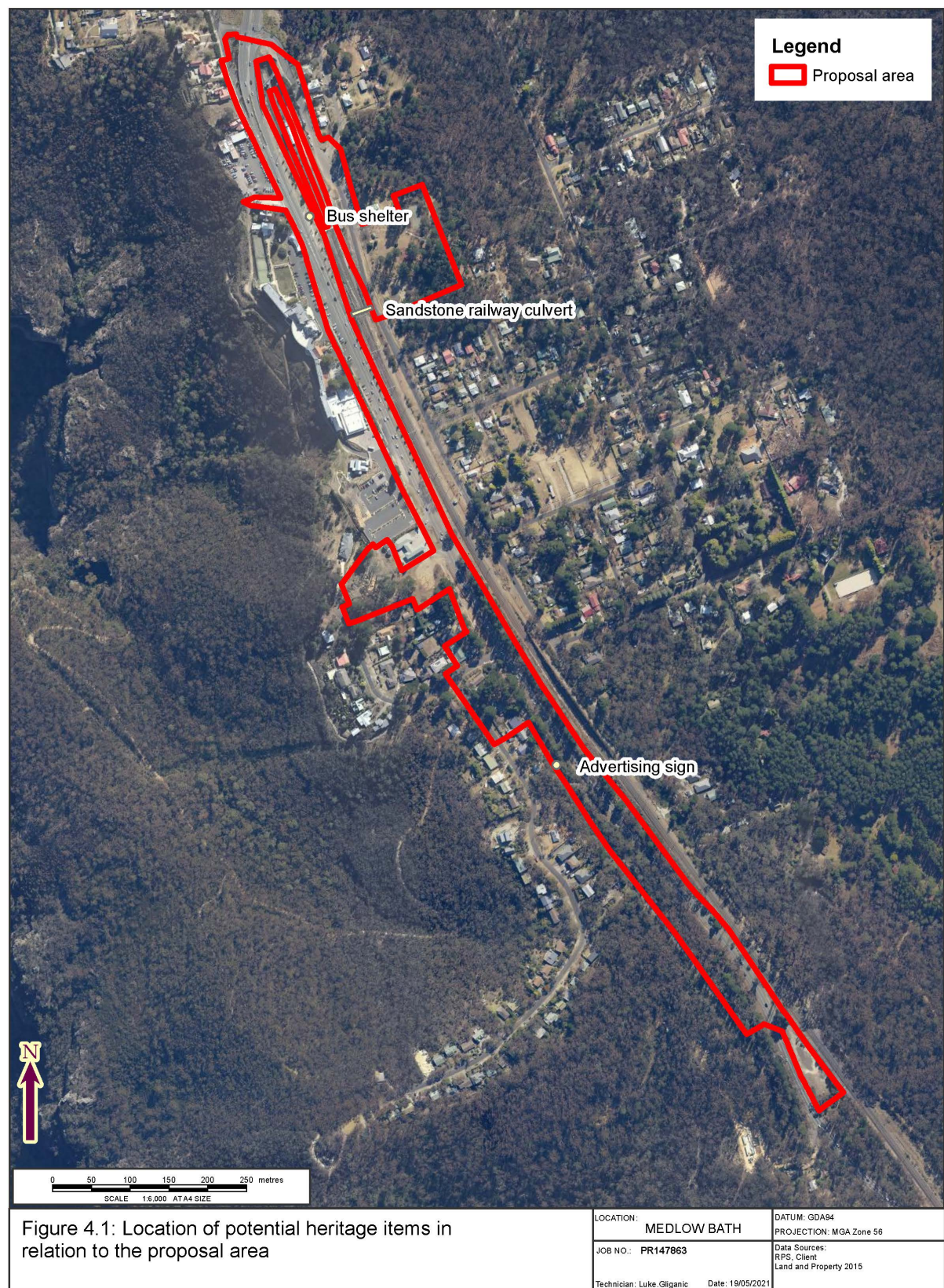


Figure 4.1: Location of potential heritage items in relation to the proposal area

5 ASSESSMENT OF ARCHAEOLOGICAL POTENTIAL

The assessment of archaeological potential assesses the potential for archaeological resources associated with earlier phases of occupation, activity or development within the proposal area. It is based on an understanding of the history of Medlow Bath, an analysis of documentary resources and the proposal area, including an analysis of the level of the ground disturbance and the associated impact on archaeological potential.

The assessment of archaeological potential is an assessment of the proposal area only. The archaeological potential of the proposal area is assessed and graded according to the definitions in Table 5.1.

Table 5.1 Levels of archaeological potential

| Level | Definition |
|----------|---|
| High | The history indicates that archaeological resources are likely to be identified. Ground disturbance is limited, and archaeological resources are likely to be intact. |
| Moderate | The history or the level of ground disturbance indicates that archaeological resources may be identified. If identified, archaeological resources may be affected or truncated due to ground disturbance. |
| Low | The history or the level of ground disturbance indicates that it is unlikely that archaeological resources would be identified. If identified, any archaeological resources are unlikely to be intact. |

5.1 Analysis of documentary resources and the proposal area

5.1.1 Roads

Completed in January 1815, the initial road through the Blue Mountains to Bathurst followed the Blaxland, Lawson and Wentworth route. Little more than a dirt track, the “*Western road from Sydney via Parramatta*” became the principal east-west road. A review of road conditions in 1865 noted that the proposal area section of the road between 18 Mile Hollow (Woodford) and Hartley had been ballasted (Department of Main Roads NSW 1976: 15, 47). Given the effort of ballasting the road just prior to the opening of the railway, it is unclear if minor alterations to the alignment of the Great Western Road through Medlow Bath were required for the construction of the railway, or its duplication in 1902.

Bituminous sealing of the road commenced immediately after it was proclaimed as the Great Western Highway, State Highway No. 5, in 1928 and was completed by 1939. Reconstruction of the Great Western Highway commenced in 1979. No major alterations to the road occurred through Medlow Bath and the proposal area until the early 2000s when a new four lane railway bridge was constructed and the road alignment improved over the railway line (Ozroads). South of the railway bridge, it seems the present road alignment through the proposal area primarily follows that of the original 1815 alignment.

Parish maps and land sales adverts dating to 1890 indicated that Railway Parade on the eastern side of the railway has been identified as a formal road since the establishment of Medlow Bath (Figure 3.1, Figure 3.2 and Figure 3.4). Throughout the development of the township, Railway Parade was the main access road for the residential subdivision of land on the eastern side of the railway. Its alignment does not appear to have altered much over the years, however it is possible that its alignment may have been shifted east slightly to accommodate duplication of the railway in 1902. A 1930s aerial images of the township (Plate 3.14 and Plate 3.15) clearly show Railway Parade as a formal road on what appears to be its present alignment along the eastern side of the railway line. It is unclear when the road was first sealed with bitumen but most likely in the mid twentieth century.

Both the Great Western Highway and Railway Parade appear to have maintained a relatively consistent alignment throughout proposal area since they were initially laid out. The level to which former road surfaces have been disturbed through the construction of the present road surface is unclear. Elsewhere in the Great Western Highway upgrading works across different parts of the Blue Mountains, sections of former road structure have been found immediately below the current road surfaces. Substantial but intermittent lengths

REPORT

of the alignment identified were Telford paved and comprised tight packed squarish blocks of sandstone. What survives is usually the base course, however, occasionally the actual surface survives capped by later road surfacing. Resultantly, evidence of former road surfaces may be present beneath the Great Western Highway from Foy Avenue to the railway overpass and along the length of Railway Parade.

5.1.2 Medlow Bath Railway Station

Medlow Bath Railway Station (SHR No.01190, Blue Mountains LEP Item No. MB003) originally comprised a 30 metre long wooden platform and associated structures built in 1881, situated on the country side of the single line (Plate 3.18). Additions to the station buildings were made in 1899. A pedestrian footbridge at the northern end of the platform was added in 1901. Duplication of the line in 1902 resulted in the replacement of the original platform with the present platform and the construction of the existing station building on the platform (Heritage NSW 2000a). The subsurface interior of the platform likely comprises fill, potentially made up from demolition material from the former platform structure. No plans of the platform construction have been identified. Early images of the island platform indicate that it had a gravelled surface with concrete deck (Plate 3.19 and Plate 3.20).

North of the 1902 station building, is a small timber signal box. Originally an open interlocking frame erected in 1909, it was covered in to create the existing signal box in around 1922. An additional siding was built in 1926. Plans from around this time indicate a waiting shed once stood at the Sydney end of the platform as well as a goods shed and associated platform on the highway side of the station. The platform was extended at the Sydney end in 1942 to bring the overall length of the platform to 600 feet (182.88 metres). Plans from this extension (Figure 3.10) and aerial imagery from 1943 (Figure 3.11) indicate that the goods shed on the highway side of the station was still present after the platform was extended but the former waiting shed had been demolished to accommodate the platform extension. The plans also indicate that the platform wall was also raised and presumably included modifications to the coping (AECOM 2017, 16; Heritage NSW 2000a).

The 1902 island platform runs north-south, with the buildings located slightly to the north. The platform is brick faced with a concrete deck and asphalt finish. Two raised round shaped slabs are located to the south of the station building probably for access to the services below the platform (Heritage NSW 2020a). Garden beds along the platform have existed in some form since the construction of the existing platform building (Plate 3.19 to Plate 3.22). No other structures appear to have existed on the platform. The level crossing at the southern end of the station was added after the platform was extended in 1942. It is unknown when the goods shed was demolished.

Overall, the station is largely intact.

5.1.3 Medlow Bath township

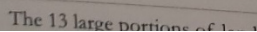
5.1.3.1 Vacant land at westbound end of proposal area

An analysis of aerial images of the township indicate that the land covering the undeveloped, vacant block of land on the western side of the Great Western Highway at the northern (westbound) end of the proposal area has remained undeveloped since the initial subdivision of Medlow Bath. An aerial image of the township dating to the 1930s (Plate 3.15) shows the land undeveloped. It further indicates that the buildings immediately south of the vacant lot seem to have been replaced over time but appear to occupy a similar footprint to the former buildings. Land north of the vacant lot, appears to have only been developed in recent years (Figure 3.11 to Figure 3.14). Given the apparent lack of development on this block, there is a **low likelihood** for archaeological resources to be present.

5.1.3.2 Vacant land at eastbound end of proposal area immediately south of Hydro Majestic

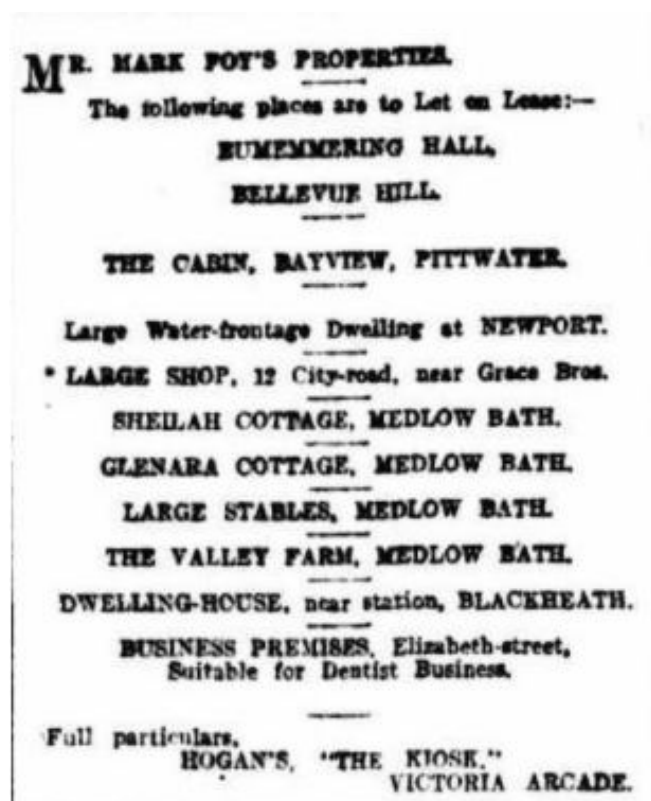
An aerial image (Plate 3.14) of Medlow Bath from the 1930s shows the land immediately south and west of the petrol station at the southern (eastbound) end of the proposal area once contained a house surrounded on three sides by stands of pine trees. Smith (2020, 28, 92) notes that Portion 36, on which this area of land is situated was called Glenara Park by Foy and contained Glenara Cottage. It appears Foy leased the property from 1912 until he managed to purchase it in 1927 (Plate 5.1 and Plate 5.2). It is unknown when the house was built or when it was destroyed, however, given the presence of sandstone blocks and stands of mature pines, there is a **moderate to high likelihood** for archaeological resources and deposits associated with the former Glenara Cottage to the present within the proposal area.

Plate 5.1: Land owned by Mark Foy (Source: Smith 2020, 28)



REPORT

Plate 5.2: Advertisement for the lease of Mark Foy's Glenara Cottage ("Advertising" Sydney Morning Herald, 29 April 1914, p.3)



5.1.3.3 Medlow Bath Park

Documentary sources for Medlow Bath Park show that part of the land was a sawmill before being reserved for public purposes. A land sales advert for Belgravia Estate (Figure 3.4) dated January 1890 shows the eastern side of the railway and road is marked as 'saw mills'. Kaldy (1983, 67) states that Brown's Saw Mills was located on the sites of the two railway cottages in Railway Parade, opposite the station. A Sawmill is further referenced on the 1890 Parish of Blackheath map, where it is noted that Portion 71 immediately adjacent to Medlow Station has special leases to Messrs Bennett and Smith for Sawmill from 1 January 188[?] to 31 December 1893 and seems to be exempt from the Crown land sales occurring at this time.

The land sales advert for Buckingham Park Estate (Figure 3.5) appears to date from the 1890s and again makes reference to the sawmill by the station as being Smith's Sawmill and shows Medlow Bath Park already reserved for public purpose to the south of the saw mills. The sawmill is no longer referenced on an advert for crown land sales at Katoomba in February 1895 (Figure 3.7), which includes land along the outskirts of Medlow Bath. However, the former sawmill area is still marked as reserved as notified on 8 November 1880 and is surrounded by an area reserved for public purposes, which comprises the present Medlow Bath Park.

The development of Medlow Bath largely occurred after the coming of the railway and the 1890s land sales. However, the undeveloped block of land north of the Hydro Majestic appears to have remained undeveloped since European occupation of the area. Medlow Bath Park was reserved as a public reserve as early as 1890. The park appears to have also remained undeveloped aside from minor landscaping and the addition of its existing recreational facilities. Although part of the land on which Medlow Bath Park sits appears to have been saw mills prior to being reserved as a park, it is unlikely that archaeological resources associated with the saw mill would be present. There is a **low likelihood** for archaeological resources to be present within Medlow Bath Park.

5.2 Assessment of archaeological potential and archaeological research potential

The archaeological potential of the majority of the proposal area is assessed to be low, with an area of moderate to high archaeological sensitivity identified south of the Hydro Majestic (Figure 5.1). The archaeological potential of the proposal area is associated with the potential for the following archaeological resources:

- Low potential evidence of former road surfaces along the Great Western Highway from Foy Avenue to the rail overpass
- Low potential evidence of former road surfaces along Railway Parade
- Low potential evidence of former waiting shed beneath *Medlow Bath Railway Station* platform
- Low to moderate potential evidence of former goods shed and platform to the west of *Medlow Bath Railway Station*
- Moderate to high potential evidence of former house “Glenara Cottage” on vacant land at south (eastbound) end of proposal area immediately south of Hydro Majestic.

If surviving, archaeological resources associated with former road surfaces are most likely to occur immediately beneath the current road surface. Surviving material is likely to be the former road base course, however, it should be noted that the former road surface may be present. If identified, it is likely that any archaeological resources associated with former road surfaces would have been impacted by twentieth century road resurfacing, and would provide negligible research potential unless an unusual road surface features survives.

If surviving, archaeological resources associated with the former waiting shed or goods shed and associated platform would likely be in the form of post holes or brick foundations dependant on the original structural material. If identified, these resources would likely provide limited information and have little to no research potential.

If surviving, archaeological resources associated with the former house would likely comprise building foundations, cess pit or well structures and associated potential occupation deposits. If identified, these resources could provide information about the structure and use of Glenara Cottage, which is relatively unknown. Potential research material would likely be limited to the local historical context.

REPORT

Figure 5.1: Archaeological potential of the proposal area



Path: C:\Users\Luke.Gliganic\Documents\LAG files\arc\PR147863 - Medlow\mxd\Fig5.1.mxd
 NEW_A4_Portrait 2018 Rev: B Produced: NW Reviewed: NW Date: 10/05/2018

RPS AUSTRALIA EAST PTY LTD (ABN 44 140 292 762)
 Level 13, 255 Pitt Street Sydney, 2001
 T: 02 8270 8300 www.rpsgroup.com.au



6 ASSESSMENT OF SIGNIFICANCE

This section provides an assessment of the significance of heritage items located in, or within the immediate vicinity of, the proposal area in accordance with the *Heritage Act* (NSW) 1977 (the Act).

In NSW, significance is assessed against the NSW Heritage Council criteria for assessing cultural and/or natural significance:

- Criterion (a): An item is important in the course, or pattern, of NSW's cultural or natural history (of the cultural or natural history of the local area)
- Criterion (b): An item has strong or special association with the life or work of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area)
- Criterion (c): An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area)
- Criterion (d): An item has strong or special association with a community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons
- Criterion (e): An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area)
- Criterion (f): An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area)
- Criterion (g): An item is important in demonstrating the principal characteristics of a class of NSW's cultural or natural places or cultural or natural environments (or a class of the local area's cultural or natural places or cultural or natural environments).

The Act also distinguishes between items of local and State significance:

- Items of local significance demonstrate historical, cultural, social, archaeological, architectural, natural or aesthetic value of significance to an area
- Items of State significance are of significance to the State in relation to the historical, cultural, social, archaeological, architectural, natural or aesthetic value of the item.

6.1 Established assessment of heritage significance for statutory heritage items

This section summarises information presented on statutory heritage lists. Full heritage listings are included in Appendix A.

6.1.1 Greater Blue Mountains Area (WHL Reference No. 917, NHL Place No. 105999)

6.1.1.1 UNESCO Outstanding Universal Value (OUV) assessment criteria

The cultural and natural significance of *Greater Blue Mountains Area* is established on the WHL. The assessment of significance against the OUV criteria as presented on the WHL is below.

Criterion (ix): The Greater Blue Mountains include outstanding and representative examples in a relatively small area of the evolution and adaptation of the genus *Eucalyptus* and eucalypt-dominated vegetation on the Australian continent. The site contains a wide and balanced representation of eucalypt habitats including wet and dry sclerophyll forests and mallee heathlands, as well as localised swamps, wetlands and grassland. It is a centre of diversification for the Australian scleromorphic flora, including significant aspects of eucalypt evolution and radiation. Representative examples of the dynamic processes in its eucalypt-

REPORT

dominated ecosystems cover the full range of interactions between eucalypts, understorey, fauna, environment and fire. The site includes primitive species of outstanding significance to the evolution of the earth's plant life, such as the highly restricted Wollemi pine (*Wollemia nobilis*) and the Blue Mountains pine (*Pherosphaera fitzgeraldii*). These are examples of ancient, relict species with Gondwanan affinities that have survived past climatic changes and demonstrate the highly unusual juxtaposition of Gondwanan taxa with the diverse scleromorphic flora.

Criterion (x): The site includes an outstanding diversity of habitats and plant communities that support its globally significant species and ecosystem diversity (152 plant families, 484 genera and c. 1,500 species). A significant proportion of the Australian continent's biodiversity, especially its scleromorphic flora, occur in the area. Plant families represented by exceptionally high levels of species diversity here include Myrtaceae (150 species), Fabaceae (149 species), and Proteaceae (77 species). Eucalypts (*Eucalyptus*, *Angophora* and *Corymbia*, all in the family Myrtaceae) which dominate the Australian continent are well represented by more than 90 species (13% of the global total). The genus *Acacia* (in the family Fabaceae) is represented by 64 species. The site includes primitive and relictual species with Gondwanan affinities (*Wollemia*, *Pherosphaera*, *Lomatia*, *Dracophyllum*, *Acrophyllum*, *Podocarpus* and *Atkinsonia*) and supports many plants of conservation significance including 114 endemic species and 177 threatened species.

The diverse plant communities and habitats support more than 400 vertebrate taxa (of which 40 are threatened), comprising some 52 mammal, 63 reptile, over 30 frog and about one third (265 species) of Australia's bird species. Charismatic vertebrates such as the platypus and echidna occur in the area. Although invertebrates are still poorly known, the area supports an estimated 120 butterfly and 4,000 moth species, and a rich cave invertebrate fauna (67 taxa).

Integrity

The seven adjacent national parks and single karst conservation reserve that comprise the GBMA are of sufficient size to protect the biota and ecosystem processes, although the boundary has several anomalies that reduce the effectiveness of its 1 million hectare size. This is explained by historical patterns of clearing and private land ownership that preceded establishment of the parks. However parts of the convoluted boundary reflect topography, such as escarpments that act as barriers to potential adverse impacts from adjoining land. In addition, much of the property is largely protected by adjoining public lands of State Forests and State Conservation Areas. Additional regulatory mechanisms, such as the statutory wilderness designation of 65% of the property, the closed and protected catchment for the Warragamba Dam and additions to the conservation reserves that comprise the area further protect the integrity of the GBMA. Since listing, proposals for a second Sydney airport at Badgerys Creek, adjacent to the GBMA, have been abandoned.

Most of the natural bushland of the GBMA is of high wilderness quality and remains close to pristine. The plant communities and habitats occur almost entirely as an extensive, largely undisturbed matrix almost entirely free of structures, earthworks and other human intervention. Because of its size and connectivity with other protected areas, the area will continue to play a vital role in providing opportunities for adaptation and shifts in range for all native plant and animal species within it, allowing essential ecological processes to continue. The area's integrity depends upon the complexity of its geological structure, geomorphology and water systems, which have created the conditions for the evolution of its outstanding biodiversity and which require the same level of protection.

An understanding of the cultural context of the GBMA is fundamental to the protection of its integrity. Aboriginal people from six language groups, through ongoing practices that reflect both traditional and contemporary presence, continue to have a custodial relationship with the area. Occupation sites and rock art provide physical evidence of the longevity of the strong Aboriginal cultural connections with the land. The conservation of these associations, together with the elements of the property's natural beauty, contributes to its integrity.

6.1.1.2 Established statement of significance

The statement of significance for the WHL item *Greater Blue Mountains Area* as presented on the WHL is:

The Greater Blue Mountains Area consists of 1.03 million ha of sandstone plateaux, escarpments and gorges dominated by temperate eucalypt forest. The site, comprised of eight protected areas, is noted for its representation of the evolutionary adaptation and diversification of the eucalypts in post-Gondwana isolation on the Australian continent. Ninety-one eucalypt taxa occur within the Greater Blue Mountains Area which is also outstanding for its exceptional expression of the structural and

REPORT

ecological diversity of the eucalypts associated with its wide range of habitats. The site provides significant representation of Australia's biodiversity with ten percent of the vascular flora as well as significant numbers of rare or threatened species, including endemic and evolutionary relict species, such as the Wollemi pine, which have persisted in highly-restricted microsites.

6.1.2 Medlow Bath Railway Station Group (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003)

6.1.2.1 NSW heritage significance assessment criteria

The heritage significance of *Medlow Bath Railway Station Group* is established on the SHR. The assessment of significance against the NSW criteria as presented on the SHR is below.

Criteria (a) Historical significance: Medlow Bath Station Group is of historical significance as part of the early construction phase of railway line duplication on the upper Blue Mountains demonstrating the technological and engineering achievements in railway construction at the beginning of the 1900s. It was built in anticipation of a boom period in the mountains, particularly in connection with large holiday resorts such as the nearby Hydro-Majestic Hotel.

Criteria (c) Aesthetic significance: Medlow Bath station building is a good example of a standard design island platform building and demonstrates typical architectural elements of Federation period standard buildings that were built between Penrith and Lithgow following the duplication of the railway line. It maintains its overall architectural quality and setting.

Criteria (d) Social significance: The place has the potential to contribute to the local community's sense of place, and can provide a connection to the local community's past

Criteria (f) Rarity: The timber signal room is a rare example of a separate platform level signal box as the majority of the signal rooms along Blue Mountains Line are incorporated into the main station building.

Criteria (g) Representativeness: The station building is one of the early examples of a large number of standard railway designs that were commonly used in the 1910s-20s after a decade from the construction of Medlow Bath station building. The 1901 superstructure of the footbridge is a typical example of standard steel beam structure supported on trestles and brick abutments with later concrete deck and steps.

Integrity/Intactness: The station building has a high degree of integrity externally, however; the interiors have lost their intactness due to the poor condition. The timber signal room is intact. The footbridge is relatively intact as it retains its original steel superstructure.

6.1.2.2 Established statement of significance

The statement of significance for the SHR item *Medlow Bath Railway Station Group* as presented on the SHR is:

Medlow Bath Railway Station is significant as part of the early construction phase of railway line duplication on the upper Blue Mountains demonstrating the technological and engineering achievements in railway construction at the beginning of the 1900s. Constructed in anticipation of a boom period in the mountains particularly in connection with large holiday resorts such as the Hydro-Majestic Hotel, Medlow Bath station building is a good example of a Federation free classical railway station. The station building demonstrates typical architectural elements of the standard Federation style island platform buildings that were built between Penrith and Lithgow when the line was duplicated.

The statement of significance for the Blue Mountains LEP 2015 item *Medlow Bath Railway Station Group* as presented on the SHI is:

The Medlow Bath Railway Station is one of a group of stations which are associated with the construction and duplication of the railway line across the Blue Mountains.

The station is a representative example of a Federation free classical railway station.

After a review of site conditions, the following is noted in reference to the heritage significance of this item.

Garden beds

REPORT

According to the Office of Rail Heritage (2012) *Conservation Guide: Railway Gardens*, railway gardens were vernacular gardens created and maintained by local railway staff. Railway gardeners worked with the materials they had at hand, and used their own personal creativity to design and maintain railway gardens, resulting in a vast array of garden styles. Traditionally, railway gardens on platforms comprised intensively worked garden beds surrounded by hard edgings. They were laid out in geometric shapes and reflected strong patterning. Hedges were clipped into rigid geometric shapes and occasionally the art of topiary was practised.

The garden beds and associated plantings on the platform are not specifically mentioned in the established statement of significance for *Medlow Bath Railway Station Group* presented on the SHR. They are, however, mentioned as a landscape feature, specifically stating the existing plantings are not significant. While the existing plantings and garden beds are not original, historic images (Plate 3.19 to Plate 3.22) demonstrate that plantings on the platform at *Medlow Bath Railway Station* have been present since the early 1900s and have remained a continuous feature. The plantings contribute to the character of the station and its overall aesthetic significance, and to present have continued to be maintained and cared for.

6.1.3 Hydro Majestic (Blue Mountains LEP 2015 Item No.MB002)

6.1.3.1 NSW heritage significance assessment criteria

The heritage significance of *Hydro Majestic* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: The grandest of the grand hotels in the mountains, the Hydro has state significance as a pioneering spa resort with advanced facilities for the health and pleasure of guests. The century and more of use as a hotel, capitalising on one of the finest situations in the mountains, is also of state significance.

Criteria (c) Aesthetic significance: The Hydro Majestic Hotel is a unique overlay of hotel building styles including the pre-fabricated Casino and Federation free-style Reception buildings and the art deco Hargravia, Belgravia and main wings and the federation free classical south wing. The hotel also includes a number of freestanding buildings with a unity of styling and detailing such as the north bunkhouse, toilet block and rear of the Road Bar.

The arrangement of buildings along the ridge parallel to the Great Western Highway with the distinctive street fencing and row of mature *radiata pinus* trees quickly became, and remains, a significant landmark on the road through the Blue Mountains.

Some individual elements including the Casino and Reception buildings are fine examples of Federation free style architecture.

The tennis courts have a rare quality with their rustic stone walling and location on the edge of the ridge

Criteria (d) Social significance: The unusual feature of a prefabricated imported casino which became a showpiece for some of the greatest singers of the Edwardian period, the art collection and the cuisine further enhance the social significance of the Hydro.

Criteria (e) Research potential: Technical interest attaches to the remains of the flying fox into the Megalong and the symbiosis between the hotel and valley below has remained a significant element in the Hydro's success.

6.1.3.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Hydro Majestic* as presented on the SHI is:

The grandest of the grand hotels in the mountains, the Hydro has state significance as a pioneering spa resort with advanced facilities for the health and pleasure of guests. The century and more of use as a hotel, capitalising on one of the finest situations in the mountains, is also of state significance. The Hydro Majestic Hotel is a unique overlay of hotel building styles including the pre-fabricated Casino and Federation free-style Reception buildings and the art deco Hargravia, Belgravia and main wings and the federation free classical south wing. The hotel also includes a number of freestanding buildings with a unity of styling and detailing such as the north bunkhouse, toilet block and rear of the Road Bar.

REPORT

The arrangement of buildings along the ridge parallel to the Great Western Highway with the distinctive street fencing and row of mature radiata pinus trees quickly became, and remains, a significant landmark on the road through the Blue Mountains.

Some individual elements including the Casino and Reception buildings are fine examples of Federation free style architecture.

The tennis courts have a rare quality with their rustic stone walling and location on the edge of the ridge.

The unusual feature of a prefabricated imported casino which became a showpiece for some of the greatest singers of the Edwardian period, the art collection and the cuisine further enhance the social significance of the Hydro.

Technical interest attaches to the remains of the flying fox into the Megalong and the symbiosis between the hotel and valley below has remained a significant element in the Hydro's success.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.4 Former Post and Telegraph Store (Blue Mountains LEP 2015 Item No.MB008)

6.1.4.1 NSW heritage significance assessment criteria

The heritage significance of *Former Post and Telegraph Store* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: The hall and store have high local significance because of their association with the Hydro Majestic and Mark Foy's touristic entrepreneurship, particularly in catering for the

Criteria (c) Aesthetic significance: The former Post and Telegraph Office and Store is an unusual example of a Federation gothic shopfront. An unusual form for a post office, the building features crenellated parapet, twisted columns and a decorative shield in the centre of the façade.

Criteria (d) Social significance: The hall had a high local profile as a centre for dances, films and, after World War II, a wide variety of Catholic and community functions, while the store and post-office played their usual key role for the residents and visitors alike.

Integrity/Intactness: High

6.1.4.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Former Post and Telegraph Store* as presented on the SHI is:

The hall and store have high local significance because of their association with the Hydro Majestic and Mark Foy's touristic entrepreneurship, particularly in catering for the interest in Jenolan Caves. The long-standing association with motor-cars is a particularly significant feature.

It is an unusual example of a Federation gothic shopfront. An unusual form for a post office, the building features crenellated parapet, twisted columns and a decorative shield in the centre of the façade.

The hall had a high local profile as a centre for dances, films and, after World War II, a wide variety of Catholic and community functions, while the store and post-office played their usual key role for the residents and visitors alike

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.5 St Luke's Anglican Church (Blue Mountains LEP 2015 Item No.MB010)

6.1.5.1 NSW heritage significance assessment criteria

The heritage significance of *St Luke's Anglican Church* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (c) Aesthetic significance: St Luke's Church of England, Medlow Bath is a representative example of a Federation carpenter gothic church built for a small rural village.

Criteria (d) Social significance: The church has had social significance for the Anglican community around Medlow Bath for over eighty years.

Integrity/Intactness: Intact

6.1.5.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *St Luke's Anglican Church* as presented on the SHI is:

The church has had social significance for the Anglican community around Medlow Bath for over eighty years. It is a representative example of a Federation carpenter gothic church built for a small rural village, although it took its present simpler shape only after storm damage in 1920.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.6 Horse Trough (Blue Mountains LEP 2015 Item No.MB013)

6.1.6.1 NSW heritage significance assessment criteria

The heritage significance of *Horse Trough* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: All the Annis and George Bills horse-troughs have some local significance as evidence of philanthropy towards animals, even though they were erected when the days of the horse on the roads were almost over.

Integrity/Intactness: High

6.1.6.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Horse Trough* as presented on the SHI is:

All the Annis and George Bills horse-troughs have some local significance as evidence of philanthropy towards animals, even though they were erected when the days of the horse on the roads were almost over.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.7 Avenue of Trees (formerly Avenue of Radiata Pines) (Blue Mountains LEP 2015 Item No.MB015)

6.1.7.1 NSW heritage significance assessment criteria

The heritage significance of *Avenue of Trees* is established on the SHI under its former name *Avenue of Radiata Pines*. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: The avenue is an integral part of the significance of the Hydro Majestic, telling testimony to the public image of the resort projected by Mark Foy and continuing today. Viewed as part of the whole, aesthetically and historically, the avenue, like the hotel, has state significance.

REPORT

Criteria (c) Aesthetic significance: The avenue is a highly significant aesthetic feature of Medlow Bath and the Hydro Majestic

6.1.7.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Avenue of Radiata Pines* (now *Avenue of Trees*) as presented on the SHI is:

The avenue is an integral part of the significance of the Hydro Majestic, telling testimony to the public image of the resort projected by Mark Foy and continuing today. Viewed as part of the whole, aesthetically and historically, the avenue, like the hotel, has state significance.

After a review of site conditions, it is noted that the original radiata pine plantings have all been replaced, consistent with the change in name of the item to *Avenue of Trees*.

6.1.8 Urunga (Blue Mountains LEP 2015 Item No.MB017)

6.1.8.1 NSW heritage significance assessment criteria

The heritage significance of *Urunga* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: The association of the house with the railway and the growth of rail traffic at Medlow Bath associated with the Hydro gives the cottage local historical significance.

Integrity/Intactness: Medium

6.1.8.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Urunga* as presented on the SHI is:

The association of the house with the railway and the growth of rail traffic at Medlow Bath associated with the Hydro gives the cottage local historical significance.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.9 House (Blue Mountains LEP 2015 Item No.MB018)

6.1.9.1 NSW heritage significance assessment criteria

The heritage significance of *House* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Criteria (a) Historical significance: Without historical data, all that can be said is that the house in its rural setting is a rare survivor of old Medlow before the Hydro made it Medlow Bath and has therefore some local significance.

Integrity/Intactness: High

6.1.9.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *House* as presented on the SHI is:

Without historical data, all that can be said is that the house in its rural setting is a rare survivor of old Medlow before the Hydro made it Medlow Bath and has therefore some local significance.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.1.10 Melbourne House, Cosy Cot, Sheleagh Cottage (Blue Mountains LEP 2015 Item No.MB019)

6.1.10.1 NSW heritage significance assessment criteria

The heritage significance of *Melbourne House, Cosy Cot, Sheleagh Cottage* has not been established on the SHI. An assessment of significance against the NSW criteria is presented below.

Criteria (a) Historical significance: The group of four houses were all constructed in the first decade of the twentieth century and demonstrate the growth of Medlow Bath at the time.

Criteria (b) Association with significant person(s): Cosy Cot (4 Station Street) was the mountain retreat of Henry Hartley, better known as Hartley Sargent (1878 - 1924), who, with his mother and step-father, ran a famous chain of bakeries and refreshment rooms in New South Wales and Victoria. Hartley's health was impaired by a period as prisoner-of-war after being injured in battle in 1917 and he spent most of his remaining years at Cosy Cot. He died there in 1924 after falling from the cliff edge immediately beyond his own grounds. The house has remained in the hands of the Sargent family.

Sheleagh Cottage (6 Station Street) Mark Foy, founder of the Hydro Majestic, commissioned Sir Herbert Ross to design a summer residence for himself and the house was erected between 1901 and 1903. It was named Shelaugh Cottage after Foy's third daughter.

Criteria (c) Aesthetic significance: The group are an eclectic mix of houses representing the architectural styles of the period. Noted aesthetic characteristics include Melbourne House being construction from iron and Sheleagh Cottage's extensive use of Mahogany and later stone adornments.

Integrity/Intactness (as presented on the SHI):

Lot 1 GWH: Reasonable

Victoria House (2 Station Street): High

Cosy Cot (4 Station Street): High

Sheleagh Cottage (6 Station Street): High

6.1.10.2 Established statement of significance

No statement of significance for the Blue Mountains LEP 2015 item *Melbourne House, Cosy Cot, Sheleagh Cottage* is presented on the SHI.

After historical research and review of site conditions, the following statement of significance is proposed:

The four houses, Lot 1 Great Western Highway, Melbourne House, Cosy Cot and Sheleagh Cottage, are significant as they represent a unique group constructed independently of each other in the early boom years of Medlow Bath. Cosy Cot and Sheleagh Cottage are significant for their association with renowned historical figures. Melbourne House and Sheleagh Cottage are aesthetically significant for their use of particular materials.

6.1.11 Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic) (Blue Mountains LEP 2015 Item No.MB026)

6.1.11.1 NSW heritage significance assessment criteria

The heritage significance of *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* is established on the SHI. The assessment of significance against the NSW criteria as presented on the SHI is below.

Note: This Statement of Significance applies to the whole of the former Hydro Majestic walking track system, not that portion remaining within the present hotel grounds.

REPORT

Criteria (a) Historical significance: The Hydro Majestic Hotel walking track complex has significance as the most extensive privately constructed walking track complex in Australia and for its association with Australia's only hydropathical resort developed on the European model.

Criteria (b) Association with significant person(s): The Hydro Majestic walking track complex has significance for its association with entrepreneur Mark Foy junior who made a fortune in retailing and invested most of it in developing the Hydro Majestic hydropathic resort. The track complex has local significance for its association with William Hargraves, Chief Clerk in Equity of NSW and Blackheath pioneer.

Criteria (c) Aesthetic significance: The Hydro Majestic walking track complex has aesthetic significance at the for the design values in its construction which demonstrate superb integration of natural and constructed features.

Criteria (e) Research potential: As nearly all of the track complex is intact, it offers an opportunity to research late 19th and early 20th century walking track design and construction techniques.

Criteria (f) Rarity: The Hydro Majestic walking track network is a rare example of a very extensive privately constructed walking track complex, mostly over 100 years old, which as survived virtually fully intact to the present time.

6.1.11.2 Established statement of significance

The statement of significance for the Blue Mountains LEP 2015 item *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* as presented on the SHI is:

The Hydro Majestic Hotel walking track complex has State Significance as the most extensive privately constructed walking track complex in Australia and for its association with Australia's only hydropathical resort developed on the European model.

The Hydro Majestic walking track complex has State Significance for its association with entrepreneur Mark Foy junior who made a fortune in retailing and invested most of it in developing the Hydro Majestic hydropathic resort. The track complex has local significance for its association with William Hargraves, Chief Clerk in Equity of NSW and Blackheath pioneer.

The Hydro Majestic walking track complex has aesthetic significance at the State level for the design values in its construction which demonstrate superb integration of natural and constructed features.

As nearly all of the track complex is intact, it offers an opportunity to research late 19th and early 20th century walking track design and construction techniques, significant at the local level.

The Hydro Majestic walking track network is a rare example of a very extensive privately constructed walking track complex, mostly over 100 years old, which as survived virtually fully intact to the present time.

Note: This Statement of Significance applies to the whole of the former Hydro Majestic walking track system, not that portion remaining within the present hotel grounds.

Historical research and review of site conditions confirms this is an accurate assessment of the heritage significance of this item.

6.2 Assessment of significance of potential heritage items

6.2.1 Bus Shelter

6.2.1.1 NSW heritage significance assessment criteria

Criteria (c) Aesthetic significance: The bus shelter is of aesthetic significance for its individual art and its collective aesthetic as it forms part of an extended group of decorated bus shelters throughout the Blue Mountains.

Criteria (d) Social significance: The bus shelter is socially significant to the local community for its historic mural of Medlow Bath and in the wider community as part of a bus shelter mural campaign across the Blue Mountains.

6.2.1.2 Statement of significance

After research and review of site conditions, the following statement of significance is proposed:

The bus shelter is of aesthetic significance for its historic mural and of social significance as it forms part of an extended mural campaign throughout the Blue Mountains.

6.2.2 Sandstone railway culvert

6.2.2.1 NSW heritage significance assessment criteria

Criteria (a) Historical significance: The culvert is historically significant as it is associated with the construction of the railway through the Blue Mountains in the 1860s and its later duplication.

6.2.2.2 Statement of significance

After historical research and review of site conditions, the following statement of significance is proposed:

The culvert is of local significance for its association with railway engineering through the Blue Mountains. The culvert provides physical evidence of the construction of the railway in the 1860s. It provides physical evidence of the original rail alignment and of the workmanship of the period. It is of historical and archaeological significance.

6.2.3 Advertising sign

6.2.3.1 NSW heritage significance assessment criteria

Criteria (a) Historical significance: The *Advertising sign* is linked to the Hydro Majestic and symbolises the prominence of the hotel in Medlow Bath.

6.2.3.2 Statement of significance

After historical research and review of site conditions, the following statement of significance is proposed:

The Advertising sign is significant for its association with the Hydro Majestic.

7 THE PROPOSAL

7.1 Proposal background

The proposal is part of the larger Great Western Highway Upgrade Program between Katoomba to Lithgow. The Medlow Bath section of the Great Western Highway is a primary north – south connection between Katoomba and Blackheath. The Great Western Highway provides one of two mountain crossings between Sydney and Lithgow. The overall project seeks to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic.

7.2 Objectives

7.2.1 Program Objectives (GWHUP)

The Great Western Highway (GWH) is a 201-kilometre highway crossing of the Great Dividing Range through the World Heritage listed Blue Mountains, connecting Bathurst and the surrounding Central West and Orana regions to Sydney.

Crossing the Great Dividing Range, the GWH follows a narrow and difficult alignment constrained by the Blue Mountains National Park, steep topography, a railway line and existing towns for which the highway acts as the main street.






The highway's topography and constrained two lane carriageway design (which in places is almost 200 years old) results in the following constraints:

- reduces freight efficiency by limiting access for safer and more sustainable high productivity vehicles
- limits access during incidents and natural disasters
- slows travel speeds with limited overtaking opportunities and steep gradients (more than double the recommended maximum level)
- causes delays of up to 80 minutes in peak times
- has higher than state average crash rates, and
- impairs amenity for local communities with high through traffic volumes and congestion.

7.2.2 Project Objectives (Medlow Bath)

As part of a staged upgrade program, the Medlow Bath project aims to deliver GWH Upgrade Program objectives.

Table 7.1: Medlow Bath addressing problems and benefits

| Current problem | Program objective | Medlow Bath response |
|---|--|--|
|  1. Economic development, productivity and recovery | Improve ability to drive regional economic development and freight productivity | Provide four lanes with dedicated turn lanes to separate heavy vehicle flow from locally turning traffic |
|  2. Resilience and future proofing | Provide a dependable and adaptable transport network that enables continuity of transport and essential services | Make network provisions for emergency services and provide safe continuous access to transport services |
|  3. Network performance | Improve transport network efficiency | Provide suitable capacity to reduce congestion during peak periods through Medlow Bath |
|  4. Safety | Reduce actual and perceived safety risks | Separate traffic flows and user groups, upgrade intersections and provide safer facilities Remove trees that have reached end of life to address risk of falling trees along the highway and railway corridor |
|  5. Movement, place and amenity | Maintain and enhance local amenity and character, and protect environmental and cultural assets | Improve active transport and local traffic connectivity along and across the corridors. Preserve local heritage assets and enhance local amenity and character through sensitive urban design |

7.2.3 Development criteria

The development criteria for the proposal at Medlow Bath include:

- maintain the functional operation of the highway to traffic and users at all times
- make best use of the defined road corridor between the road rail boundary and the heritage stone wall of the Hydro Majestic to maintain heritage value
- provide four lanes separated by median suitable for 60km/hr at Medlow Bath
- provide a safe all movement intersection for Bellevue Crescent that provides for u-turning traffic impacted by median separation of highway traffic
- provide a shared path for active transport users appropriately linked to the Great Blue Mountains Trail and public transport nodes
- remove the railway level crossing and pedestrian refuge in favour of a physically separated and fully accessible structure to improve rail safety and provide for accessible public transport.
- adopt conditions of approval and/or maintain developer built turning provisions at the Hydro Majestic. Eastbound right turn bay, left in left out plus one vehicle access closure.
- remove failing pine trees that have reached end of life to address the community safety risk of falling trees

REPORT

- provide for trees in the median for as much of the length as possible to maintain the village feel
- adjust, maintain, relocate or reinstate property access to all private properties along the highway frontage
- work to enhance local amenity in developing a design that preserves or reinterprets local heritage values
- adopt water quality control measures to improve the management of stormwater out flows into the drinking catchment.
- no impact to the Greater Blue Mountains World Heritage Area.

7.2.4 Urban design objectives

The urban design vision adopted for the project is:

Within the context of the rugged terrain and bushland setting of the Blue Mountains and the unique natural and cultural landscapes and precincts through which it passes, the Great Western Highway should:

Reinforce the journey sequence of bushland and village; Evoke a sense of its history and heritage; Provide connectivity and permeability for pedestrians; Provide views and a clear sense of orientation for users; Maximise the amenity of the public domain; Create a road design that integrates urban design and engineering.

To achieve this vision, the urban design objectives for the proposal include:

- develop an integrated design that fits with the existing high visual qualities, ecology and character of Medlow Bath and the Blue Mountains setting
- minimise impacts to the integrity of heritage sites, significant trees and cultural values of the community within the proposal
- contribute to the functionality of public spaces and enhance local and regional connectivity.

7.3 Description of proposal

Transport for NSW (Transport for NSW) proposes to upgrade approximately 1.2 kilometres of the Great Western Highway at Medlow Bath between Railway Parade and approximately 330m south of Bellevue Crescent (the proposal). This upgrade is part of the Great Western Highway Duplication project between Katoomba and Lithgow which aims to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic.

In addition to the road modifications, the proposal will also improve active transport links and public transport accessibility.

The proposal is shown in Figure 7.1 to Figure 7.5. Key features of the proposal area shown in Figure 7.6

Key features of the proposal would include:

- construction of a four lane divided carriageway with consolidated access points at upgraded intersections including:
 - upgraded Bellevue Crescent intersection to include three way traffic signals for safe access/egress
 - provision of a U-turn bay for traffic turning east bound to west bound at Bellevue Crescent
 - right turn bay in east bound carriageway median for Hydro Majestic Hotel (no right turn egress)
 - improvements on Railway Parade to formalise parking provisions, U-turns and commuter parking
- construction of full depth highway pavement and associated local road, driveway, footpath, kerb and gutter reconstruction work within the proposal area

REPORT

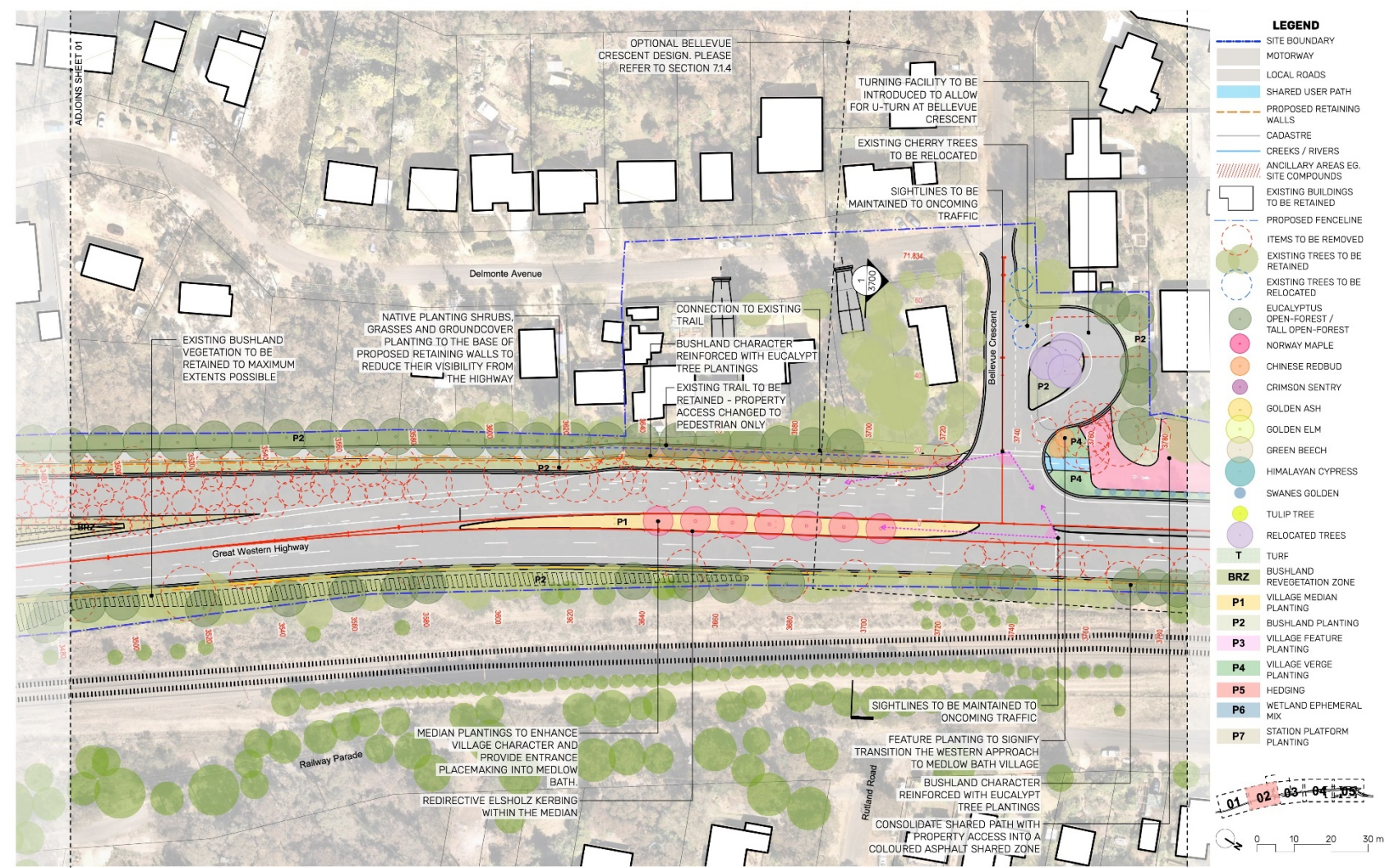
- construction of a new pedestrian bridge that connects Railway Parade, Medlow Bath Station and new indented bus bays on both sides of the Highway in line with Transport Access Program requirements
- shared use (pedestrian/cyclist) path adjacent to westbound carriageway
- retaining wall and traffic barrier construction adjacent to existing rail corridor
- utility relocation and stormwater drainage upgrade as required over length of the project including water quality control measures in Railway Parade
- provision of 6m raised landscaped median for trees protected with modified redirective kerb.

Figure 7.1: The proposal (plan 1 – southernmost section) (Source: Spackman Mossop Michaels 2021)



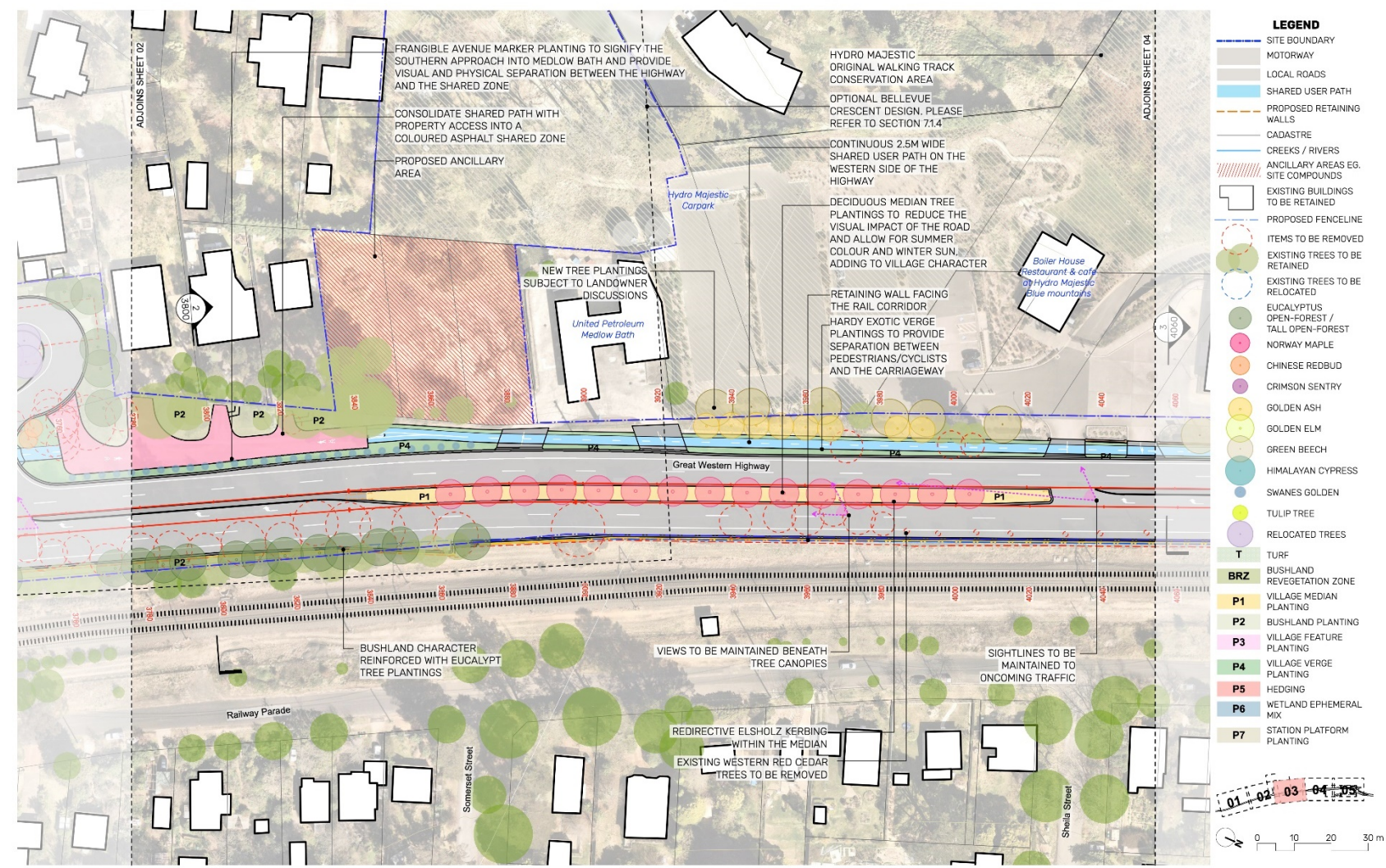
REPORT

Figure 7.2: The proposal (plan 2 – southern section) (Source: Spackman Mossop Michaels 2021)



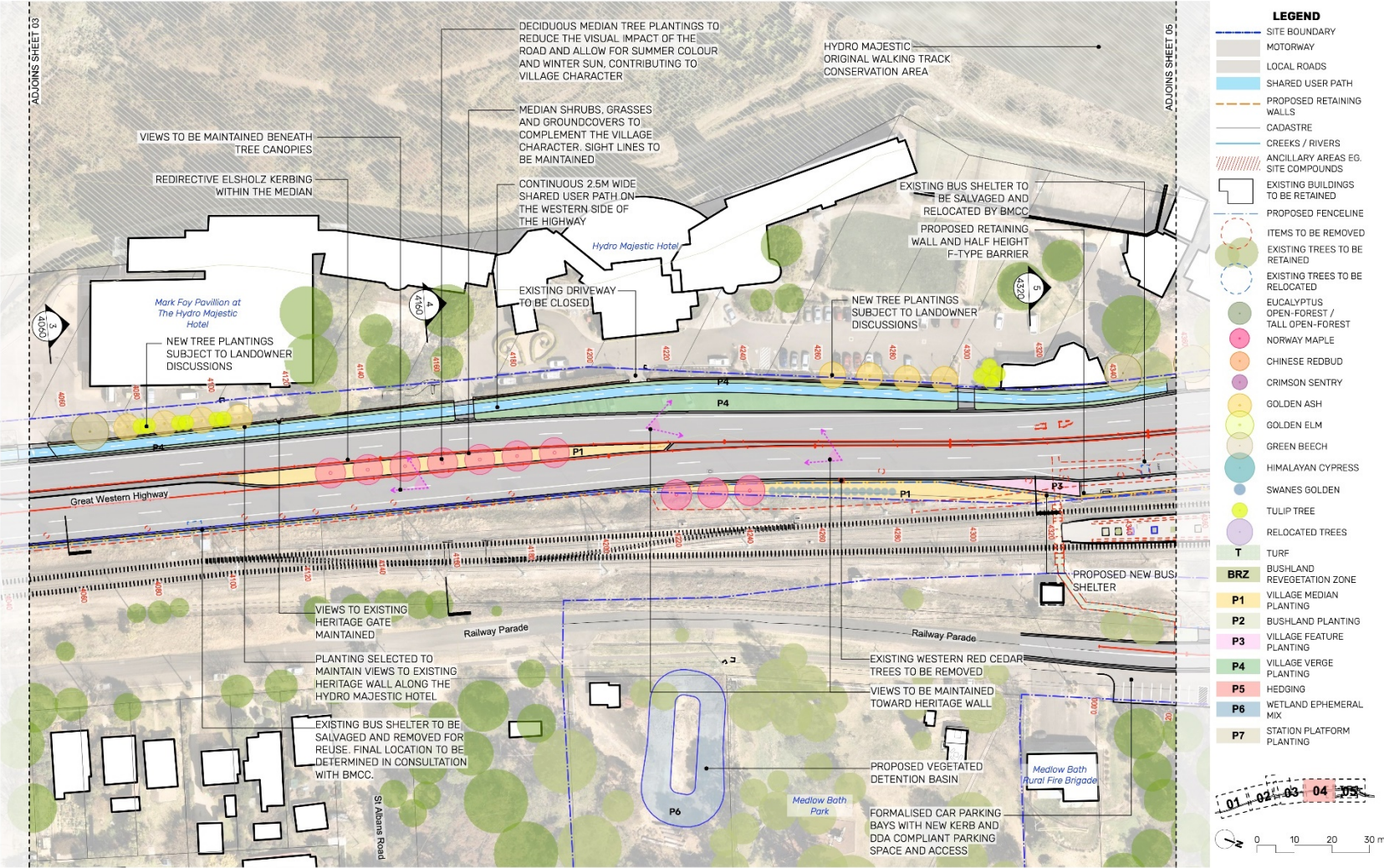
REPORT

Figure 7.3: The proposal (plan 3 – middle section) (Source: Spackman Mossop Michaels 2021)



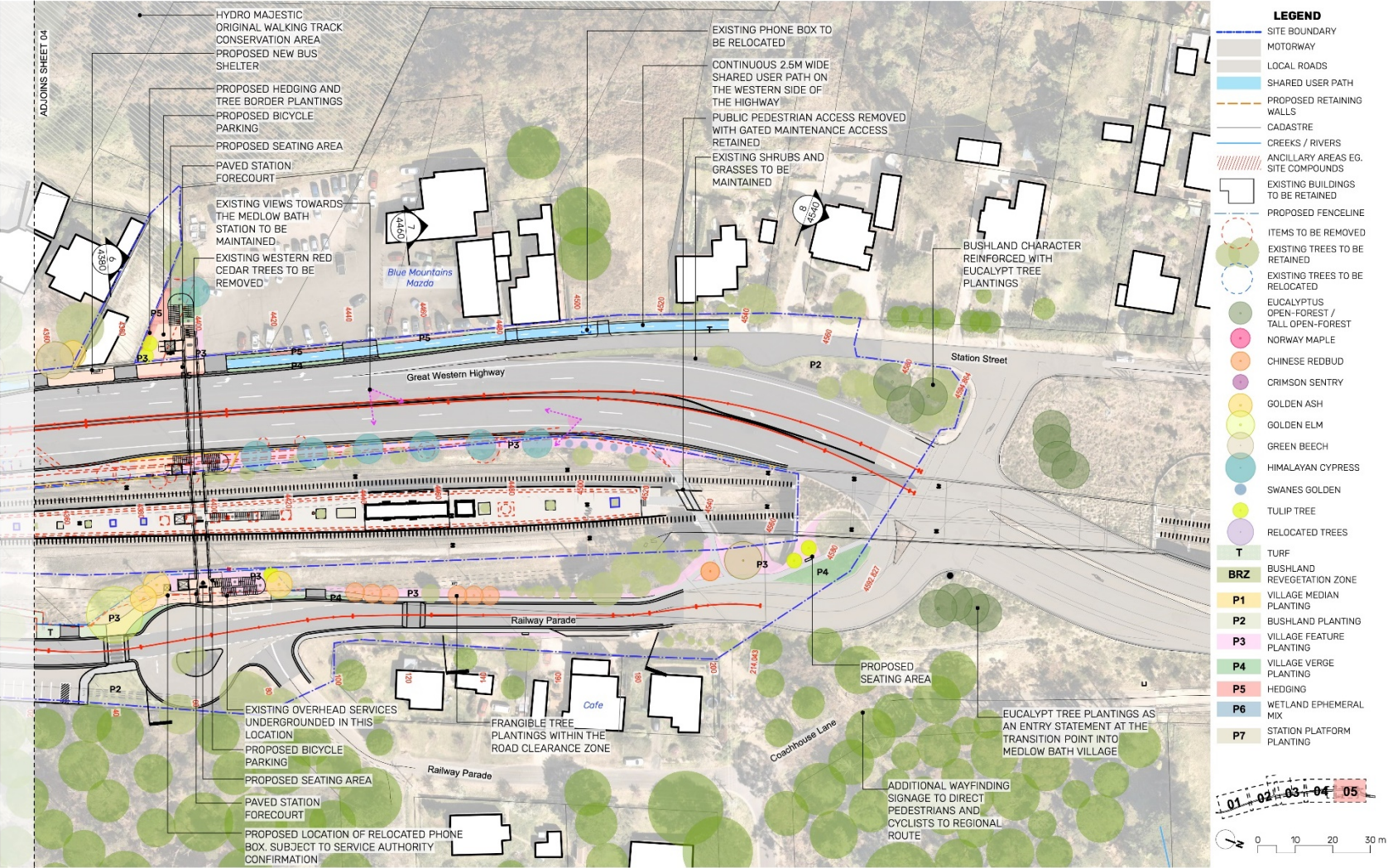
REPORT

Figure 7.4: The proposal (plan 4 – northern section) (Source: Spackman Mossop Michaels 2021)



REPORT

Figure 7.5: The proposal (plan 5 – northernmost section) (Source: Spackman Mossop Michaels 2021)



REPORT

Figure 7.6: Key features of the proposal (Source: Spackman Mossop Michaels 2021)



7.3.1 Proposed alternative intersection at Bellevue Crescent

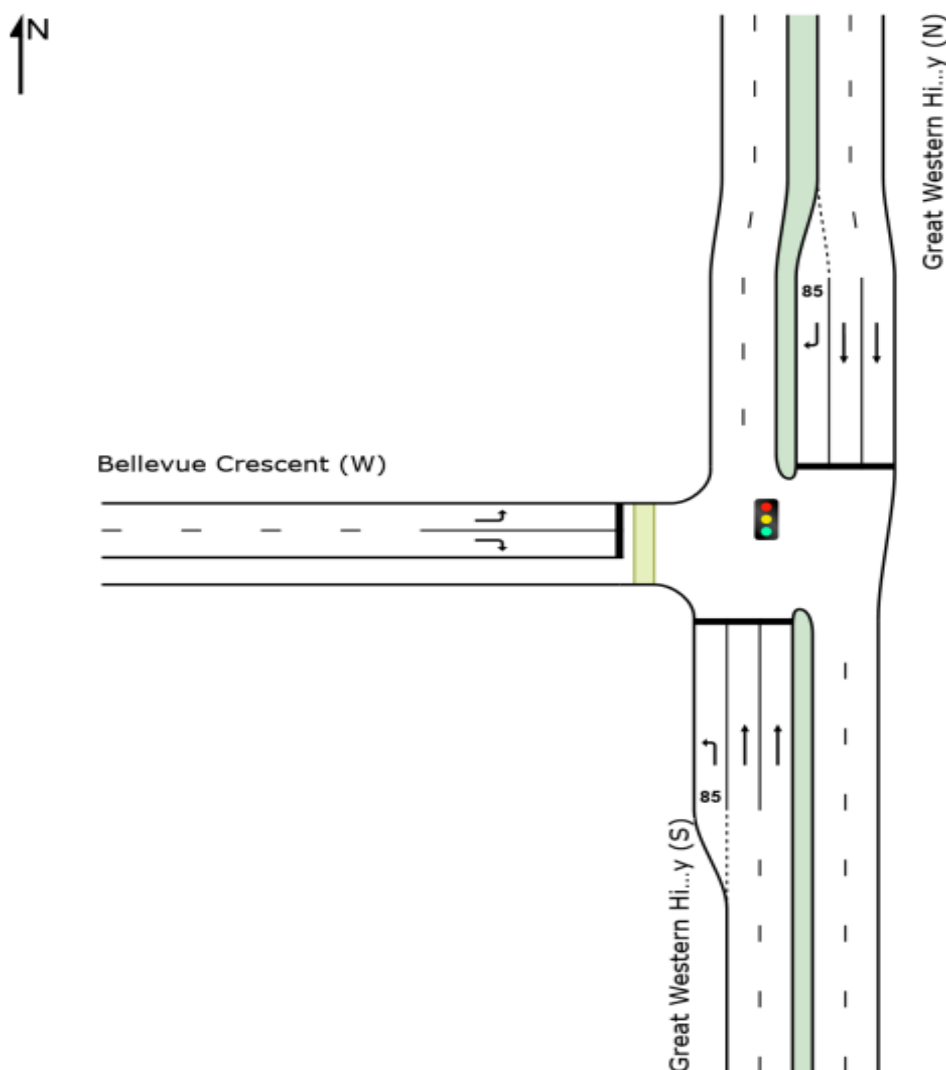
As part of the design for the proposal, a new alternative signalised intersection is being considered to the Great Western Highway with a new road through vacant Lots to connect to the existing Bellevue Crescent and approximately 25 metres south of the United Petrol Station (Figure 7.8).

At the time of writing of this report, an alternative design was being considered for Bellevue Crescent and includes the following key design features:

- a signalised intersection will be built along the southern perimeter of the United Petrol Station in Medlow Bath utilising a corridor (anticipated to be 20 metres) through vacant Lots.
- closing the existing Bellevue Crescent and Great Western Highway intersection but still maintaining a service road/shared zone for the properties fronting the highway
- creating new access options from Bellevue Crescent to the petrol station Hydro Majestic Pavillion
- allows left and right turns out of Bellevue Crescent on to the Great Western Highway (enabling west and east bound movement) and left turn into new Bellevue Crescent from Great Western Highway westbound

Details of this intersection are summarised in Figure 7.7

Figure 7.7: Proposed alternative intersection at Bellevue Crescent



REPORT

Figure 7.8: The proposal (plan 6 – alternate Bellevue Crescent design) (Source: Spackman Mossop Michaels 2021)



7.4 Design

The following sections provide a description of the design criteria, major design features and engineering constraints of the proposal. These features are based on the concept design and would be further refined during detailed design.

7.4.1 Design criteria

The concept design for the proposal was prepared in accordance with the following standards:

- T HR CI 12030 ST Overbridges and Footbridges Design Standard (Transport for NSW, 2020)
- Australian Standards: amended by RMS Supplement (2012)
- Austroads Guide to Road Design (Austroads, 2009) and RMS supplements to the Austroads Guide
- Austroads Road Safety Audit Manual (Austroads, 2009)
- Beyond the Pavement 2020: Urban design approach and procedures for road and maritime infrastructure planning, design and construction (Transport for NSW Centre for Urban Design, 2020)
- NSW Speed Zone Guidelines (Roads and Traffic Authority of NSW, 2011)
- Road Safety Audit Manual and Checklist (Roads and Traffic Authority of NSW, 2011)
- RMS Delineation Manual (2012)
- RMS Road Design Guide (RMS, undated)
- Soils and Construction – Managing Urban Stormwater, Volume 1 (Landcom, 2004) and Volume 2D (Department of Environment and Climate Change, 2008). Guide to Road Design – Austroads (Austroads, 2009).
- Disability Standards for Accessible Public Transport 2002 (DSAPT)

Key design criteria for the proposal are summarised in Table 7.2.

Table 7.2: Key design criteria

| Design features | Requirement |
|---|---|
| Number of lanes | Typical lane arrangement of two lanes in each direction with some turning lanes for access roads off the GWH and to key landmarks. |
| Lane widths | 3.35 metre for through lanes and 3.30 metre for turn lanes (plus lane widening at curves, as required) |
| Design vehicle for main road alignment | Main road alignment - 26 metre B-double. |
| Design vehicle at intersections | <ul style="list-style-type: none"> • Station Street - 12.5 metre 4 axle rigid truck (27 tonnes) • Bellevue Crescent (including U-turn) - Prime mover and semi-trailer (up to 19 metres) |

REPORT

| Design features | Requirement |
|--|---|
| | <ul style="list-style-type: none"> Right hand turn bay into Hydro Majestic Hotel – service vehicle up to 8.8 metres) |
| Posted Speed Limit | <ul style="list-style-type: none"> Main road alignment – 60 km/h Side roads – 50 km/h |
| <ul style="list-style-type: none"> Design Speed | <ul style="list-style-type: none"> Main road alignment – 70 km/h Intersection (at Bellevue Crescent) – 60 km/h Turn in to side roads – 60 km/h |
| Median width | <ul style="list-style-type: none"> Southern portion (at Bellevue Crescent intersection) – 5.10 metres southern approach and 1.8 metres for northern approach to allow for right hand turn bay at signals Mid portion (at Hydro Majestic Hotel) – typically 5.10 metres raised median and 1.80 metres at right hand turn bay into the hotel Northern portion (between Hydro Majestic Hotel and Railway Parade) – 1.8 metres |
| Pavement type | Pavement structure which would consist of asphalt over lean mix concrete and consider acoustic requirements |
| Footpaths/cycle paths and shared zones | <ul style="list-style-type: none"> Southern portion (at Bellevue Crescent intersection) – includes a shared zone for local traffic only (to access 100 to 104 GWH) and pedestrians and is typically 6.7 metres wide Mid portion (at Hydro Majestic Hotel) – 2.5 metre shared path on the western side of the road and pedestrian path from footbridge to bus stop on the eastern side Northern portion (between Hydro Majestic Hotel and Railway Parade) – 2.5 metre shared path on the western side of the road |
| Pedestrian Bridge | To allow safe access to the area, a pedestrian bridge (including stairs and lifts) will be installed to span from Railway Parade to Medlow Bath Station and then across to the western side of GWH (as well as access to the eastern side of GWH to enable use of bus stop serviced by eastbound services). |

REPORT

| Design features | Requirement |
|-----------------------------|--|
| Flood Considerations | Not considered to be within a flood prone area. One in 100 Average Recurrence Interval (ARI) Minor and Major Tributary flood under current climatic conditions. |

7.4.2 Engineering constraints

A number of constraints and performance objectives influenced the development of the proposal design, including the following:

- **Existing Utilities:** the presence of multiple existing underground and above ground public utilities need to be managed. Existing utilities have been identified, and discussions held with the service providers to either relocate utilities within the widened road corridor or protect the assets, while ensuring they can be accessed for scheduled maintenance and emergencies during construction.
- **Operational Traffic:** access to the Great Western Highway needs to be maintained during construction as there are no detour options. In addition, access to side streets (Station Street, Railway Parade and Bellevue Crescent) and driveways along Great Western Highway need to be maintained to ensure access for residents and businesses. The traffic management plan should recognise the requirement for maintaining adjacent access, including emergency access and traffic flow during peak periods.
- **Urban Amenity:** Great Western Highway is one of Australia's most historic roads and the route has largely remained unchanged since its construction in the 1830's. The area has several heritage items within close proximity which notably include the locally listed Hydro Majestic Hotel (located westerly adjacent to the proposal) and the State heritage registered Medlow Bath Station Group (located east of the main road works but still part of the Proposed due to the pedestrian bridge). Medlow Bath is the first built up area east of Katoomba, which needs to retain its village feel as part of an upgraded highway.
- **Improved water quality:** the proposal is within the Sydney Water Catchment area and therefore engineering controls for improved water quality will be designed to ensure a neutral or beneficial effect.
- **Potential contamination within the proposal alignment:** geotechnical investigations and design to manage potential contamination (as detailed within the Phase 1 Investigation which identified potential contamination from the United Petrol Station, the Mazda car dealership, stockpiled ballast and uncontrolled fill material). The presence/absence of these will be identified via a targeted detailed site investigation (Phase 2).

7.4.3 Major design features

7.4.3.1 Upgrade of the Great Western Highway at Medlow Bath

The upgrade of the Great Western Highway from 330 metres south of Bellevue Crescent to the existing bridge at Railway Parade would provide:

- two through lanes in each direction separated by a landscaped central median and a new pedestrian bridge allowing access between Railway Parade, Medlow Bath train station and both sides of the Great Western Highway
- designed at intersection of Great Western Highway and Bellevue Crescent to incorporate:
 - three way traffic signals
 - a right hand turn bay for eastbound traffic from Great Western Highway into Bellevue Crescent
 - a left turn bay for westbound traffic to turn from Great Western Highway into Bellevue Crescent

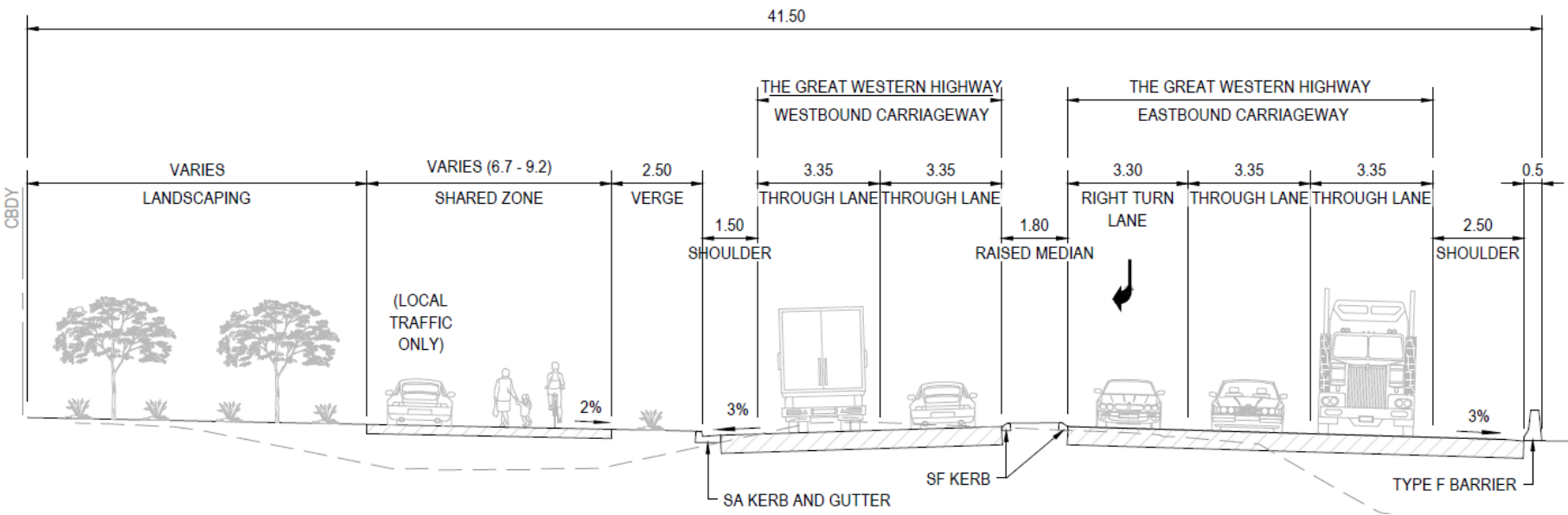
REPORT

- new U turn bay at 106 Great Western Highway
- right hand turn bays integrated into the central medians enabling safe access to the Hydro Majestic Hotel
- a shared path on the western side of Great Western Highway for the extent of the proposal
- kerb and gutters to separate the road traffic from the pedestrians and cyclists
- a new retaining wall between the railway line and the eastbound lanes of the Great Western Highway
- defined commuter area on at Railway Parade adjacent to the station to include:
 - U turn bay
 - raised pedestrian crossings to allow access to eastern side from new footbridge to parking and bus stop as well as to existing cafe
 - formalised parking including define space for mobility parking
 - two kiss and ride locations adjacent to the new pedestrian bridge
- retention of access to Station Street from westbound lane of Great Western Highway
- minor relocation of existing bus stops and shelters on both sides of the highway to allow safer access.

An indicative cross section of the main alignment of Great Western Highway at Medlow Bath is shown in Figure 7.9 to Figure 7.12.

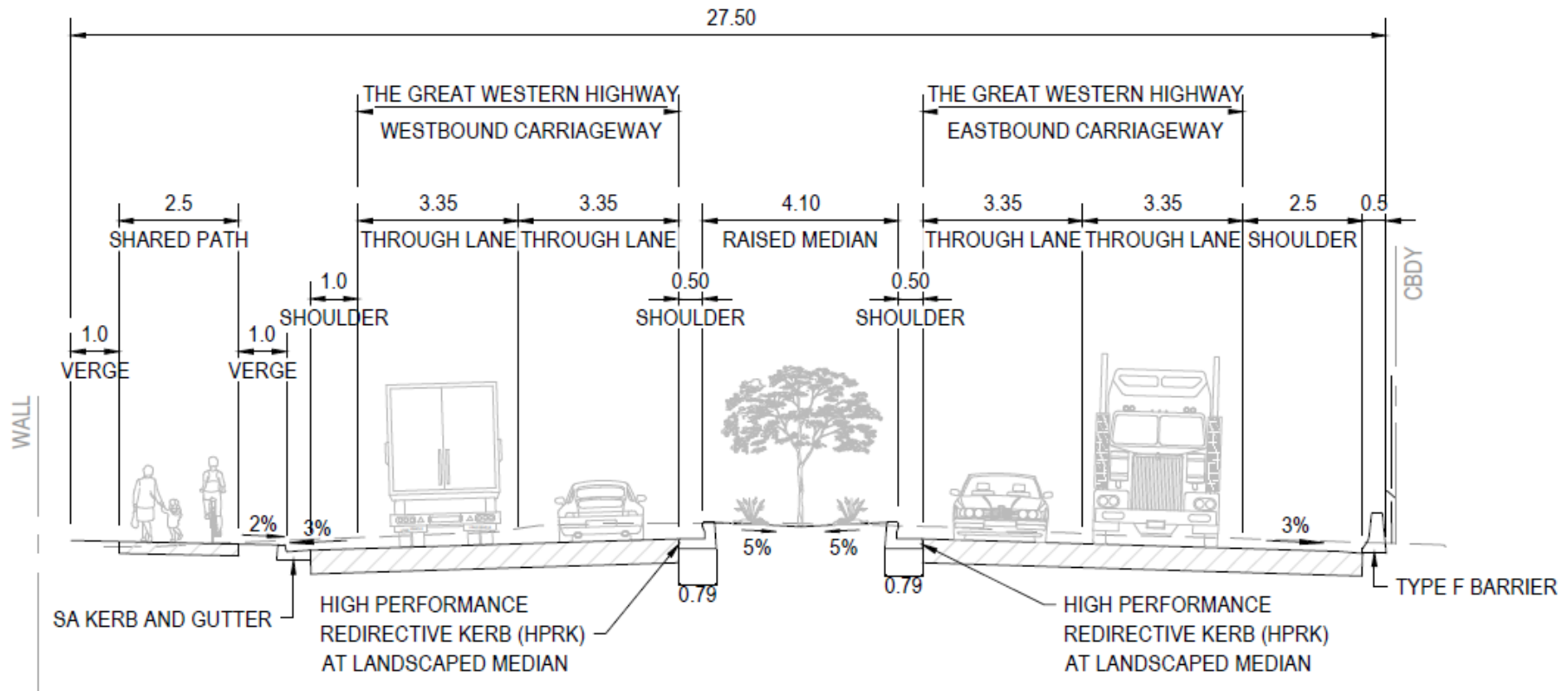
REPORT

Figure 7.9: Typical cross section within southern section of proposal area (i.e. just north of Bellevue Crescent)



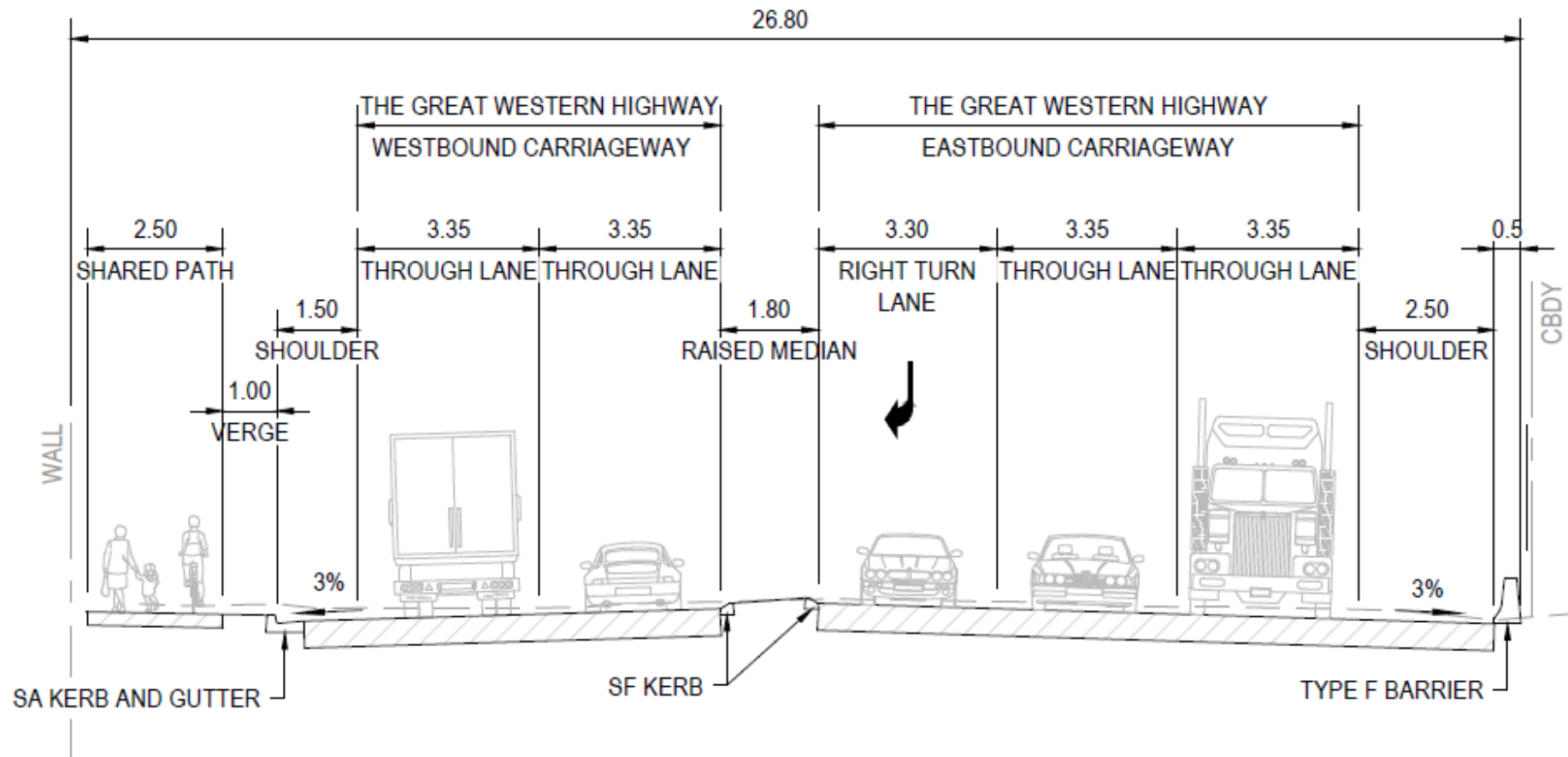
REPORT

Figure 7.10: Typical cross section within midpoint of the proposal area (i.e. near Hydro Majestic Hotel) – without right hand turn lane



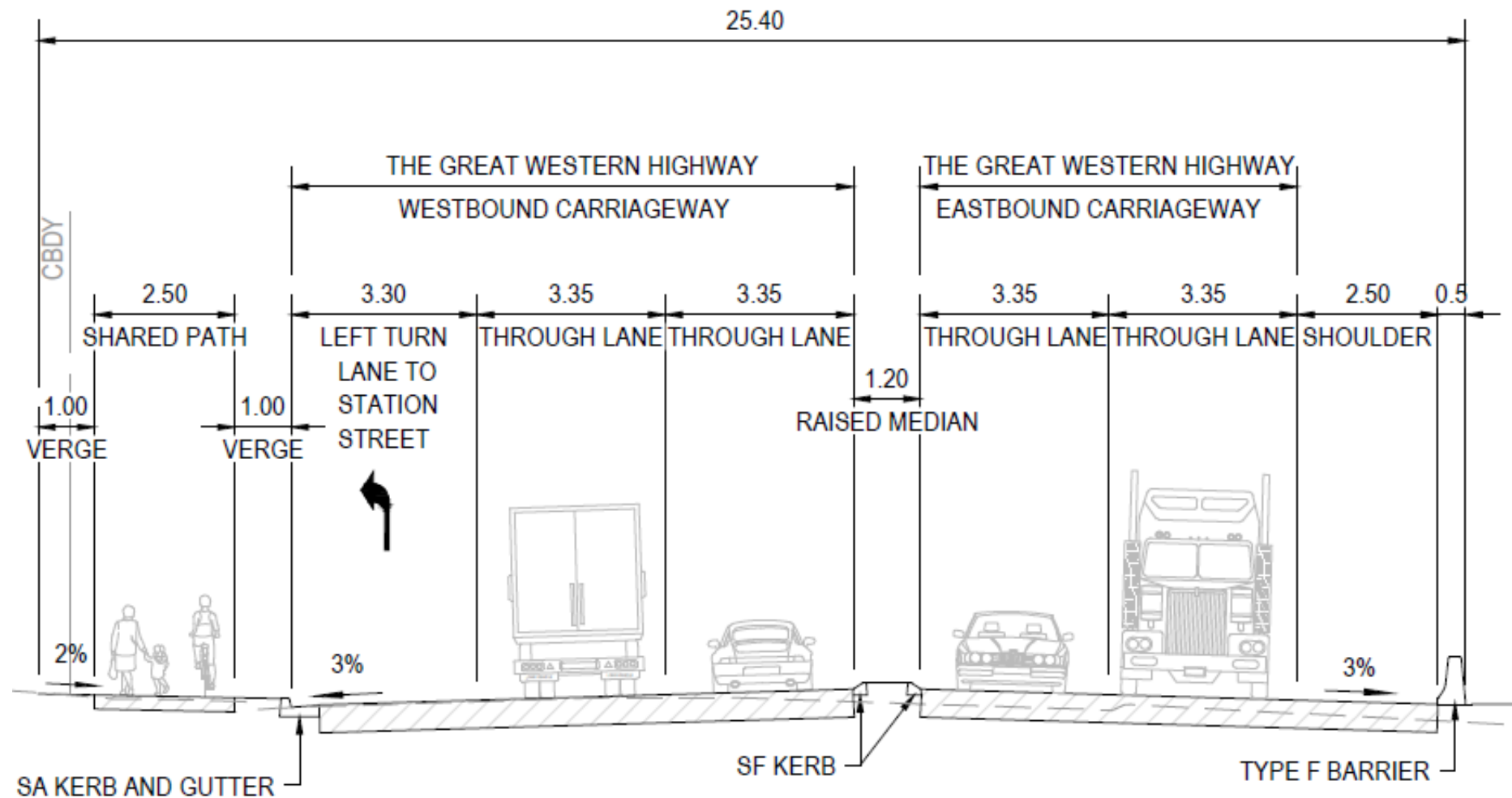
REPORT

Figure 7.11: Typical cross section within midpoint of the proposal area (i.e. near Hydro Majestic Hotel) – with right and turn lane



REPORT

Figure 7.12: Typical cross section within the northern section of proposal area (i.e. near Station Street)



7.4.3.2 Construction of new pedestrian bridge

The proposal includes the construction of a pedestrian bridge that spans about 60 metres, crossing from Railway Parade to the western side of Great Western Highway. This bridge includes stairs and lifts at Railway Parade, the Medlow Bath Station and on both sides of the Great Western Highway. The existing pedestrian rail level crossing at the southern side of the station platform would be removed. The clearance of the bridge will be a minimum of 6.1 metres over the road and 7.2 metres over the station platform.

The incorporation of these lifts and stairs would significantly improve connectivity of the area for commuters and tourists in line with Transport Accessibility Program (TAP) requirements. This will provide safe all ability access to the public transport services including the Medlow Bath railway station and bus services on Great Western Highway and Railway Parade.

The design of the bridge would be sympathetic to cultural and aesthetic characteristics of the area.

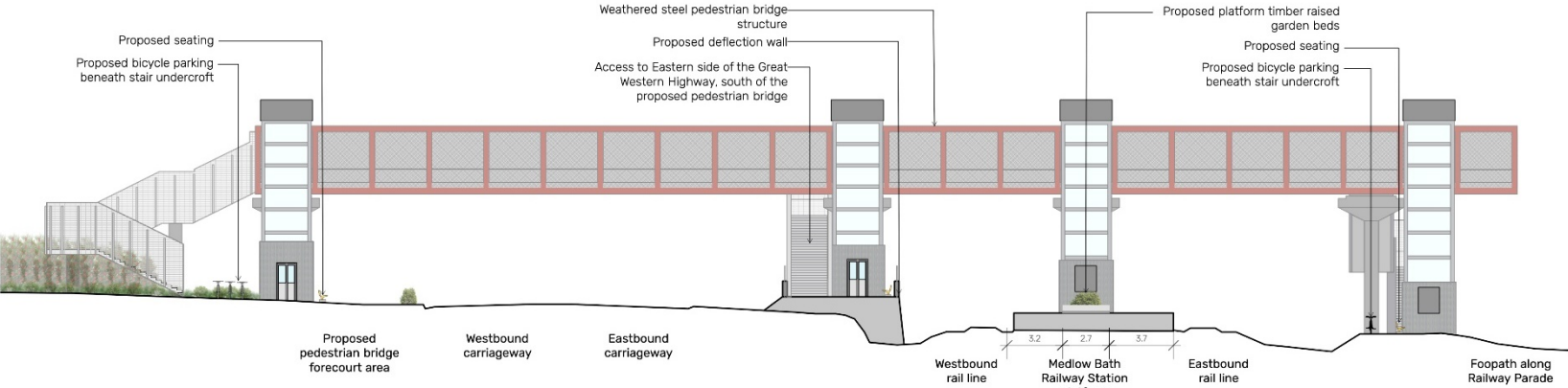
The proposed footbridge would be a three-span steel truss bridge with reinforced concrete piers and abutments. The structure would span the full width of the widened Great Western Highway, with abutments at the western side of the Great Western Highway and on Railway Parade. Piers are also located on the eastern side of the highway and on the railway station platform. The piers and abutments would be of reinforced concrete construction.

At each access/egress location on the structure there would be an independent lift structure, satisfying the requirements of T HR CI 12030 *ST Overbridges and Footbridges Design Standard* (Transport for NSW, 2020).

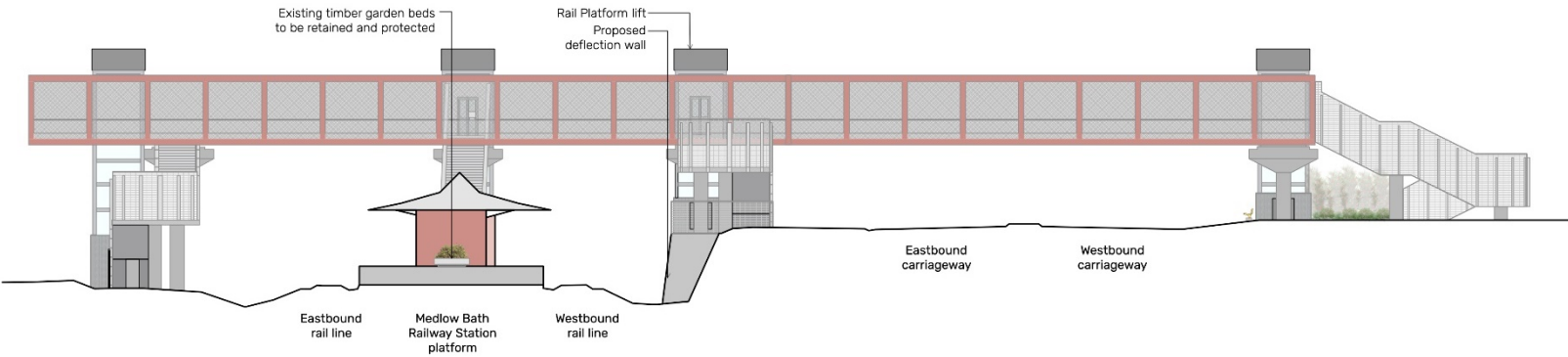
Indicative designs and detailed plan of the new pedestrian bridge are shown in Figure 7.13 to Figure 7.15. These are subject to detailed design.

REPORT

Figure 7.13: Indicative design of the new pedestrian bridge (subject to detailed design) (Source: Spackman Mossop Michaels 2021)



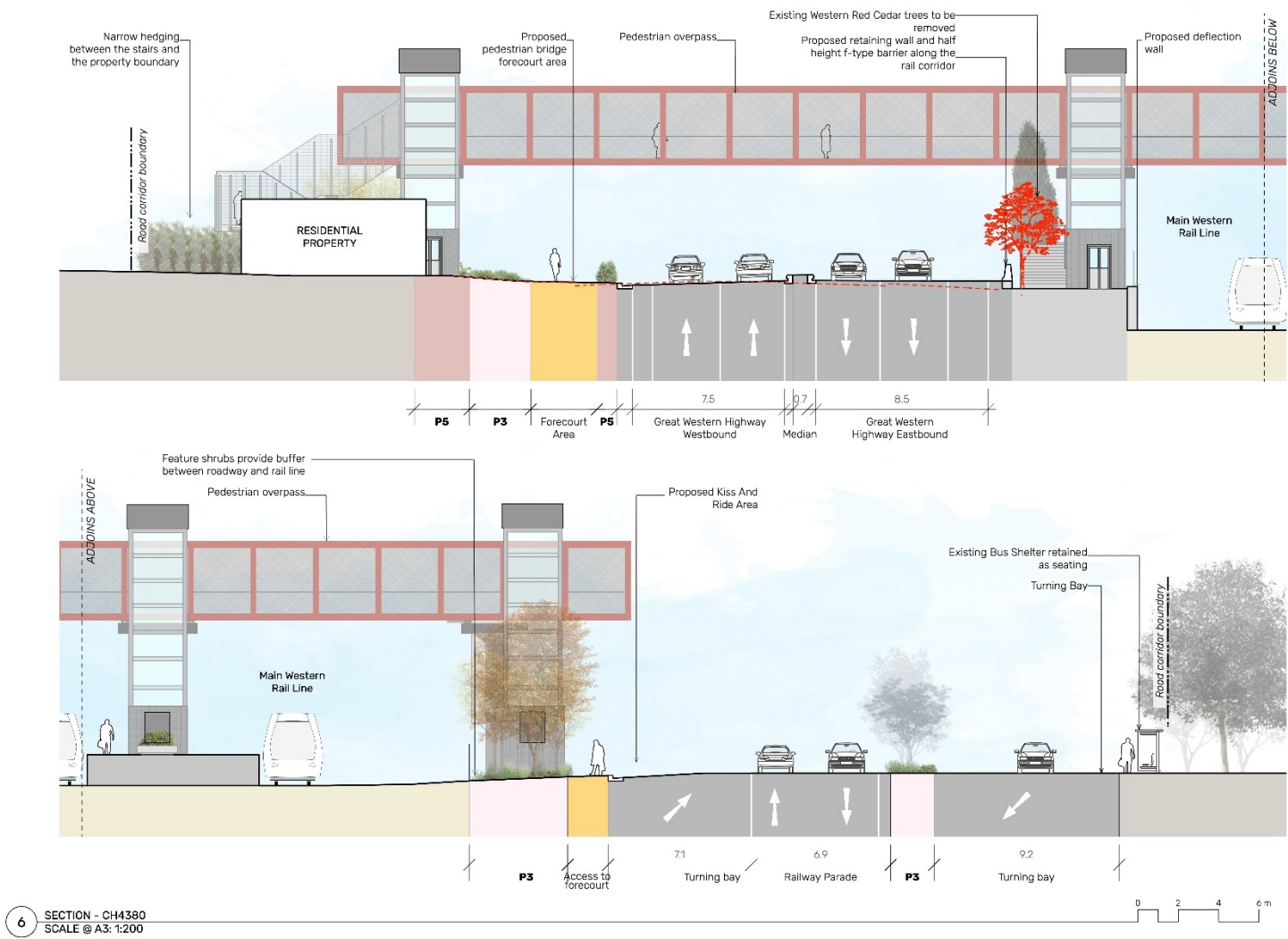
9 SOUTH ELEVATION - PEDESTRIAN OVERPASS
SCALE @ A3: 1:250



10 NORTH ELEVATION - PEDESTRIAN OVERPASS
SCALE @ A3: 1:250

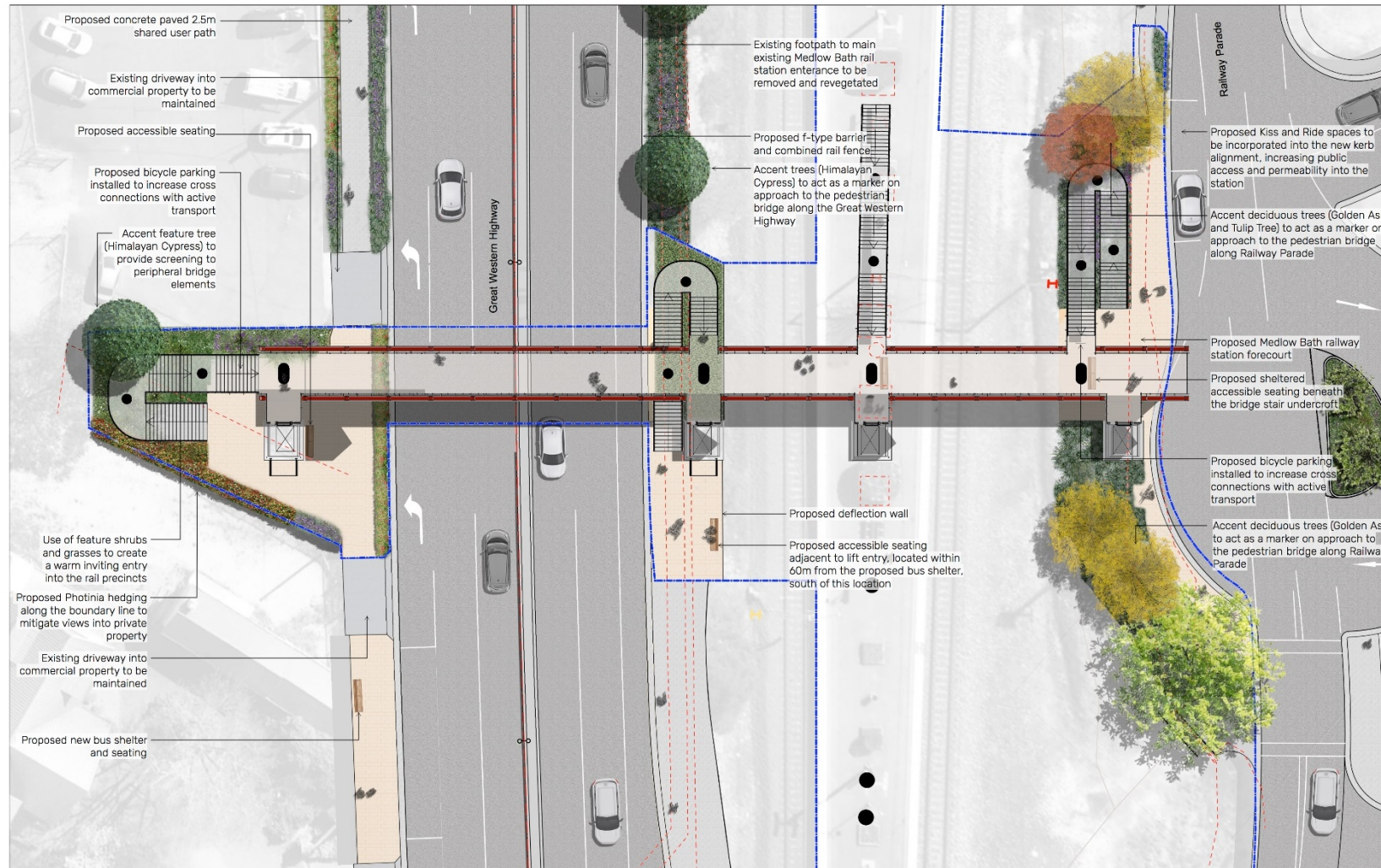
REPORT

Figure 7.14: Indicative cross section of proposed pedestrian bridge (subject to detailed design) (Source: Spackman Mossop Michaels 2021)



REPORT

Figure 7.15: Proposed pedestrian bridge crossing detailed plan (Source: Spackman Mossop Michaels 2021)



7.4.3.3 Construction of intersections on Great Western Highway

The proposal works to the Great Western Highway would incorporate intersection upgrades including:

- three way signalised intersection lanes for access/egress at Bellevue Crescent which will also include:
 - a right hand turn bay for eastbound vehicles from Great Western Highway into Bellevue Crescent
 - a left hand turn bay for westbound vehicles from Great Western Highway into Bellevue Crescent
 - U turn bay
- right hand turn bay on Great Western Highway for entry into the Hydro Majestic Hotel by eastbound vehicles.

7.4.3.4 Construction of bus bays on both sides of Great Western Highway

The proposal would include bays for local bus services to connect with the area and train station. This would include one on the westbound carriageway and one on the eastbound carriageway of the highway adjacent to the pedestrian bridge and lifts. The school bus stop in Railway Parade used to collect school children from the linking train services would be removed due to the upgraded highway bus stops.

7.4.3.5 Urban design and landscaping features

Key urban design and landscaping features of the proposal are:

- Retaining existing tree plantings where possible along Great Western Highway
- Landscaped median / verges including either:
 - native shrub and grass plantings with taller native trees to respond to nearby ecological features
 - ornamental shrub, groundcover plantings and trees to respond to other cultural aspects.

7.4.3.6 Drainage design

The proposal includes drainage infrastructure along the Great Western Highway. This includes cross-drainage structures and features such as:

- Reinforced box culverts
- Reinforced concrete pipes
- Pavement drainage pit and pipe system
- Swales and drainage along the main road alignment
- Transverse drainage across driveways/property access points
- Scour protections at drainage outlets
- Bridge deck drainage.

7.4.4 Ancillary facilities

Ancillary facilities would be required to support construction of the proposal and are still being developed. Ancillary facilities will include the following:

- Site compounds
- Laydown areas
- Stockpile sites
- Hardstands for the construction plant (including cranes)

REPORT

- Temporary sediment basins.

Stockpile locations would be refined during the detailed design phase using the criteria set out in the *Stockpile Management Guideline* (RMS, 2015). Where possible, the stockpile areas would be located on sites:

- Not prone to flash flooding
- More than 40 metres from a watercourse
- More than 50 metres from the nearest dwelling
- In previously disturbed areas that do not require the clearing of native vegetation
- In plain view of the public to deter theft and illegal dumping
- Outside the drip line of trees and on level ground wherever possible.

Bridge laydown areas for large precast structures would need to be in proximity to the bridge construction area.

Ancillary sites would be securely fenced with temporary fencing. Signage would be erected advising the general public of access restrictions. Upon completion of construction, the temporary site compound, work areas and stockpiles would be removed, the site cleared of all rubbish and materials and rehabilitated.

Where amendments or additional ancillary facilities are identified during construction, the contractor would consult with Transport for NSW to confirm the suitability of the proposed amendment or additional facility, and whether any additional environmental assessment is required.

7.4.5 Construction sedimentation basins

Construction of the proposal has the potential to affect water quality through erosion of exposed or disturbed areas and subsequent sedimentation of watercourses. To mitigate these effects, sediment basins would be installed within the proposal area to trap sediments and other pollutants from disturbed areas.

Sediment basins would collect a high proportion of sediment-laden runoff from disturbed areas of the proposal area. The ideal location of the sediment basins is on the downstream side of the proposal area. The proposed locations for sediment basins have also considered site constraints such as heritage, environmental, accessibility for maintenance or other constraints such as utilities.

The design criteria for the sediment basins are defined in the *Blue Book* (Soils and Construction, 2004 and 2008 Volume 2D Main Road) and Roads and Maritime *General Specifications* G36 and G38. The sediment basins would need to provide sufficient volume for settling and storage of sediments. The settling zone volume are estimated using the appropriate design rainfall depth and catchment areas. The storage zone is estimated using the Revised Universal Soil Loss Equation (RUSLE).

The final size and location of the basins would be confirmed during detailed design. Additional soil and water management measures would also be developed during detailed design and included in the CEMP.

7.5 Public utility adjustment

Transport for NSW has been consulting with public utility authorities as part of the design process to identify and locate existing utilities and incorporate utility authority requirements for relocations and/or adjustments.

Preliminary investigations have indicated that the following existing utilities are within the extent of the proposal:

- Overhead (majority of local network) and underground electricity – Endeavour Energy & RailCorp
- Water reticulation – Sydney Water Corporation
- Sewer reticulation – Sydney Water Corporation
- Natural gas – Jemena Gas
- Telecommunications – Telstra, Optus, NBN Co, etc.

REPORT

These affected utilities would be relocated or protected as part of the proposal, with the final location of any relocated utilities subject to consultation with the relevant utility providers.

Most of the utilities affected are on the western side of the Great Western Highway. As part of the proposal these services would be relocated to a new configuration outside the new road pavement keeping but still on the western side of the highway. The eastern side of the highway has little opportunity for services due to the widening of the road and inclusion of a retaining wall on the rail corridor boundary.

Some of the utilities on the western verge cross the rail corridor to Railway Parade (including Endeavour Energy 11kV via overhead infrastructure and Jemena Gas via a PE conduit).

There has been no request to date from any utility authorities to upgrade or future proof any assets.

Utilities that would be impacted by the proposal are identified in Table 7.3: Utility adjustment. Generally, utilities that would require relocation as part of the proposal would be relocated underground within the new road alignment.

Table 7.3: Utility adjustment

| Service | Location | Requirement | Service Type | Service Provider |
|------------------------------------|---|--|--|-------------------------|
| Power (Great Western Highway side) | Distribution Overhead/Underground on western verge of GWH between Bellevue Crescent and Hydro Majestic. Streetlighting and Low Voltage Overhead on western and eastern verges of GWH. 11kV overhead supply crosses the rail corridor near the middle of the project boundary. | Relocation required | 11kV high voltage, 415V and street lighting | Endeavour Energy |
| Power (Railway Parade side) | 11kV overhead between the rail corridor and Railway Parade | Section requires relocating underground at the pedestrian bridge | 11kV high voltage | RailCorp |
| Gas | Mainly present on the western verge of the GWH. Crosses to Railway Parade on the eastern side of the rail corridor | Relocation required | 300kPa 160mm diameter PE pipe | Jemena Gas |
| Tele-communication (including NBN) | Telstra network present on the western and eastern verge of the GWH and at Railway Parade. Optus fibre within the rail corridor. | Relocation and/or protection required | Fibre optic and copper. DA and CC network. Underground network mainly within Telstra network. Optus Inter Office Fibre. | Telstra Optus NBN |

REPORT

| Service | Location | Requirement | Service Type | Service Provider |
|---------------------------|--|---------------------------------------|--|--------------------------|
| Water – Potable and sewer | Mainly present on the western verge of the GWH and section at Railway Parade turning bay | Relocation and/or protection required | 150mm potable main and 110mm sewer main on Westbound verge of GWH. 100 mm potable main and 450 mm sewer main on railway parade. | Sydney Water Corporation |

7.6 Property acquisition

The proposal is mainly occurring within the existing road corridor. However based on the current design, the property acquisitions outlined in Table 7.4: Current property acquisitions are required.

Table 7.4: Current property acquisitions

| Address | Purpose |
|---|--|
| Part of 46 Great Western Highway, Medlow Bath (Lot C/DP413431) | Western footing of pedestrian bridge |
| 16 - 18 Railway Parade, Medlow Bath (Lot 1/1/DP2590) | Permanent drainage basin |
| 106 Great Western Highway Medlow Bath (Lot 9/DP701200) | Space for U-turn bay on Bellevue Crescent |
| 128W Great Western Highway Medlow Bath (Lots 219 and 220/DP1211208) | Provide additional space for road corridor |
| Part of 52-88 Great Western Highway, Medlow Bath (Lot 20/DP25570) – at rear of petrol station | Alternative Bellevue Crescent alignment option |
| 90-98 Great Western Highway, Medlow Bath (Lots 3 and 4/DP25570 and Part of 5/DP25570) | Alternative Bellevue Crescent alignment option |

8 HERITAGE IMPACT ASSESSMENT

This section assesses the impact of the proposal on items of heritage significance located in, or within the immediate vicinity of, the proposal area. The level of impact is assessed in accordance with the definitions in Table 8.1 as defined in the *Material Threshold Policy* (Heritage NSW, February 2020).

Table 8.1: Defining levels of impact

| Impact | Definition |
|-----------------------------------|--|
| Total loss of significance | Major adverse impacts to the extent where the place would no longer meet the criteria for listing on the SHR. |
| Adverse impact | Major (that is, more than minor or moderate) adverse impacts to State heritage significance. Moderate adverse impacts to State heritage significance Minor adverse impacts to State heritage significance |
| Little to no impact* | An alteration to State heritage significance that is so minor that it is considered negligible. * Little to no impact (as opposed to no impact) acknowledges that any change will result in some level of impact/alteration to State heritage significance. |
| Positive impact | Alterations that enhance the ability to demonstrate the State heritage significance of an SHR listed place. |

8.1 Summary of heritage impact

The following aspects of the proposal respect or enhance the heritage significance of the item or conservation area for the following reasons:

- The proposal respects the heritage significance of a number of heritage items by, where possible, keeping within the designated road reserve alignment and avoiding impact to abutting heritage curtilages and significant heritage fabric.
- The proposal requires the removal of the potential heritage item, *bus shelter*. The proposal respects the identified potential heritage significance of bus shelter by proposing to relocate the shelter elsewhere within the township, enabling it to retain its mural and setting within Medlow Bath township and the Blue Mountains LGA.
- Detailed design phase should consider the following in order to mitigate potential visual impacts:
 - minimise bulk of new built forms and clearly separate new from existing heritage fabric
 - respond to existing and significant architectural detail, such as the architectural detailing of the station building, or the footbridge.

The following aspects of the proposal could detrimentally impact on heritage significance. The reasons are explained as well as the measures taken to minimise impacts:

- The new pedestrian bridge would introduce a new visual element to *Medlow Bath Railway Station* that would visually dominate the heritage setting as well as block views to and from the station complex. The pedestrian bridge would also impact views and vistas across Medlow Bath, impacting views to and from

REPORT

surrounding heritage items. The bridge is currently at 20% detailed design. This impact could be minimised to some degree throughout the detailed design phase.

- Construction of the highway and installation of pedestrian bridge would physically impact *Avenue of trees* through the reduction of its heritage curtilage and the removal of a large number of trees. An Arboricultural Impact Assessment has been recommended to determine the full impact of the proposal and recommend appropriate management and mitigation measures to potentially reduce this impact.
- Construction of the alternate intersection design for Bellevue Crescent:
 - could potentially impact archaeological resources associated with the former Glenara Cottage. An Historic (non-Aboriginal) Archaeological Assessment of this area has been recommended to determine the full impact of the proposal and recommend appropriate management and mitigation measures which may potentially reduce this impact.
 - would impact the remaining stands of pine trees seemingly associated with the former Glenara Cottage. Some trees would require removal, while others may have their critical root zones impacted. An Arboricultural Impact Assessment has been recommended to determine the full impact of the proposal and recommend appropriate management and mitigation measures to potentially reduce this impact.
 - would have a minor adverse impact on *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026) through the reduction of their respective heritage curtilages.

The following sympathetic solutions have been considered and discounted for the following reasons:

- **No action (i.e. 'do nothing' approach):**
 - The proposed road works aim to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic. The current alignment is narrow and difficult, constrained by the Blue Mountains National Park, steep topography, a railway line and existing towns for which the highway acts as the main street. These factors result in the following issues:
 - reduces freight efficiency by limiting access for safer and more sustainable high productivity vehicles
 - limits access during incidents and natural disasters
 - slows travel speeds with limited overtaking opportunities and steep gradients (more than double the recommended maximum level)
 - causes delays of up to 80 minutes in peak times
 - has higher than state average crash rates, and
 - impairs amenity for local communities with high through traffic volumes and congestion.
- It is noted both a tunnel and pedestrian underpass have previously been suggested as alternatives. TfNSW has indicated neither option is feasible. There is no location in Medlow Bath with the available space required to allow a pedestrian underpass below both the railway and the highway. Likewise, the upgrade through Medlow Bath is not suited to a tunnel, due to the existing gradient and short length of this section and would not address safety issues currently on the Highway. Widening the highway within the existing corridor is the most responsible approach within the program budget.
- A bypass has also been suggested as an alternative. Medlow Bath is constrained by the ridgetop terrain, private property and the rail line. TfNSW notes that bypassing the existing highway through the township on the east was considered and ruled out in the 1950s. The existing corridor through Medlow Bath is well suited to widening. The proposal builds on work completed in 2002 during the bridge replacement over the railway lines at Station Street.

8.2 Impact assessment discussion

8.2.1 Construction of a four lane divided carriage way

The road upgrade to a four lane divided carriage way is required to provide a safer and more efficient link between Central West NSW and the Sydney Motorway Network for freight, tourist and general traffic. The proposed four lane divided carriageway would utilise the full width of the exiting road reserve through the Medlow Bath township and would tie into existing road alignments south of Bellevue Crescent and at Station Street. Construction would include full depth highway pavement and associated local road, driveway, footpath, kerb and gutter reconstruction work.

At the southern (eastbound) end of the proposal area, provision of a turning bay into Bellevue Crescent on the western side of the highway is proposed, which may have a major adverse impact to the potential heritage item, *advertising sign*. The potential heritage item is located immediately beside a retaining wall (RW1) proposed for construction between the new turn lane and the existing access track. Construction of the turn lane and retaining wall may require the removal and subsequent demolition of the *advertising sign*, resulting in total loss of significance.

Proposed widening and construction of the highway would have little to no impact on the *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002) as the proposed road does not impede on the heritage curtilage. However, vibration from construction may have a moderate adverse impact to the significant fabric of the stone fence by causing destabilisation. Additionally, excavation works associated with the road construction may have an adverse impact on significant trees *Pinus radiata* located within the Hydro Majestic's heritage curtilage through impact to critical root zones. Recommendations have been made within this SOHI for vibration monitoring and to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to these significant trees and advise appropriate management and mitigation measures for detailed design and construction.

Proposed widening and construction of the highway would have a major adverse impact on *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB017). The proposal would impact and greatly reduce the heritage curtilage of the item and require the removal of a significant number of trees. The strong landscape presence of *Avenue of trees* is recognised and is proposed to be replaced by plantings of Norway Maple within the median strip as an entrance marker and to continue the historic theme of trees leading you into Medlow Bath. Reflecting the historic character of *Avenue of trees* in this way would mitigate the impact to some degree. However, it is noted that the median plantings extend beyond the original extent of *Avenue of trees* (formerly *Avenue of Radiata Pines*), which terminated at the boundary of the Hydro Majestic. This extension does not reflect the original nature, extent and significance of the heritage item and would further impact the *Avenue of trees* through misrepresentation. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to *Avenue of trees* and advise appropriate management and mitigation measures for detailed design and construction. Recommendations have also been made to refine the proposed tree plantings in consultation with relevant stakeholders.

The footpath adjacent to *Avenue of trees* is proposed to be removed, and the western entrance to Medlow Bath Railway Station via the existing footbridge closed. The current location of *Avenue of trees* and the existing footpath are, at present, proposed to be replaced with accent trees (Himalaya Cypress) to act as a marker on approach to the pedestrian bridge along the Great Western Highway. This species has been selected as it is in line with the Blue Mountains City Council street planting guideline. This would have a major adverse impact on *Avenue of trees* through the removal of trees and the reduction of its heritage curtilage. However, the linear nature of the plantings, colour and shape of these trees would reflect the historic character of *Avenue of trees* and would mitigate this impact to some degree.

Proposed works along northbound Station Street would have little to no impact on *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019) as the proposed road does not impede on the heritage curtilage. However, vibration from construction may have a minor adverse impact on significant fabric, particularly the house and shop located at Lot 1 Great Western Highway which abuts the proposal area.

Proposed works on Railway Parade include formalisation of existing parking, addition of U-turn area and provision of two kiss and ride spaces for commuters are required in order to meet Transport Access Program (TAP) requirements. These works would have little to no physical impact on *Urunga* (Blue Mountains LEP 2015 Item No.MB017) as they do not impact the curtilage of the heritage item, however they would have a

REPORT

minor adverse visual impact through the alteration of *Urunga's* setting and views to and from the heritage item. The works would have little to no impact on *House* (Blue Mountains LEP 2015 Item No. MB018) as they would not impact the heritage curtilage of this item or alter its setting, views or vistas.

Provision of a 6 metre raised landscape median for trees within the road alignment would have little to no impact on surrounding heritage items. The median strip would be planted with deciduous trees to strengthen the landscape aesthetic of Medlow Bath and link it to the surrounding townships' seasonal landscape cultural identity. This would enhance the character of the current landscape of Medlow Bath and would complement surrounding significant heritage plantings. Historically significant plantings within Medlow Bath are largely introduced species and have typically comprised stands of trees such as *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB017).

Excavation for the proposed road works would impact subsurface deposits. The potential for archaeological resources within the proposed road construction footprint is assessed to be low. Potential archaeological resources are likely associated with former road surfaces of the Great Western Highway and Railway Parade, and if present, have likely been impacted by twentieth century road resurfacing and would unlikely be intact or provide research potential. Therefore, the proposed road works would have little to no impact on potential archaeological resources of the proposal area. An unexpected archaeological finds procedure has been recommended as an added measure to minimise any potential impact.

The following activities associated with the construction of the road are considered to have little to no impact on any identified heritage items:

- upgrades to the Bellevue Crescent intersection to include three way signals for access and egress, and provision of a turning bay and service road access
- provision of service road and driveways north of Bellevue Crescent
- provision of new driveways and associated carports on Delmonte Avenue

8.2.1.1 Alternate design for Bellevue Crescent

TfNSW have proposed an alternate design for linking Bellevue Crescent to Great Western Highway, which means the preferred design discussed above, would no longer be required. The alternate design would see the current intersection at Bellevue Crescent and Great Western Highway closed and a new signalised intersection and road connecting to the rear of Bellevue Crescent placed on the vacant lot immediately south of the petrol station/Hydro Majestic. This extension of Bellevue Crescent would occur within the heritage curtilage of the *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026). While still forming part of the grounds of the Hydro Majestic, there are no extant structures present or evidence of walking tracks within this section of the property. However, this area does contain a number of Moveable Heritage items and is the location on which the former Glenara Cottage. Potential remains of Glenara Cottage were identified during the site inspection and remnants of the stands of mature pines that once surrounded Glenara Cottage still remain.

Construction of the alternate design for Bellevue Crescent would have a moderate adverse impact on the remaining stands of pine trees present in this portion of the proposal area. If archaeological resources associated with Glenara Cottage are present in this portion of the proposal area, the alternate design could have a major adverse impact on archaeological resources or deposits. The alternate design would also reduce the heritage curtilage of the *Hydro Majestic*. Cumulatively, these impacts would result in a minor adverse impact on *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002). The alternate design would have a minor adverse impact on *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026) through the reduction of its heritage curtilage.

An Historic (non-Aboriginal) Archaeological Assessment of the vacant lot immediately south of the petrol station and *Hydro Majestic* has been recommended to inform the detailed design. This report would also detail appropriate management and mitigation measures for any identified archaeological resources. An Arboricultural Assessment of the mature pines on the lot has also been recommended to assess their significance and advise appropriate management and mitigation measures.

8.2.2 Construction of a new pedestrian bridge

A proposed new pedestrian bridge connecting Railway Parade, Medlow Bath Station and new indented bus bays on both sides of the highway is required to provide safe crossing of the highway and railway line. The existing pedestrian rail level crossing at the southern end of the station platform would be removed. A new 60 metre long pedestrian bridge would be constructed over the highway and provide both stair and lift access to Medlow Bath Station as well as Princes Highway and Railway Parade, improving accessibility of the station in line with the requirements of the Commonwealth *Disability Discrimination Act 1992* (DDA) and the *Disability Standards for Accessible Public transport 2020* (DSAPT).

The introduction of lifts and stairs would significantly improve connectivity of the area for commuters and tourists in line with the Transport Access Program (TAP) requirements. It would provide safe all ability access to the public transport services including the Medlow Bath Railway Station and bus services on Great Western Highway and Railway Parade.

The proposed pedestrian bridge would be a three-span steel truss bridge with reinforced concrete piers and abutments. The structure would be of weathered steel, have a concrete base and be stainless steel tensile mesh safety screens. It would span the full width of the widened Great Western Highway, with abutments at the western side of the Great Western Highway and on Railway Parade. Piers would also be located on the eastern side of the highway and on the railway station platform. The piers and abutments would be of reinforced concrete construction. Stair abutments and retaining walls are proposed to be clad in sandstone. At each access/egress location on the structure there would be an independent lift structure, satisfying the requirements of T HR CI 12030 ST Overbridges and Footbridges Design Standard (TfNSW 2020).

The bridge design has been informed by what type of structure would be most sympathetic to the historic setting of Medlow Bath in conjunction with what type of structure would meet Sydney Trains' fire rating and collision loading requirements. The steel truss style bridge was selected as it meets both these criteria. In relation to heritage, the open, see through truss structure is more visually sympathetic to alternative options such as solid concrete. The weathered steel is low maintenance and meets Sydney Trains safety requirements. In addition, the selected colour scheme is intended to reference the surrounding natural landscape. While the nature and engineering requirements of the bridge mean it will have an inevitable visual impact, the design intention is to incorporate colours and materials to avoid dominating the surrounding landscape as much as possible.

The proposed pedestrian bridge would have a minor adverse physical impact on *Medlow Bath Railway Station* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003) through the installation of bridge piers, stairs and lifts. Removal of at least one garden bed on the platform and relocation of light poles would also be required. Although already heavily modified, the garden beds along the platform relate to the overall character of the station and have been present at the station since the 1902 platform was constructed. Excavation would be required on the station's island platform to accommodate bridge piers, lift wells and stairs. Excavation may impact the significant 1902 station platform. However, the platform has been resurfaced. Subsurface layers of the platform likely comprise fill and are considered to be of low archaeological potential. No physical impact would occur to the Station Building, signal room or the 1901 footbridge as the proposed pedestrian bridge would be located at the southern end of the platform. The existing pedestrian rail level crossing at the southern end of the station platform would also be removed. Removal of the level crossing would not impact the significant 1902 station platform, as the level crossing was added to the station after the platform was extended southward in 1942.

The proposed pedestrian bridge would have a moderate to major adverse impact on *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB017). The installation of bridge piers, lift and stairs would occur within the heritage curtilage and likely impact critical root zones of trees, or require the removal of trees altogether. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to *Avenue of trees* and advise appropriate management and mitigation measures for detailed design and construction.

The proposed pedestrian bridge would have a minor adverse impact on the potential heritage item, *bus shelter*. Removal of the *bus shelter* is required for the provision of a bus bay on the southbound side of the highway. The bush shelter is considered potentially significant for its historic mural, which forms part of a wider mural campaign through the Blue Mountains LGA. The proposed impact to the *bus shelter* would be mitigated by relocating the *bus shelter* elsewhere, enabling it to retain its mural and setting within Medlow Bath township and the Blue Mountains LGA.

REPORT

The western bridge pier would be located immediately adjacent to the heritage curtilage boundary of *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No. MB026). However construction of the pedestrian bridge would not physically impact the heritage item as works would not impede on the heritage curtilage.

A visual impact assessment (VIA) (see Section 8.3) concluded the proposal, in particular the installation of the pedestrian bridge, would have an overall moderate to high visual impact on the proposal area. Although clearly separating old from new, the large weathered steel and concrete structure of the bridge would visually dominate the historic setting of *Medlow Bath Railway Station* and obstruct views and vistas both to and from the station complex. Overall, the proposed pedestrian bridge would have a major visual impact on *Medlow Bath Railway Station*. This visual impact extends across the proposal area, whereby the proposed pedestrian bridge would have a moderate to major visual impact on views to and from the following heritage items:

- *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002)
- *Former Post and Telegraph Store* (Blue Mountains LEP 2015 Item No.MB008)
- *St Luke's Anglican Church* (Blue Mountains LEP 2015 Item No.MB010)
- *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB015)
- *Urunga* (Blue Mountains LEP 2015 Item No.MB017)
- *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019)
- *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No. MB026)
- *Bus shelter* (potential heritage item)

Views to and from *House* (Blue Mountains LEP 2015 Item No. M018) are unlikely to be impacted due to the current closed setting of the heritage item.

However, the proposed pedestrian bridge is currently at concept design stage and would be subject to detailed design. To mitigate the potential visual impact to some degree, recommendations have been made within this SOHI in regards to the detailed design.

The installation of the pedestrian bridge would require ground disturbance for bridge piers, lift wells and stairs on either side of the Great Western Highway, on the station platform and adjacent to Railway Parade. The potential for archaeological resources within the footprint of the proposed pedestrian bridge is assessed to be low. Potential archaeological resources are likely associated with the former waiting shed on the station platform or the former goods shed and platform on the western side of *Medlow Bath Railway Station*. If surviving, archaeological resources associated with the former waiting shed are likely to have been impacted by the construction of the existing platform. Therefore, construction of the pedestrian footbridge on the station platform would likely have little to no impact on archaeological resources associated with the former waiting shed.

While archaeological resources associated with the former goods shed and associated platform are unlikely to have survived demolition, if present they may possibly remain relatively intact as no construction appears to have occurred along the western side of the station since the goods shed and platform were removed. Therefore, construction of the pedestrian footbridge between the station and highway may have a moderate to major impact on potential archaeological resources, if surviving. An unexpected archaeological finds procedure has been recommended as an added measure to minimise any potential impact.

8.2.3 Shared use (pedestrian/cyclist) path

The proposed shared use path adjacent to the westbound carriageway would have little to no impact on the *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002) as the proposed path does not impede on the heritage curtilage. However, vibration from construction may have a moderate adverse impact to the significant fabric of the stone fence by causing destabilisation. Additionally, excavation works associated with the path may have an adverse impact on significant trees *radiata pinus* located within the Hydro Majestic's heritage curtilage through impact to critical root zones. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the

REPORT

potential impact to these significant trees and advise appropriate management and mitigation measures for detailed design and construction.

The proposed shared use path adjacent to the western side of the carriageway would have little to no impact on *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019) as the proposed path does not impede on the heritage curtilage. However, vibration from construction may have a minor adverse impact on significant fabric, particularly the house and shop located at Lot 1 Great Western Highway which abuts the proposal area.

8.2.4 Retaining wall and traffic barrier construction

Five retaining walls (RW1 – RW5) are proposed across the proposal area. RW1 would be a highly visible retaining wall, requiring a high quality finish, while RW2 – RW5 would be stand rail facing retaining walls with half height F-type Barrier. Sawcut, sandstone cladding in natural finish is proposed to be used for all retaining walls, combined with a planting buffer at the base of the structure.

Retaining wall 1 (RW1) on the western side of the highway at the southern end of the proposal area may have a major adverse impact to the potential heritage item, *advertising sign*. The potential heritage item is located immediately beside RW1 proposed for construction between the new turn lane into Bellevue Crescent and the existing access track. Construction of the retaining wall may require the removal and subsequent demolition of the *advertising sign*, resulting in total loss of significance.

Retaining walls RW2, RW3 and RW4 are proposed along the eastern side of Great Western Highway, south of the Hydro Majestic. These three retaining walls would not impact heritage items located within the proposal area.

RW5 is proposed within the heritage curtilage of *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB017). Construction would require excavation that could potentially impact critical root zones of trees, or require the removal of trees altogether. Construction of the retaining wall would have a moderate to major adverse impact on *Avenue of trees*. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to *Avenue of trees* and advise appropriate management and mitigation measures for detailed design and construction.

The proposed RW5 could have a moderate visual impact on *Medlow Bath Railway Station* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003) and *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002). Views and vistas to and from the heritage items would be obscured by the retaining wall. This potential impact may be mitigated to some degree through detailed design of form, height and materials.

The five retaining walls would introduce a new visual element to the landscape of the proposal area, resulting in a minor to moderate visual impact to surrounding heritage items. However, at present, the same material and landscape design is proposed for each of the five walls, providing visual cohesion across the proposal area, mitigating this impact to some degree.

8.2.5 Utility relocation and stormwater drainage upgrade

8.2.5.1 Utility relocation

Utilities that would require relocation as part of the proposal would be relocated underground within the new road alignment. The potential for archaeological resources within the proposed road construction footprint is assessed to be low. Potential archaeological resources are likely associated with former road surfaces of the Great Western Highway and Railway Parade, and if present, have likely been impacted by twentieth century road resurfacing and would unlikely be intact or provide research potential. Therefore, the proposed utility relocation would have little to no impact on potential archaeological resources of the proposal area. Recommendations have been included within this SOHI to address any unexpected archaeological finds.

Utilities removed or relocated beneath the shared use path along the western side of the highway, would have little to no impact on the *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002) as the proposal area does not impede on the heritage curtilage. However, excavation may have a moderate adverse impact to the significant fabric of the stone fence by causing destabilisation. Additionally, excavation works associated with utility relocation may have an adverse impact on significant trees *radiata pinus* located within

REPORT

the Hydro Majestic's heritage curtilage through impact to critical root zones. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to these significant trees and advise appropriate management and mitigation measures for detailed design and construction.

8.2.5.2 Stormwater drainage upgrades

Stormwater drainage upgrades would require the installation of new stormwater pipes and pits along the length of the proposal area, either side of the highway, beneath the railway line, along Bellevue Crescent, along the western side of Station Street and along the eastern side of Railway Parade. Subsurface excavation associated with the installation of stormwater pipes would occur within the heritage curtilage of *Avenue of trees* (Blue Mountains LEP 2015 Item No.MB017) and require excavation that could potentially impact critical root zones of trees, or require the removal of trees altogether. Construction of the retaining wall would have a moderate to major adverse impact on *Avenue of trees*. Recommendations have been made within this SOHI to engage a suitably qualified arborist to undertake an arboricultural assessment to fully investigate the potential impact to *Avenue of trees* and advise appropriate management and mitigation measures for detailed design and construction.

Stormwater drainage would connect in with existing drainage channels and drain toward the combined onsite detention and water quality basin proposed within Medlow Bath Park. This would then connect into the existing rock lined channel within Medlow Bath Park. A new stormwater pipe would cross the railway alignment and the existing crossing which comprises the potential heritage item, *sandstone railway culvert*, would be made redundant. No work is proposed on the existing potential heritage item, *sandstone railway culvert*. Therefore, the proposed stormwater drainage upgrade would likely have little to no impact on sandstone railway culvert. Recommendations have been made in this SOHI to minimise any inadvertent harm to potential significant fabric.

8.2.6 Ancillary facilities

Ancillary facilities for site compounds, laydown and stockpile areas and hardstands would be required to support construction of the proposal. These areas are proposed within a vacant block of land and a railway stockpile compound area adjacent to the highway. No ground disturbance would occur within these compound areas. However, within the vacant land adjacent to the petrol station, there is moderate to high potential for archaeological resources associated with Glenara Cottage to present. While the use of ancillary facilities would have little to no heritage impact and little to no archaeological impact, given the potential for archaeological resources to be present, recommendations have been made to ensure no inadvertent impact to potential archaeological resources occurs.

8.2.7 Sediment basin

A sediment basin is proposed for within Medlow Bath Park. While ground disturbance may be required, Medlow Bath Park is not considered to have archaeological potential, and no known, or identified heritage items, are located within Medlow Bath Park. Resultantly, the sediment basin would have little to no heritage impact and little to no archaeological impact.

8.2.8 Landscaping design, materials and finishes

The proposed urban design materials palette for the proposal area is cohesive, comprising mainly of natural reds, greys and browns, accented with brushed stainless steel. These contrasting elements are complemented by native and exotic plantings to enhance wayfinding and create welcoming spaces that are visually inviting. Planting species would be in line with Blue Mountains City Council weed list and street tree masterplan guidelines.

Key landscaping design elements of the proposal area include:

- Accent tree plantings at key points, including areas identified for placemaking, marker moments for proposed bus stops and station entries. Deciduous tree plantings would aim to provide colour and foliage in the summer months and allow for light to permeate during the winter months.

REPORT

- Village median plantings to evoke historical therapeutic treatments, for which Medlow Bath was known for during the 1900's. Median species would consist of massed colourful exotic shrubs and groundcovers, contrasting colour and texture to the existing bushland character Medlow Bath.
- Village feature planting of trees within medians to provide colour and foliage in the summer months and allow for light to permeate during the winter months, whilst also providing a canopy that allows for views to be maintained.

The landscaping design, materials and finishes would have a minor visual impact on surrounding heritage items. Proposed plantings complement the existing heritage items stylistically, while built structures and modern landscaping would be of contrasting form and materials to clearly separate old from new.

8.3 Visual impact assessment

Potential impacts of the proposal on landscape character and visual amenity have been assessed in the *Great Western Highway Upgrade Medlow Bath - Urban Design and Landscape Character and Visual Impact Assessment (LCVIA)*. A summary of the assessment in relation to heritage is presented in this section.

Viewpoints selected for the proposal are illustrated in Figure 8.1 and assessed in Table 8.2. Artist's impressions have been prepared for Viewpoints 1 to 6 to provide an illustration of how the proposal may appear during operation and are included in Figure 8.2 to Figure 8.7.

Figure 8.1: Visual impact assessment viewpoint locations (Spackman Mossop Michaels 2021)



Figure 8.2: Viewpoint 1 visualisation (Spackman Mossop Michaels 2021)



Figure 8.3: Viewpoint 2 visualisation (Spackman Mossop Michaels 2021)



Figure 8.4: Viewpoint 3 visualisation (Spackman Mossop Michaels 2021)



Figure 8.5: Viewpoint 4 visualisation (Spackman Mossop Michaels 2021)



Figure 8.6: Viewpoint 5 visualisation (Spackman Mossop Michaels 2021)



Figure 8.7: Viewpoint 6 visualisation (Spackman Mossop Michaels 2021)



REPORT

Table 8.2: Summary of visual impacts (Spackman Mossop Michaels 2021)

| Viewpoint Number | Visible Proposal Elements | | Sensitivity | Magnitude | Rationale | | Visual Impact |
|------------------|---|---|-------------|-----------|--|--|---------------|
| 1 | <ul style="list-style-type: none">• Pedestrian bridge• Widened dual carriageway• Upgraded shared user path• Shared zone• Turning area along Bellevue Crescent | <ul style="list-style-type: none">• Widened median and tree plantings• New tree, shrub and groundcover vegetation.• Roadside lighting (TBC) | HIGH | MODERATE | <ul style="list-style-type: none">• Increased hardstand across proposal extents• Proximity to proposal• Time before landscape mitigation can be effective | <ul style="list-style-type: none">• Changes to character• Dissection of distant bushland views due to scale, size and materiality of pedestrian bridge in the distance• Removal of existing vegetation | HIGH-MODERATE |
| 2 | <ul style="list-style-type: none">• Pedestrian bridge• Widened dual carriageway• Upgraded shared user path | <ul style="list-style-type: none">• Widened median and tree plantings• New tree, shrub and groundcover vegetation.• Roadside lighting (TBC) | MODERATE | MODERATE | <ul style="list-style-type: none">• Increased hardstand across proposal extents• Proximity to proposal• Minimal change to character | <ul style="list-style-type: none">• Distance of pedestrian bridge and partial screening of peripheral elements• Removal of existing trees• Transient Viewers | MODERATE |
| 3 | <ul style="list-style-type: none">• Pedestrian bridge• Widened dual carriageway• Upgraded shared user path | <ul style="list-style-type: none">• New tree, shrub and groundcover vegetation.• Roadside lighting (TBC) | LOW | MODERATE | <ul style="list-style-type: none">• Increased hardstand across proposal extents• Proximity to proposal• Limited opportunity for landscape mitigation | <ul style="list-style-type: none">• Dissection of distant bushland views due to scale, size and materiality of pedestrian bridge in the foreground• Removal of existing vegetation | MODERATE-LOW |
| 4 | <ul style="list-style-type: none">• Pedestrian bridge• Widened dual carriageway (in the distance)• Upgraded shared user path | <ul style="list-style-type: none">• New tree, shrub and groundcover vegetation.• Roadside lighting (TBC) | HIGH | MODERATE | <ul style="list-style-type: none">• Increased hardstand along existing gravel trail• Proximity to proposal• Marginal changes to character | <ul style="list-style-type: none">• Dissection of distant bushland views due to scale, size and materiality of pedestrian bridge in the midground• Potential for landscape mitigation | HIGH-MODERATE |
| 5 | <ul style="list-style-type: none">• Pedestrian bridge• Railway Parade road formalisation & new footpath | <ul style="list-style-type: none">• New tree, shrub and groundcover vegetation.• Widened dual carriageway | MODERATE | MODERATE | <ul style="list-style-type: none">• No change to existing character• Localised vegetation clearing• Proximity to proposal | <ul style="list-style-type: none">• Scale, size and materiality of pedestrian bridge in the foreground• Potential for landscape mitigation• Transient Viewers | MODERATE |
| 6 | <ul style="list-style-type: none">• Pedestrian bridge• Railway Parade road formalisation & new footpath | <ul style="list-style-type: none">• New tree, shrub and groundcover vegetation.• Roadside lighting (TBC) | MODERATE | HIGH | <ul style="list-style-type: none">• Proximity to the proposal• Scale, size and materiality of pedestrian bridge in the mid-foreground• Localised vegetation clearing | <ul style="list-style-type: none">• Increase light spill• Minimal changes to character• Increased hardstand in foreground• Potential for landscape mitigation | HIGH-MODERATE |
| 7 | <ul style="list-style-type: none">• Bellevue Crescent Realignment• New tree, shrub and groundcover vegetation | <ul style="list-style-type: none">• Signalised intersection at Bellevue Crescent• Roadside lighting (TBC) | HIGH | HIGH | <ul style="list-style-type: none">• Proximity to the proposal• Significant vegetation clearing• Increase light spill | <ul style="list-style-type: none">• Change in landuse and character• Increased hardstand in foreground• Minor opportunities for landscape mitigation | HIGH |

8.4 Summary Statements of Heritage Impact

8.4.1 Greater Blue Mountains Area (WHL Reference No. 917, NHL Place No. 105999)

The proposal would have **little to no impact** on this heritage item. No physical impact is proposed within the heritage curtilage of this heritage item and the proposed new pedestrian bridge would not visually impact views and vistas to or from this heritage item.

8.4.2 Medlow Bath Railway Station Group (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003)

The proposal would have a **major adverse impact** on this heritage item. While the proposal would have a minor physical impact on significant fabric through the relocation of elements such as light poles and the partial closure of the 1902 footbridge, the station's setting would be significantly altered through the partial removal of garden beds and the addition of a pedestrian bridge. The pedestrian bridge would add an additional built form to the station complex, which would not only visually dominate the heritage item but fundamentally change it. This impact may be mitigated to some degree through detailed design.

8.4.3 Hydro Majestic (Blue Mountains LEP 2015 Item No.MB002)

The proposal may have a **minor to moderate adverse physical impact** and would have a **moderate to major adverse visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage, however vibration from construction may have a minor to moderate adverse impact to the significant fabric of the stone fence by causing destabilisation. Additionally, excavation works associated with the proposal may have a minor to moderate adverse impact on significant trees *Pinus radiata* located within the Hydro Majestic's heritage curtilage through impact to critical root zones. This impact may be mitigated through the an arboricultural impact assessment which could advise appropriate management and mitigation measures for detailed design and construction. Installation of the pedestrian bridge would introduce a new built form to the views and vistas both to and from the heritage item. This impact may be mitigated to some degree through detailed design.

The proposed alternate design for Bellevue Crescent would have an additional **minor adverse impact** on this heritage item through the reduction of its heritage curtilage and impact on potentially significant pine plantings and archaeological resources. This impact could be mitigated through the preparation of an Historic (non-Aboriginal) Archaeological Assessment and Arboricultural Assessment.

8.4.4 Former Post and Telegraph Store (Blue Mountains LEP 2015 Item No.MB008)

The proposal would have **little to no physical impact** and a **moderate visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item. Installation of the pedestrian bridge would introduce a new built form to the views and vistas both to and from the heritage item. This impact may be mitigated to some degree through detailed design.

8.4.5 St Luke's Anglican Church (Blue Mountains LEP 2015 Item No.MB010)

The proposal would have **little to no physical impact** and a **minor to moderate visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item. The proposed new pedestrian bridge would not visually impact views to this heritage item, however it would alter views from the heritage item.

8.4.6 Horse Trough (Blue Mountains LEP 2015 Item No.MB013)

The proposal would have **little to no impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item and the proposed new pedestrian bridge would not visually impact views and vistas to or from this heritage item.

8.4.7 Avenue of trees (formerly *Avenue of Radiata Pines*) (Blue Mountains LEP 2015 Item No.MB015)

The proposal would have a **major adverse impact** on this heritage item. The proposal would impact and greatly reduce the heritage curtilage of this item and require the removal of a significant number of trees. Replacement of the removed trees with plantings in the median to reflect the historic significance of this heritage item would mitigate this impact to some degree. This impact may be further mitigated through the an arboricultural assessment which could advise appropriate tree management and mitigation measures for detailed design and construction. Construction of the pedestrian bridge would also add a new built form within the heritage curtilage that would visually dominate any surviving characteristics. This impact may be mitigated to some degree through detailed design.

8.4.8 Urunga (Blue Mountains LEP 2015 Item No.MB017)

The proposal would have **little to no physical impact** and a **moderate to major visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item. Installation of the pedestrian bridge would introduce a new built form to the views and vistas both to and from the heritage item. This impact may be mitigated to some degree through detailed design.

8.4.9 House (Blue Mountains LEP 2015 Item No.MB018).

The proposal would have **little to no impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item and the proposed new pedestrian bridge would have little to no visual impact on views and vistas to or from this heritage item.

8.4.10 Melbourne House, Cosy Cot, Sheleagh Cottage (Blue Mountains LEP 2015 Item No.MB019)

The proposal would have a **little to no physical impact** and a **moderate to major adverse visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage or to significant fabric of this heritage item. However, vibration from adjacent construction may have a minor adverse impact on significant fabric, particularly the house and shop located at Lot 1 Great Western Highway which abuts the proposal area. Installation of the pedestrian bridge would introduce a new built form to the views and vistas both to and from the heritage item. This impact may be mitigated to some degree through detailed design.

8.4.11 Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic) (Blue Mountains LEP 2015 Item No. MB026)

The proposal 'preferred option' would have **little to no physical impact** and a **moderate adverse visual impact** on this heritage item. No physical impact is proposed within the heritage curtilage of this item for the preferred design option. However, the proposed 'alternate design' for Bellevue Crescent would have an additional **minor adverse impact** on this heritage item through the reduction of its heritage curtilage. No walking tracks associated with this heritage item appear to be within the alternate design proposal area, therefore the alternate design proposal would have no physical impact on significant fabric of this heritage item.

8.4.12 Bus Shelter (potential heritage item)

The proposal would have a **minor to moderate physical impact** and a **moderate adverse visual impact** on this potential heritage item. While removal of the *bus shelter* is required for the proposal, this would be mitigated by relocating the *bus shelter* elsewhere within the township, enabling it to retain its mural and setting within Medlow Bath and the Blue Mountains LGA. However, relocation will require some work to the consolidation of the mural to prevent further loss, or to sympathetically reinstate missing portions resulting from the move.

8.4.13 Sandstone railway culvert (potential heritage item)

The proposal would have a **little to no impact** on this potential heritage item. While new stormwater drains may connect to the same drainage network, no physical impact is proposed to any potentially significant fabric.

8.4.14 Advertising sign

The proposal may have a **major adverse impact** on this potential heritage item. The potential heritage item may require removal and subsequent demolition to accommodate the proposed turning bay into Bellevue Crescent. This impact may be mitigated to some degree through detailed design and careful construction planning or alternatively archival recording.

8.5 Impact to archaeological potential

The proposal may have a **moderate to major adverse impact** on the archaeological potential of the proposal area, if archaeological resources survive. The proposal area has been assessed to be of low to moderate archaeological potential.

Former road surfaces would likely have been impacted by twentieth century road resurfacing and are unlikely to be present beneath the current road surface. Station infrastructure and waiting shed from the 1902 platform is unlikely to have survived demolition and construction of the existing station platform. However, if archaeological resources associated with the former goods shed and associated siding platform exist along the western boundary, they may possibly remain relatively intact as no construction appears to have occurred along the western side of the station since the goods shed and platform were removed.

Archaeological resources associated with Glenara Cottage may remain in the form of building foundations, cess pits or well structures. Little development appears to have occurred in this location since the property was demolished as evidenced by sandstone blocks identified within the ground's surface during the visual inspection and the presence of remnant mature pine plantings, indicating the potential for relatively intact subsurface deposits.

Therefore, works associated with the proposal may have a moderate to major adverse impact on the archaeological potential of the proposal area. An Historic (non-Aboriginal) Archaeological Assessment of the vacant lot immediately south of the petrol station and *Hydro Majestic* has been recommended to inform the detailed design and mitigate any potential impact to archaeological resources associated with Glenara Cottage.

An unexpected archaeological finds procedure has also been recommended as a management measure for areas of low archaeological potential.

9 RECOMMENDATIONS AND MITIGATION MEASURES

The following mitigation measures are recommended to ensure that the proposal would not cause any further impact to heritage items than outlined in this report. Failure to implement these measures may result in additional or inadvertent impact to heritage.

Recommendation 1: Detailed design

The detailed design should be developed and refined in consultation with either a heritage architect or a built heritage consultant. The detailed design should aim to further minimise the impact of the proposal, with particular reference to the pedestrian bridge through the use of appropriate form, proportion and materials. Bulk should be minimised, and new built forms should be clearly separate from existing heritage fabric. Where appropriate, the detailed design should also respond to existing and significant architectural detail, such as the architectural detailing of the station building, or the footbridge. Detailed design should be in accordance with appropriate Sydney Trains and TfNSW guidelines, including:

- *Railway Footbridges Heritage Conservation Strategy 2016*
- *Heritage Platforms Conservation Management Strategy 2015*
- *Heritage Technical Note: Installation of New Electrical and Data Services at Heritage Sites 2017*
- *Station Component Guide 2017*

Recommendation 2: Section 60 Application for Medlow Bath Railway Station (SHR No.01190)

A Section 60 Application would be required for proposed works within the SHR curtilage of Medlow Bath Railway Station. The Application must be granted prior to works commencing.

Recommendation 3: Historic (non-Aboriginal) Archaeological Assessment

An Historic (non-Aboriginal) Archaeological Assessment (HAA) should be undertaken on the Hydro Majestic land proposed for use for the alternate design arrangement for Bellevue Crescent known as Lots 3, 4, 5 and 20 of DP25570. The HAA should be undertaken by a suitably qualified archaeologist in accordance with the *Heritage Act 1977* and the Heritage NSW publication *Assessing Significance of Historical Archaeological Sites and Relics* (2009). The purpose of the HAA is to determine the nature, extent and significance of any archaeological resources associated with the former Glenara Cottage in this area and provide appropriate management recommendations in relation to the proposal.

Recommendation 4: Non-Aboriginal heritage awareness training

- a. Works within the proposal area are being undertaken in an area of heritage significance. Prior to works commencing, contractors shall be briefed as to the sensitive nature of the proposal area and informed of any recommended mitigation measures or controls required
- b. Non-Aboriginal heritage awareness training must be provided for all contractors and personnel prior to commencement of works to outline the identification of potential heritage items and associated procedures to be implemented in the event of the discovery of non-Aboriginal heritage materials, features or deposits (that is, unexpected finds), or the discovery of human remains.

Recommendation 5: Protection of significant fabric

Works should be undertaken with care. To avoid impact to significant fabric during the construction of the proposal, it is recommended:

- a. machinery should be placed with sufficient clearance to significant heritage structures to avoid any inadvertent harm to significant fabric or incidental damage from vibration as per the TfNSW

REPORT

recommended minimum working distances for vibration intensive plant from sensitive receiver (Table 9.1). In particular, care should be taken when working near:

- *Hydro Majestic's* stone fence
- *Medlow Bath Railway Station* platform structures, platform edges and footbridge
- *Former Post and Telegraph Store*
- *Urunga*
- *Melbourne House, Cosy Cot and Sheleagh Cottage*, in particular Lot 1 Great Western Highway
- *Sandstone Railway culvert*
- Archaeologically sensitive vacant land north of petrol station

b. Protection of significant fabric – *Hydro Majestic stone fence*

- i. protective barriers or fencing should be erected between the works corridor boundary and the *Hydro Majestic's* stone fence for the duration of works within the vicinity of this significant fabric to ensure no inadvertent harm occurs
- ii. machinery and works should be placed with sufficient clearance to significant fabric and associated protective barriers to avoid inadvertent harm from machinery or incidental damage from vibration
- iii. vibration monitoring of the stone fence should be put in place for the duration of works

c. Protection of significant fabric – *Sandstone Railway culvert*

- i. redundancy of the *Sandstone Railway culvert* should not include work to significant fabric
- ii. if closure or blocking of the culvert is required, these works should be undertaken in a manner that would not impact significant fabric
- iii. if work to significant fabric is required, this should be undertaken in consultation with either a heritage architect or heritage consultant, and be conducted in a manner that minimises harm as much as practicable

d. Protection of significant fabric – *bus shelter*

- i. measures should be put place to protect significant fabric of the *bus shelter* during its proposed removal and relocation
- ii. relocation position, and details of where and how it will be removed, stored and relocated, should be determined in consultation with Blue Mountains City Council
- iii. after relocation, conservation of the mural should be undertaken to prevent further loss, or to sympathetically reinstate missing portions

e. Protection of significant fabric – *advertising sign*

- i. if removal of the advertising sign is required for the proposal, it should be salvaged and relocated
- ii. relocation position, and details of where and how it will be removed, stored and relocated, should be determined in consultation with Blue Mountains City Council
- iii. if removal of the *advertising sign* is not required for the proposal, appropriate measures should be put in place to protect it during proposed works, such as the installation of protective barriers or fencing

f. Protection of significant fabric – potential archaeological site of former Glenara Cottage

- i. prior to use as an ancillary facility / stockpile area, the vacant land north of the petrol station should be covered with geotextile, or other suitable protective material, to ensure no inadvertent harm to potential archaeological resources occurs
- ii. no ground scraping, levelling or landscaping of this area should occur before, during or after the use of the area as an ancillary facility / stockpile area
- iii. this protection measure may not be required if a HAA does not identify any significant archaeological potential.

Table 9.1: TfNSW recommended minimum working distances (in metres) for vibration intensive plant from sensitive receiver

| Plant item | Rating / Description | Cosmetic damage (BS 7385) Light-framed structures | Cosmetic damage (DIN 4150) Heritage and other sensitive structures | Human response (EPA's Vibration guideline) |
|-------------------------|--------------------------------------|--|---|--|
| Vibratory Roller | < 50 kN (typically 1-2 tonnes) | 5 m | 14 m | 15 m to 20 m |
| | < 100 kN (typically 2-4 tonnes) | 6 m | 16 m | 20 m |
| | < 200 kN (typically 4-6 tonnes) | 12 m | 33 m | 40 m |
| | < 300 kN (typically 7-13 tonnes) | 15 m | 41 m | 100 m |
| | > 300 kN (typically 13-18 tonnes) | 20 m | 54 m | 100 m |
| | > 300 kN (> 18 tonnes) | 25 m | 68 m | 100 m |
| Small Hydraulic Hammer | (300 kg - 5 to 12 tonne excavator) | 2 m | 5 m | 7 m |
| Medium Hydraulic Hammer | (900 kg – 12 to 18 tonne excavator) | 7 m | 19 m | 23 m |
| Large Hydraulic Hammer | (1600 kg – 18 to 34 tonne excavator) | 22 m | 60 m | 73 m |
| Vibratory Pile Driver | Sheet piles | 20 m | 50 m | 100 m |
| Pile Boring | ≤ 800 mm | 2 m (nominal) | 40 m | 4 m |
| Jackhammer | Hand held | 1 m (nominal) | 2 m | 2 m |

Note: The minimum working distances are indicative and will vary depending on the particular item of plant, local geotechnical conditions and the dominant frequency of the construction vibration levels. They apply to cosmetic damage of typical light-framed residential buildings and heritage/fragile buildings and assume that construction vibration could include low frequency content with associated increased risk of cosmetic damage. Vibration monitoring is recommended to confirm the minimum working distances at specific sites. Additionally, further detailed analysis based on the frequency dependent guideline vibration levels in BS7385-2:1993 and DIN4150-3:2016 may be utilised in conjunction with site-specific measurements to derive alternative cosmetic damage objectives and minimum working distances. For heritage listed / fragile structures, specialist advice from an appropriately qualified structural engineer who is familiar with heritage structures is required to support any proposed relaxation of the initial cosmetic damage screening criterion. Any such relaxation shall be approved by Roads and Maritime or under the environmental license as relevant.

Recommendation 6: Protection and management of significant trees

A qualified arborist should be engaged to undertake an Arboricultural Impact Assessment of the proposal area, with a particular focus on trees associated with heritage items, *Hydro Majestic* (Blue Mountains LETP Item No.MB002), *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Item No.MB015) and *Medlow Bath Hydro Majestic original walking track complex (only the parts within the grounds of the Hydro Majestic)* (Blue Mountains LEP 2015 Item No.MB026) . Management and protection measures recommended in the Arboricultural Impact Assessment should be implemented accordingly to ensure the protection and management of significant trees throughout the implementation of the proposal.

Recommendation 7: Tree replacement

Trees removed as part of the proposal within the heritage curtilage of *Hydro Majestic* (Item No.MB002), *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Item No.MB015) or *Medlow Bath Hydro Majestic original walking track complex* (only the parts within the grounds of the *Hydro Majestic*) (Blue Mountains LEP 2015 Item No.MB026) should be replaced in a manner that is consistent with, and accurately reflect, the extent, nature and significance of the respective heritage item. The location, species and number of trees to be planted should be determined in consultation with the land owner, Blue Mountains City Council and a qualified arborist with reference to the identified heritage significance of the respective heritage item.

Recommendation 8: Protection and management of moveable heritage

All moveable heritage identified as part of this assessment is to be managed in accordance with a moveable heritage procedure. Moveable heritage identified on *Hydro Majestic* (Blue Mountains LETP Item No.MB002) land should be managed in accordance with Section 6.5, Conserving Moveable Heritage, in the *Hydro Majestic Hotel, Medlow Bath, Conservation Management Plan* (Graham Brooks and Associates 2010).

Recommendation 9: Archival photographic recording

Prior to construction, an archival photographic recording of the heritage items impacted by the proposed works is to be prepared in accordance with the NSW Heritage Division of the Department of Environment and Heritage guidelines titled "*Photographic Recording of Heritage Items using Film or Digital Capture*". The photographic should be prepared by a heritage consultant and must document significant heritage elements and items that will be impacted by the proposed works. The record should also document significant views and vistas as selected by the heritage consultant.

The archival recording should include, but is not limited to, the following:

- *Medlow Bath Railway Station Group* (SHR No.01190, TfNSW Section 170 SHI No. 4801011, Blue Mountains LEP 2015 Item No.MB003)
- *Hydro Majestic* (Blue Mountains LEP 2015 Item No.MB002)
- *Former Post and Telegraph Store* (Blue Mountains LEP 2015 Item No.MB008)
- *Avenue of trees* (formerly *Avenue of Radiata Pines*) (Blue Mountains LEP 2015 Item No.MB015)
- *Urunga* (Blue Mountains LEP 2015 Item No.MB017)
- *Melbourne House, Cosy Cot, Sheleagh Cottage* (Blue Mountains LEP 2015 Item No.MB019)
- *Medlow Bath Hydro Majestic original walking track complex* (only the parts within the grounds of the *Hydro Majestic*) (Blue Mountains LEP 2015 Item No.MB026)
- *Bus Shelter* (potential heritage item)
- *Sandstone Railway culvert* (potential heritage item)
- *Advertising sign* (potential heritage item)

Recommendation 10: Heritage interpretation

A heritage interpretation plan should be formulated and implemented in accordance with the Heritage NSW, *Interpreting Heritage Places and Items* (Heritage Office (former) 2005) as part of the proposed upgrade of the Great Western Highway. This is to be undertaken with the consent and co-operation of authorised owners or land managers and Blue Mountains City Council.

Heritage interpretation should communicate the history of Medlow Bath, with reference to its identified heritage items, and enable audiences to engage with the significance of these places and the wider Blue Mountains area. It should be integrated into the broader cultural heritage design and heritage interpretation strategy for the overall Great Western Highway Katoomba to Lithgow upgrade project, and pick up themes relevant to the overall Great Western Highway route as well as Medlow Bath.

Recommendation 11: Unexpected finds procedure

In the event that unexpected archaeological resources are identified in the course of the proposal, all work in the affected area should cease, the area should be cordoned off, and Heritage NSW should be notified, in accordance with Section 146 of the *Heritage Act 1977*. The TfNSW (2016) *Unexpected Heritage Finds Guideline* should be adhered to.

Recommendation 12: Further assessment required for any design modification

If the proposed works, or proposal area, are modified to those discussed in this report, additional heritage advice may be required to appropriately manage and mitigate any potential impacts caused by these changes.

10 REFERENCES

- ABC News 2011, 'Thousand lose power as winds lash NSW', *ABC News*, 5 July, available online at <https://www.abc.net.au/news/2011-07-05/thousands-lose-power-as-winds-lash-nsw/2783646>, accessed 29 June 2021.
- AECOM 2017, *New Intercity Fleet – Springwood to Lithgow Rail Corridor Modifications, Medlow Bath Station Statement of Heritage Impact*. Unpublished report to Transport for NSW.
- Artefact 2015, *Great Western Highway, Katoomba to Mount Victoria, Statement of Heritage Impact*. Unpublished report to Roads and Maritime Services.
- Blue Mountains City Council 2012, *Medlow Park, including Crown Reserve 33660 for Public Recreation: Draft Plan of Management for Public Exhibition*. Unpublished report.
- Conway, J. 1966, 'Blaxland, Gregory (1778-1853)', *Australian Dictionary of Biography*, National Centre of Biography, Australian National University, available online at <http://adb.anu.edu.au/biography/blaxland-gregory-1795/text2031>, accessed online 19 October 2020.
- Crowland, E.C. "The Story of the New South Wales Railways", *Royal Australian Historical Society Journal and Proceedings*, vol. 40, part 5, 1954, pp. 245-286.
- Department of Main Roads New South Wales 1976, *The Roadmakers: a history of main roads in New South Wales*, John Sands Pty Ltd, Sydney.
- Futurepast Heritage Consulting Pty Ltd 2013, 'Conservation Strategy for RailCorp Railway Residences'
- Graham Brooks & Associates 2010, *Hydro Majestic Hotel, 52-88 Great Western Highway, Medlow Bath Conservation Management Plan*.
- Heritage NSW 2020a *Medlow Bath Railway Station Group*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=5012100>, accessed 14 October 2020.
- Heritage NSW 2020b *Mb002: Hydro Majestic*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170280>, accessed 14 October 2020.
- Heritage NSW 2020c *Mb003: Medlow Bath Railway Station*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170281>, accessed 14 October 2020.
- Heritage NSW 2020d *Mb008: Post and Telegraph Store, Former*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170284>, accessed 14 October 2020.
- Heritage NSW 2020e *Mb010: St Luke's Anglican Church*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170286>, accessed 14 October 2020.
- Heritage NSW 2020f *Mb013: Horse Trough*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170355>, accessed 14 October 2020.
- Heritage NSW 2020g *Mb015: Avenue of Radiata Pines*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170357>, accessed 14 October 2020.
- Heritage NSW 2020h *Mb017: Urunga*, available online at <https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170290>, accessed 14 October 2020.

REPORT

Heritage NSW 2020i Mb019: *Melbourne House, Cosy Cot, Sheleagh Cottage*, available online at <<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170292>>, accessed 14 October 2020.

Heritage NSW 2021 Mb018: *House*, available online at <https://www.hms.heritage.nsw.gov.au/App/Item/ViewItem?itemId=1170291>, accessed 24 June 2021.

Hydro Majestic 2020, "Our History", *The Hydro Majestic*, available online at <<https://www.hydromajestic.com.au/home/our-history>>, accessed 26 October 2020.

Merriman, J. 2015. 'Bills Horse Troughs', *Blue Mountains Local Studies*, 10 November, available online at <<https://bluemlocalstudies.wordpress.com/2015/11/10/bills-horse-troughs/>>, accessed 26 October 2020.

National Museum of Australia (NMA) 2020, *Defining Moments: Blue Mountains Crossing*, National Museum of Australia, available online at <<https://www.nma.gov.au/defining-moments/resources/blue-mountains>>, accessed 19 October 2020.

NSW Rail 2020, *Station Names*, available online at <https://nswrail.net/library/station_names.php>, accessed 19 October 2020.

Smith, J. 2020, *The life and loves of Mark Foy Creator of the Hydro Majestic Hotel*, Den Fenella Press, Wentworth Falls.

Transport for NSW 2020, *Great Western Highway Upgrade, Katoomba to Lithgow Preliminary Environmental Investigation*.

Ozroads, "Great Western Highway History and Development" *Ozroads The Australian Roads Website*, available online at <<https://www.ozroads.com.au/NSW/Highways/GtWestern/history.htm>>, accessed 19 October 2020.

UNESCO 2020, *Greater Blue Mountains Area*, available online at <<https://whc.unesco.org/en/list/917/>>, accessed 14 October 2020.

Walsh, G.P. 1981, 'Foy, Mark (1865–1950)', *Australian Dictionary of Biography, National Centre of Biography*, Australian National University, <<https://adb.anu.edu.au/biography/foy-mark-6367/text10721>>, published first in hardcopy 1981, accessed online 29 June 2021

Appendix A Heritage listings

Greater Blue Mountains Area

[Description](#)[Maps](#)[Documents](#)[Gallery](#)[Video](#)[Indicators](#)

Greater Blue Mountains Area

The Greater Blue Mountains Area consists of 1.03 million ha of sandstone plateaux, escarpments and gorges dominated by temperate eucalypt forest. The site, comprised of eight protected areas, is noted for its representation of the evolutionary adaptation and diversification of the eucalypts in post-Gondwana isolation on the Australian continent. Ninety-one eucalypt taxa occur within the Greater Blue Mountains Area which is also outstanding for its exceptional expression of the structural and ecological diversity of the eucalypts associated with its wide range of habitats. The site provides significant representation of Australia's biodiversity with ten percent of the vascular flora as well as significant numbers of rare or threatened species, including endemic and evolutionary relict species, such as the Wollemi pine, which have persisted in highly-restricted microsites.

Description is available under license [CC-BY-SA IGO 3.0](#)

English French Arabic Chinese Russian

Spanish Japanese Dutch



Australia

S33 42 0 E150 0 0

Date of Inscription: 2000

Criteria: (ix)(x)

Property : 1,032,649 ha

Buffer zone: 86,200 ha

Ref: 917



Earthstar Geo... Powered by [Esri](#)

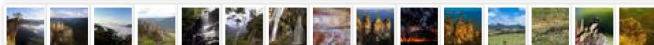
[Media](#) [News](#) [Links](#)

https://whc.unesco.org/pg_friendly_print.cfm?cid=31&id_site=917&

1/7



Blue Mountains, Three Sisters Hanging Rock © Tourism Australia



**State of Conservation (SOC)
by year**

2019 2004 2001

Outstanding Universal Value

Brief synthesis:

The Greater Blue Mountains Area (GBMA) is a deeply incised sandstone tableland that encompasses 1.03 million hectares of eucalypt-dominated landscape just inland from Sydney, Australia's largest city, in south-eastern Australia. Spread across eight adjacent conservation reserves, it constitutes one of the largest and most intact tracts of protected bushland in Australia. It also supports an exceptional representation of the taxonomic, physiognomic and ecological diversity that eucalypts have developed: an outstanding illustration of the evolution of plant life. A number of rare and endemic taxa, including relict flora such as the Wollemi pine, also occur here. Ongoing research continues to reveal the rich scientific value of the area as more species are discovered.

The geology and geomorphology of the property, which includes 300 metre cliffs, slot canyons and waterfalls, provides the physical conditions and visual backdrop to support these outstanding biological values. The property includes large areas of accessible wilderness in close proximity to 4.5 million people. Its exceptional biodiversity

https://whc.unesco.org/pg_friendly_print.cfm?cid=31&id_site=917&

2/7

values are complemented by numerous others, including indigenous and post-European-settlement cultural values, geodiversity, water production, wilderness, recreation and natural beauty.

Criterion (ix): The Greater Blue Mountains include outstanding and representative examples in a relatively small area of the evolution and adaptation of the genus *Eucalyptus* and eucalypt-dominated vegetation on the Australian continent. The site contains a wide and balanced representation of eucalypt habitats including wet and dry sclerophyll forests and mallee heathlands, as well as localised swamps, wetlands and grassland. It is a centre of diversification for the Australian scleromorphic flora, including significant aspects of eucalypt evolution and radiation. Representative examples of the dynamic processes in its eucalypt-dominated ecosystems cover the full range of interactions between eucalypts, understorey, fauna, environment and fire. The site includes primitive species of outstanding significance to the evolution of the earth's plant life, such as the highly restricted Wollemi pine (*Wollemia nobilis*) and the Blue Mountains pine (*Pherosphaera fitzgeraldii*). These are examples of ancient, relict species with Gondwanan affinities that have survived past climatic changes and demonstrate the highly unusual juxtaposition of Gondwanan taxa with the diverse scleromorphic flora.

Criterion (x): The site includes an outstanding diversity of habitats and plant communities that support its globally significant species and ecosystem diversity (152 plant families, 484 genera and c. 1,500 species). A significant proportion of the Australian continent's biodiversity, especially its scleromorphic flora, occur in the area. Plant families represented by exceptionally high levels of species diversity here include Myrtaceae (150 species), Fabaceae (149 species), and Proteaceae (77 species). Eucalypts (*Eucalyptus*, *Angophora* and *Corymbia*, all in the family Myrtaceae) which dominate the Australian continent are well represented by more than 90 species (13% of the global total). The genus *Acacia* (in the family Fabaceae) is represented by 64 species. The site includes primitive and relictual species with Gondwanan affinities (*Wollemia*, *Pherosphaera*, *Lomatia*,

https://whc.unesco.org/pg_friendly_print.cfm?cid=31&id_site=917&

3/7

Dracophyllum, *Acrophyllum*, *Podocarpus* and *Atkinsonia*) and supports many plants of conservation significance including 114 endemic species and 177 threatened species.

The diverse plant communities and habitats support more than 400 vertebrate taxa (of which 40 are threatened), comprising some 52 mammal, 63 reptile, over 30 frog and about one third (265 species) of Australia's bird species.

Charismatic vertebrates such as the platypus and echidna occur in the area. Although invertebrates are still poorly known, the area supports an estimated 120 butterfly and 4,000 moth species, and a rich cave invertebrate fauna (67 taxa).

Integrity

The seven adjacent national parks and single karst conservation reserve that comprise the GBMA are of sufficient size to protect the biota and ecosystem processes, although the boundary has several anomalies that reduce the effectiveness of its 1 million hectare size.

This is explained by historical patterns of clearing and private land ownership that preceded establishment of the parks. However parts of the convoluted boundary reflect topography, such as escarpments that act as barriers to potential adverse impacts from adjoining land. In addition, much of the property is largely protected by adjoining public lands of State Forests and State Conservation Areas. Additional regulatory mechanisms, such as the statutory wilderness designation of 65% of the property, the closed and protected catchment for the Warragamba Dam and additions to the conservation reserves that comprise the area further protect the integrity of the GBMA. Since listing, proposals for a second Sydney airport at Badgerys Creek, adjacent to the GBMA, have been abandoned.

Most of the natural bushland of the GBMA is of high wilderness quality and remains close to pristine. The plant communities and habitats occur almost entirely as an extensive, largely undisturbed matrix almost entirely free of structures, earthworks and other human intervention. Because of its size and connectivity with

other protected areas, the area will continue to play a vital role in providing opportunities for adaptation and shifts in range for all native plant and animal species within it, allowing essential ecological processes to continue. The area's integrity depends upon the complexity of its geological structure, geomorphology and water systems, which have created the conditions for the evolution of its outstanding biodiversity and which require the same level of protection.

An understanding of the cultural context of the GBMA is fundamental to the protection of its integrity. Aboriginal people from six language groups, through ongoing practices that reflect both traditional and contemporary presence, continue to have a custodial relationship with the area. Occupation sites and rock art provide physical evidence of the longevity of the strong Aboriginal cultural connections with the land. The conservation of these associations, together with the elements of the property's natural beauty, contributes to its integrity.

Protection and management requirements

The GBMA is protected and managed under legislation of both the Commonwealth of Australia and the State of New South Wales. All World Heritage properties in Australia are 'matters of national environmental significance' protected and managed under national legislation, the *Environment Protection and Biodiversity Conservation Act 1999*. This Act is the statutory instrument for implementing Australia's obligations under a number of multilateral environmental agreements including the World Heritage Convention. By law, any action that has, will have or is likely to have a significant impact on the World Heritage values of a World Heritage property must be referred to the responsible Minister for consideration. Substantial penalties apply for taking such an action without approval. Once a heritage place is listed, the Act provides for the preparation of management plans which set out the significant heritage aspects of the place and how the values of the site will be managed.

Importantly, this Act also aims to protect matters of national environmental significance, such as World

Heritage properties, from impacts even if they originate outside the property or if the values of the property are mobile (as in fauna). It thus forms an additional layer of protection designed to protect values of World Heritage properties from external impacts. In 2007, the GBMA was added to the National Heritage List, in recognition of its national heritage significance under the Act.

A single State government agency, the New South Wales Office of Environment and Heritage, manages the area. All the reserves that comprise the GBMA are subject to the *National Parks and Wildlife Act 1974* and the *Wilderness Act 1987*. Other relevant legislation includes the *Threatened Species Conservation Act 1995*, the *Environmental Planning and Assessment Act 1979*, the *Sydney Water Catchment Management Act 1998* and the *Heritage Act 1977*.

At the time of nomination statutory management plans for the constituent reserves of the GBMA were in place or in preparation, and these are reviewed every 7-10 years.

Currently all management plans have been gazetted, and those for three component reserves (Wollemi, Blue Mountains, and Kanangra-Boyd National Parks, which constitute 80% of the property) are under revision for greater emphasis on the protection of identified values.

An over-arching Strategic Plan for the property provides a framework for its integrated management, protection, interpretation and monitoring.

The major management challenges identified in the Strategic Plan fall into six categories: uncontrolled or inappropriate use of fire; inappropriate recreation and tourism activities, including the development of tourism infrastructure, due to increasing Australian and overseas visitor pressure and commercial ventures; invasion by pest species including weeds and feral animals; loss of biodiversity and geodiversity at all levels; impacts of human-enhanced climate change; and lack of understanding of heritage values.

The set of key management objectives set out in the Strategic Plan provides the philosophical basis for the management of the area and guidance for operational

strategies, in accordance with requirements of the World Heritage Convention and its Operational Guidelines.

These objectives are also consistent with the Australian World Heritage management principles, contained in regulations under the *Environmental Protection and Biodiversity Conservation Act*.


Media

-  [Greater Blue Mountains Area \(UNESCO/NHK\)](#)

News (3)

- [World Heritage Centre Statement on the Greater Blue Mountains Area](#) 07-Jun-2017
- [Climate change threatens UNESCO World Heritage sites](#) 10-Apr-2007
- [World Heritage Committee Inscribes 61 New Sites on World Heritage List](#) 30-Nov-2000

Links

-  [View photos from OUR PLACE the World Heritage collection](#)
- [Blue Mountains World Heritage Institute](#)
- [Blue Mountains Web](#)
- [Blue Mountains National Park \(NSW National Parks & Wildlife Service\)](#)
- [Greater Blue Mountains \(Department of Sustainability, Environment, Water, Population and Communities \)](#)
- [Protectedplanet.net](#)

Medlow Bath Railway Station group

Item details

Name of item:

Medlow Bath Railway Station group

Type of item:

Complex / Group

Group/Collection:

Transport - Rail

Category:

Railway Platform/ Station

Location:

Lat: -33.6718342307 Long: 150.2800811890

Primary address:

Great Western Highway, Medlow Bath, nsw 2780

Local govt. area:

Blue Mountains

Local Aboriginal Land Council:

Deerubbin

Boundary:

West: RailCorp property boundary to Great Western Highway; East: RailCorp property boundary to Railway Parade; North: Northern edge of the footbridge; South: 5 metres from the south end of the level crossing.

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|-----------------------|-------------|----------------|--------|--------|-------------------|
| Great Western Highway | Medlow Bath | Blue Mountains | | | Primary Address |
| Main Western railway | Medlow Bath | Blue Mountains | | | Alternate Address |

Owner/s

| Organisation Name | Owner Category | Date Ownership Updated |
|-------------------|------------------|------------------------|
| RailCorp | State Government | 05 Nov 98 |

Statement of significance:

Medlow Bath Railway Station is significant as part of the early construction phase of railway line duplication on the upper Blue Mountains demonstrating the technological and engineering achievements in railway construction at the beginning of the 1900s. Constructed in anticipation of a boom period in the mountains particularly in connection with large holiday resorts such as the Hydro-Majestic Hotel, Medlow Bath station building is a good example of a Federation free classical railway station. The station building demonstrates typical architectural elements of the standard Federation style island platform buildings that were built between Penrith and Lithgow when the line was duplicated.

Date significance updated: 15 Apr 13

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Designer/Maker:

NSW Government Railways

Builder/Maker:

Unknown

Construction years:

1902-1922

Physical description:**BUILDINGS**

Station Building - type 11, island station building 'A8' design, brick (1902)

Signal Room - gabled roof, timber (c1922)

STRUCTURES

Island Platform - brick faced (1902)

Footbridge (1901, 1994)

STATION BUILDING (1902)

External: Constructed of face brick with corrugated metal gabled roof extending as an awning to both platforms, the Medlow Bath station building is an early phase island building in standard 'A8' Federation style design. It features 6 bays with linear arrangement along the platform with tuckpointed red brickwork with engaged piers between the bays. Other features include rendered and moulded two rows of string courses, moulded cornice, timber framed windows and doors with contrasting decorative trims and sills, standard iron brackets over decorative corbels supporting ample platform awnings, fretted timber work to both ends of awnings and gable ends, timber finials to gable apex, tall corbelled chimneys, timber framed double-hung windows with multi-paned and coloured upper sashes, and timber door openings with multi-paned fanlights with coloured glazing.

Internal: Medlow Bath Station is an unattended station and its interiors are in an abandoned state. The floor layout of the building comprises a booking office, waiting room, ladies waiting and toilets and male toilets with access from the south end of the building. The interiors generally feature custom orb ceilings with ceiling roses, fireplaces with no grates, timber floor boards to main rooms and tiling to toilets, beaded dado line and timber bead style moulded cornices. Toilet fittings are modern.

SIGNAL ROOM (c1922)

External: A small timber building at the booking office end of the station on axis with the platform. Historical evidence suggest that this weatherboard building covered an interlocking frame, which was originally erected as an open frame. It appears to be used as a store room after being taken out of service in 1957. It has a steep gabled corrugated metal roof, rusticated timber boarding with small four-paned windows on three elevations, and a four-panelled timber door with timber awning on the south elevation.

Internal: Access to the interior of the signal room was not available (2009).

ISLAND PLATFORM (1902)

A typical island platform running north-south, with the buildings located approximately at the centre. The platform is brick faced with a concrete deck and asphalt finish. Two raised round shaped slabs are located to the south of the station building probably for access to the services below the platform. A number of garden beds along the axis of the platform enhance the setting of the station. Modern platform furniture including light fittings, signage, timber bench seating and aluminium palisade fencing at both ends of the platform are other features along the platform.

FOOTBRIDGE (1901, 1994)

A standard concrete slab structure supported on original brick abutments and two steel trestles with new stairs to the platform and bridge with new concrete deck over the tracks spanning between the Great Western Highway and Railway Parade. The footbridge marks the northern end of the station. A concrete level crossing with relatively new fabric is also located on the southern end of the station. 1994 metal balustrades provides safety along the edges of the stairs and the bridge.

MOVEABLE ITEMS

Two timber bench style seats in the waiting room.

LANDSCAPE FEATURES

Other than small plantings along the platform no notable landscape features have been identified. The existing plantings are not considered significant.

Physical condition and/or

Archaeological potential:

Station Building - Good externally, however; internally it is in poor condition.

Signal Room - Good externally, internal condition unknown

Island Platform - Very Good

Footbridge - Very Good

Date condition updated: 03 Dec 08

Modifications and dates:

c1922 - 1909 interlocking frame covered by timber Signal box

1926 - additional siding built

1942 - platform extended at the Sydney end

1957 - line electrified

1994 - footbridge upgraded with new deck, stepway, and the superstructure cleaned and repainted.

Current use:

Railway Station

Former use:

Railway Station, Signal Box

History

Historical notes:

The single track main line from Katoomba to Blackheath was opened in 1868. A halt stop was established at Medlow Bath in 1881. Its initial name was Brown Siding, because of Brown's pioneering sawmill close by in Railway Parade. The station became Medlow in 1883 and Medlow Bath in 1903 with the opening of Mark Foy's Hydro.

The 1881 platform was 30 metres (100 feet) long and was situated on the down side of the single line where the present station is located. Additions to the station buildings were made in 1899, but duplication of the line in 1902 prompted the replacement of the original platform by the present island platform. The existing buildings date from that time and like most stations between Emu Plains and Lithgow, Medlow Bath received a standard Federation style brick building.

Also on the platform is a small timber signal box. An open interlocking frame was erected on the platform in 1909 but evidence suggests that the frame was not covered by the existing structure until c1922. It was taken out of service in 1957 but continued to be used as a store. It is now a rare example of a separate platform level signal box.

An additional siding was built in 1926 and in 1942 the platform was extended at the Sydney end.

The gatekeepers cottage is now privately owned.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|---|---|
| 3. Economy-Developing local, regional and national economies | Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements | (none)- |
| 3. Economy-Developing local, regional and national economies | Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements | Building and maintaining the public railway system- |
| 7. Governing-Government | Government and Administration-Activities associated with the governance of local areas, regions, the State and the nation, and the administration of public programs - includes both principled and corrupt activities. | Developing roles for government - building and administering rail networks- |

Assessment of significance

SHR Criteria a)

[Historical significance]

Medlow Bath Station Group is of historical significance as part of the early construction phase of railway line duplication on the upper Blue Mountains demonstrating the technological and engineering achievements in railway construction at the beginning of the 1900s. It was built in anticipation of a boom period in the mountains, particularly in connection with large holiday resorts such as the nearby Hydro-Majestic Hotel.

SHR Criteria c)

[Aesthetic significance]

Medlow Bath station building is a good example of a standard design island platform building and demonstrates typical architectural elements of Federation period standard buildings that were built between Penrith and Lithgow following the duplication of the railway line. It maintains its overall architectural quality and setting.

SHR Criteria d)

10/14/2020

Medlow Bath Railway Station group | Heritage NSW

[Social significance]

The place has the potential to contribute to the local community's sense of place, and can provide a connection to the local community's past.

SHR Criteria f)

[Rarity]

The timber signal room is a rare example of a separate platform level signal box as the majority of the signal rooms along Blue Mountains Line are incorporated into the main station building.

SHR Criteria g)

[Representativeness]

The station building is one of the early examples of a large number of standard railway designs that were commonly used in the 1910s-20s after a decade from the construction of Medlow Bath station building. The 1901 superstructure of the footbridge is a typical example of standard steel beam structure supported on trestles and brick abutments with later concrete deck and steps.

Integrity/Intactness:

The station building has a high degree of integrity externally, however; the interiors have lost their intactness due to the poor condition. The timber signal room is intact. The footbridge is relatively intact as it retains its original steel superstructure.

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Procedures /Exemptions

| Section of act | Description | Title | Comments | Action date |
|----------------|-------------------------|---------------------|--|-------------|
| 57(2) | Exemption to allow work | Standard Exemptions | <p>SCHEDULE OF STANDARD EXEMPTIONS HERITAGE ACT 1977</p> <p>Notice of Order Under Section 57 (2) of the Heritage Act 1977</p> <p>I, the Minister for Planning, pursuant to subsection 57(2) of the Heritage Act 1977, on the recommendation of the Heritage Council of New South Wales, do by this Order:</p> <p>1. revoke the Schedule of Exemptions to subsection 57(1) of the Heritage Act made under subsection 57(2) and published in the Government Gazette on 22 February 2008; and</p> <p>2. grant standard exemptions from subsection 57(1) of the Heritage Act 1977, described in the Schedule attached.</p> <p>FRANK SARTOR Minister for Planning Sydney, 11 July 2008</p> <p>To view the schedule click on the Standard Exemptions for Works Requiring Heritage Council Approval link below.</p> | Sep 5 2008 |

 [Standard exemptions](#) for works requiring Heritage Council approval

Listings

| Heritage Listing | Listing | Listing | Gazette | Gazette | Gazette |
|------------------|---------|---------|---------|---------|---------|
|------------------|---------|---------|---------|---------|---------|

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=5012100>

5/6

10/14/2020

Medlow Bath Railway Station group | Heritage NSW

| | Title | Number | Date | Number | Page |
|--|-------|--------|-----------|--------|------|
| Heritage Act - State Heritage Register | | 01190 | 02 Apr 99 | 27 | 1546 |
| Heritage Act - s170 NSW State agency heritage register | | | | | |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|---------|---------------------|------|-----------------------------------|------------------------------|
| Tourism | Attraction Homepage | 2007 | Medlow Bath Railway Station group | View details |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Heritage Office

Database number:

5012100

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb003 : Medlow Bath Railway Station

Item details

Name of item:

Mb003 : Medlow Bath Railway Station

Primary address:

Great Western Highway, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|-----------------------|-------------|----------------|--------|--------|-----------------|
| Great Western Highway | Medlow Bath | Blue Mountains | | | Primary Address |

Statement of significance:

The Medlow Bath Railway Station is one of a group of stations which are associated with the construction and duplication of the railway line across the Blue Mountains.

The station is a representative example of a Federation free classical railway station

Date significance updated: 07 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1902-1902

Physical description:

Island platform running north south with main building towards south end and smaller building north of main building.

Main Building:

Gabled 6 bay building on axis of island platform with awnings to east and west sides supported on curved steel brackets on stone consoles.

Corrugated steel roof

Stop chamfered purlins to awning

Tuckpointed red brick walls with engaged piers between bays

Sandstone splay course to plinth, 2 no. moulded sandstone string courses and moulded stone cornice.

Stone architraves to window above upper string course level

16 over 1 pane double hung windows with stone sills

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170281>

1/4

10/14/2020

Mb003 : Medlow Bath Railway Station | Heritage NSW

Lavatory windows fixed
Doors with 8 pane toplights

North Building:

Small gabled building on axis with platform, located north of main building
Corrugated steel roof
Beaded barge boards to gables
Weatherboard cladding
2 over 2 pane double hung windows in east and north walls
6 pane horizontal sliding window in west wall
Door in south wall

Physical condition and/or**Archaeological potential:**

Main Building: Good

Date condition updated: 08 Feb 00

Modifications and dates:

Main Building:

Doors replaced with flush doors
Some windows modified

North Building:

Skillion awning over door
Flush door

Current use:

Railway Station

Former use:

Railway Station

History

Historical notes:

Although the railway line from Katoomba to Blackheath was completed in 1868, no halt was established at Medlow Bath until 1881. Its initial name was Brown Siding, because of Brown's pioneering sawmill close by in Railway Parade, but became Medlow in 1883 and Medlow Bath in 1903 (Kaldy 6,67).

The 1881 platform was 30 metres (100 feet) long and was situated on the down side of the single line where the present station is located. Additions to the station buildings were made in 1899 (Davies III 137), but duplication of the line in 1902 prompted the replacement of the original platform by the present island platform. With the opening of Mark Foy's Hydro in 1904, the halt became much busier and in 1909 a signal box was constructed along with a main crossover line. An additional siding was built in 1926 and in 1942 the platform was extended at the Sydney end, with some consequential rearrangement of the crossovers (Singleton 17).

Historic themes

| | | |
|----------------------------------|------------------------------|--------------|
| Australian theme (abbrev) | New South Wales theme | Local |
|----------------------------------|------------------------------|--------------|

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170281>

2/4

10/14/2020

Mb003 : Medlow Bath Railway Station | Heritage NSW

| | | |
|--|--|--------------|
| | | theme |
| 3. Economy-Developing local, regional and national economies | Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements | (none)- |

Assessment of significance

SHR Criteria a)

[Historical significance]

The Medlow Bath Railway Station is one of a group of stations rebuilt in 1902 when the rail line was duplicated.

SHR Criteria c)

[Aesthetic significance]

The station is a representative example of a Federation free classical railway station built to a standard pattern used for the majority of stations on the Blue Mountains railway following the duplication of the railway line.

Integrity/Intactness:

Main Building: High

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan2005 | MB003 | 07 Oct 05 | 122 | |
| Heritage study | | MB003 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB003 | Croft & Associates Pty Ltd & Meredith Walker | | Y e s |
| Heritage Study Review, Blue Mountains | 1992 | MB003 | Tropman and Tropman | | Y e s |
| Blue Mountains Heritage Register Review | 1999 | MB003 | Jack, R. I. for University of Sydney | RIJ & PH | Y e s |
| Technical Audit BM Heritage Register | 2008 | MB003 | Blue Mountains City Council | City Planning Branch | N o |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|---------|------------------|------|--|----------------|
| Written | Davies, Paul | 1978 | A History of New South Wales Railway Architecture 1890-1915 | |
| Written | Kaldy, Elaine | 1983 | Medlow 1883 and Now | |
| Written | Singleton, C. C. | 1949 | Australasian Railway and Locomotive Historical Society Bulletin, Vol. 23, No 142, August | |

Note: internet links may be to web pages, documents or images.

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170281>

3/4

10/14/2020

Mb003 : Medlow Bath Railway Station | Heritage NSW



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170281

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb002 : Hydro Majestic

Item details

Name of item:

Mb002 : Hydro Majestic

Other name/s:

Belgravia Hydropathic Hotel

Primary address:

52-88 Great Western Highway, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|-----------------------------|-------------|----------------|--------|--------|-----------------|
| 52-88 Great Western Highway | Medlow Bath | Blue Mountains | | | Primary Address |

Statement of significance:

The grandest of the grand hotels in the mountains, the Hydro has state significance as a pioneering spa resort with advanced facilities for the health and pleasure of guests. The century and more of use as a hotel, capitalising on one of the finest situations in the mountains, is also of state significance. The Hydro Majestic Hotel is a unique overlay of hotel building styles including the pre-fabricated Casino and Federation free-style Reception buildings and the art deco Hargravia, Belgravia and main wings and the federation free classical south wing. The hotel also includes a number of freestanding buildings with a unity of styling and detailing such as the north bunkhouse, toilet block and rear of the Road Bar.

The arrangement of buildings along the ridge parallel to the Great Western Highway with the distinctive street fencing and row of mature radiata pinus trees quickly became, and remains, a significant landmark on the road through the Blue Mountains.

Some individual elements including the Casino and Reception buildings are fine examples of Federation free style architecture.

The tennis courts have a rare quality with their rustic stone walling and location on the edge of the ridge.

The unusual feature of a prefabricated imported casino which became a showpiece for some of the greatest singers of the Edwardian period, the art collection and the cuisine further enhance the social significance of the Hydro.

Technical interest attaches to the remains of the flying fox into the Megalong and the symbiosis between the hotel and valley below has remained a significant element in the Hydro's success.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170280>

1/12

by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1904-

Physical description:

The Hydro Majestic Hotel consists of a series of buildings stretching north-south along the edge of a steep slope overlooking the Megalong Valley. The main hotel buildings in the centre are connected with a long enclosed promenade along the western side.

From north to south the buildings are;

A free standing residence (north residence)

the Hydro Road Bar, on the Great Western Highway boundary

a lavatory block to the west of the Hydro Road Bar

Tennis Courts

Belgravia Wing

Main Block,

A wishing well is to the north of the Main Block.

Casino,

Reception,

Hargravia Wing,

South Wing.

The dining room is on the north side of the South Wing.

The two bunkhouses are freestanding buildings at the south end of the site, close to the Great Western Highway.

West of the bunkhouses is a freestanding plant building. Service buildings and kitchens are located to the south of the dining room (east of the south wing)

The Great Western Highway boundary of the site is marked by a fence of rockfaced sandstone piers with turned sandstone balusters and sandstone cappings. Gateposts are taller rockfaced sandstone piers.

A row of radiata pines are planted inside the fence, complementing those on the opposite side of the Great Western Highway.

A croquet lawn is located to the east of the Belgravia Wing and the tennis court.

A swimming pool is located to the east of the Hargravia Wing.

Walking paths leading down to the Megalong Valley are located to the west of the hotel.

North residence:

A gabled 2 storey building with a hipped roofed wing on the west (rear) side, possibly built on the remains of the original Belgravia hotel.

The roof is corrugated steel with exposed rafters.

The walls are of cavity brickwork with sandstone sills and sandstone footings.

A stair with a catslide roof is on the east side

Paired framed and sheeted 1/2 glazed doors are on the south side.

A triangular bay window is in the west wall of the first floor.

Hydro Road Bar:

The Hydro Road Bar has a single storey octagonal pavilion at the core. The pavilion has turned timber posts and an asbestos shingled roof with terracotta ridging and a ball finial. The pavilion has been extended south along the highway frontage and a rendered crenellated, parapetted façade to the Great western Highway, returning to the north and south has been added. The rear (west) walls are lined with rusticated weatherboards and have large picture windows.

The east façade has recessed entries in breakfronts towards each end. The entries are through paired glazed doors with sidelights.

The roof over the north and south extensions is a corrugated steel skillion.

Lavatory Building:

A single storey pyramidal roof building with an asbestos shingled roof and terracotta ridging set at 45 degrees to the adjacent buildings. The southeast wall is set back from the line of the eaves to allow a walkway to the ledged and sheeted lavatory doors. The west corner of the building is open.

The walls of the building are painted brick.

Tennis courts:

Two tennis courts set out end to end with a sandstone wall around the east and north sides. The sandstone wall is partly sparrowpecked and partly eroded in the manner of a romantic ruin. The wall has arched openings and the entry to the south court is recessed towards the court. Some of the wall has brick merlons.

A raked retaining wall is on the west side of the courts.

Belgravia Wing:

A rendered two storey building in the inter-war art deco style with a crenellated parapet wall entered through a breakfront in the centre. The breakfront has rounded corners, is taller than the side wings and has a straight parapet.

The entry doors are recessed into the breakfront and are a pair of glazed doors.

A vertical window of glass bricks is above the entry doors and has reeded reveals.

The windows to the hotel rooms are aluminium framed.

A stair at the south end of the building has a rendered spandrel.

Wishing Well:

A wishing well with a dry stone base and an asbestos shingled roof with terracotta ridging supported on steel posts and timber roof framing.

Main Block:

A three storey rendered building in the inter-war art deco style with a crenellated parapet and horizontal banding.

The building has timber hopper windows to the upper floors.

The central entry is through a semi-circular parapeted tower element, supported on the ground floor on circular columns.

The entry doors are glazed with a toplight and sidelights.

A stair at the south end of the building has a rendered spandrel.

Casino:

The casino is a single storey building with a large central domed roof and verandah to the east, south and north. The roof has a central lantern with a lead roof and bracketed eaves.

The roof to the dome is zinc shingles. The chimneys, to the north and south of the dome are rendered with moulded corbels. The verandah has a single slope roof with a parapetted wall to the east, returning one bay to the west.

The east wall is painted brick with sandstone arches. The parapet is decorated with pediments at each end and a balustrade. The walls are flemish bond brickwork

Central paired doors on the east side have carvings and a toplight. The windows either side of the door are in the form of french doors with a fixed panel below.

The west side of the Casino has a large curved front with a wide, plain eaves. Picture windows overlook the Megalong Valley.

The Casino is connected to the Reception by a skillion roof walkway with a crenellated parapet to the west. The east wall of the walkway is rendered. A hipped roof addition at the south end of the east side has brick piers infilled with casement windows and toplights.

Reception:

A single storey gabled building on a north-south axis with a hipped roofed porte cochere (added c. 1920s) centred on the east side. The porte-cochere has paired face brick piers. The east end of the building has a smaller gabled breakfront.

The building has a terracotta tiled roof. Chimneys at each end of the building are spatterdash with a three brick corbel.

The walls of the building are painted brick with a spatterdash frieze and a rendered base. The gable ends are shingled.

The entry doors are glazed with a decorative brick surround. Pairs of french doors either side of the porte-cochere have metal hoods.

The west side of the building has a crenellated parapet and arched windows with rendered sills. A terrace with a steel balustrade and rendered base is on the east side overlooking the Megalong Valley

Hargravia Wing:

A two storey rendered building with crenellated parapets. The east façade has turrets at each end and a central element with the name "HARGRAVIA 1903". A bullnosed verandah is on the first floor and a wide skillion with a terracotta tiled roof extends east of the verandah.

The east entry is a central circular breakfront to the ground floor skillion with a metal tray roof supported on square posts with turned timber brackets and a turned frieze. The first floor verandah has a glazed infill.

The west elevation has arched openings with rendered architraves and weatherboard spandrels on the ground floor. The windows to the openings are sets of 3 double hung windows. First floor windows are double hung with label moulds.

Dining Room:

A single storey building with rendered parapet walls and a crenellated parapet. Picture windows open to the east. A central breakfront on the east wall marks the chimney.

South Wing:

A two storey building with rendered walls and a corrugated steel mansard roof. The north end of the building is built on an exposed rock outcrop.

The wing has double hung windows with label moulds.

The west wall has arched openings to the ground floor with sets of three double hung windows and boarded timber spandrels below.

North Bunkhouse:

A long hipped roof building running north-south close to the Great Western Highway with an asbestos shingled roof and terracotta ridging. A small gable is at the north and south ends of the east elevation.

A central entry on the east side had a gabled roof (now removed)) and has a pair of 10 pane glazed doors.

The walls are rendered to the east, corrugated steel to the west.

Windows on the east side are casement with toplight and to the west are 2 over 2 pane

10/14/2020

Mb002 : Hydro Majestic | Heritage NSW

double hung.

South Bunkhouse:

A gabled fibro building with a corrugated asbestos roof with a skillion on the centre of the north side. High level 4 pane windows are on the west side of the building.

Plant Building:

A 2 storey gabled face brick building. The main gable is on an north-south axis. Single storey gabled wings are at the north and south ends of the east side. A skillion is on the west side.

The roof is corrugated steel with plain bargeboards and exposed rafters.

The walls are of English bond brickwork. The north single storey wing has corrugated walls to the east and south, and the east end of the north wall.

A bullseye opening in the gables is bricked in.

Framed and sheeted doors open into the north and south walls.

Physical condition and/or

Archaeological potential:

Good to Poor - See Further Comments after construction dates

Date condition updated: 07 Feb 00

Modifications and dates:

North residence:

Skillion roof over east stair

Hydro Road Bar:

Lavatory Building:

Corrugated sheeting around north corner

Lattice screen on southeast side.

Tennis courts:

Belgravia Wing:

Wishing Well:

Main Block:

Casino:

Reception:

Hargravia Wing:

4 panel hotel room doors with toplights on the east side.

Dining Room:

South Wing:

Balconies on steel brackets to second floor rooms on west side.

North Bunkhouse:

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170280>

6/12

10/14/2020

Mb002 : Hydro Majestic | Heritage NSW

South Bunkhouse:

Plant Building:

Steel and concrete stair on east side

Further information:

Construction Dates:

North residence: c.1922

Hydro Road Bar: c.1900 east façade c.1920s

Lavatory Building: c.1900

Tennis courts: c.1900

Belgravia Wing: c.1923

Wishing Well: c.1900

Main Block: c.1930s

Casino: c.1904

Reception: c.1900

Hargravia Wing: 1903

Dining Room: c.1920s

East Wing: c.1910s

North Bunkhouse: c.1900

South Bunkhouse: c.1930s

Plant Building: c.1900

Condition:

North residence: Reasonable

Hydro Road Bar: Reasonable

Lavatory Building: Fair

Tennis courts: Good

Belgravia Wing: Good

Wishing Well: Good

Main Block: Good

Casino: Good

Reception: Good

Hargravia Wing: Good

Dining Room: Good

East Wing: Good

North Bunkhouse: Poor

South Bunkhouse: Poor

Plant Building: Poor

Current Uses:

North residence: Residence

Hydro Road Bar: Bar/function space

Lavatory Building: Storage

Tennis courts: Tennis courts

Belgravia Wing: Hotel accommodation

Wishing Well: Wishing well

Main Block: Hotel accommodation

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170280>

7/12

10/14/2020

Mb002 : Hydro Majestic | Heritage NSW

Casino: Hotel public space
Reception: Reception
Hargravia Wing: Hotel accommodation
Dining Room: Dining room
East Wing: Hotel accommodation
North Bunkhouse: Unused
South Bunkhouse: Unused
Plant Building: Unused

Previous Uses:

North residence: Residence
Hydro Road Bar: Bar
Lavatory Building: Lavatories/Storage
Tennis courts: Tennis courts
Belgravia Wing: Hotel accommodation
Wishing Well: Wishing well
Main Block: Hotel accommodation
Casino: Hotel public space
Reception: Reception
Hargravia Wing: Hotel accommodation
Dining Room: Dining room
East Wing: Hotel accommodation
North Bunkhouse: Staff accommodation
South Bunkhouse: Staff accommodation
Plant Building: Electricity plant

Intactness:

North residence: Reasonable
Hydro Road Bar: Reasonable
Lavatory Building: Reasonable
Tennis courts: High
Belgravia Wing: High
Wishing Well: Good
Main Block: High
Casino: High
Reception: High
Hargravia Wing: High
Dining Room: Medium
East Wing: High
North Bunkhouse: Reasonable
South Bunkhouse: Reasonable
Plant Building: Low

Current use:

Hotel

Former use:

Hotel; Spa

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170280>

8/12

History

Historical notes:

The iconic hotel at Medlow Bath was created by Mark Foy, the Sydney businessman, sportsman and playboy (1865 - 1950) by bringing together three existing buildings and by developing the complex around them from 1904 onwards. The three earlier buildings were:

1. the country retreat of W.H. Hargraves, registrar in Equity and a trustee of the Australian Museum in Sydney, son of the man who claimed credit for the discovery of gold in New South Wales in 1851 (Johns 120; Mitchell 346-7). The single-storeyed house, with elaborate tree and shrub plantings, was bought by Mark Foy in 1901 and developed into the Hargravia section of the Hydro (Kaldy 20).
2. the existing hotel called the Belgravia, to the north of Hargraves' house. The Belgravia had been opened in 1891 by Mr and Mrs Ellis and was acquired by Mark Foy in 1903 (Silvey 43,98; Hungerford and Donald 103).
3. a cottage owned by Alfred Tucker, whose widow later ran the Wonderland Park guesthouse to the north of the gatekeeper's cottage (Kaldy 30).

In 1904 Mark Foy opened his hydropathic establishment, advertising cures for nervous, alimentary, respiratory and circulatory ailments, but excluding sufferers from infectious diseases such as tuberculosis and those with mental illness. The Hydro was, however, from the start advertised as 'the most enjoyable place to spend one's holidays' (Kaldy 33-4).

A prefabricated casino of striking aspect was imported from Chicago and erected between Hargravia and Belgravia, while the famous picture gallery joined the buildings together, giving superb views over the Megalong Valley. There was initially in 1904 a resident doctor, Georg Bauer, from a Swiss spa, to add prestige, but by the end of the decade the family hotel had triumphed over the hydropathy (Kaldy 36; Silvey 43).

Entertainment in the casino was lavish, with international stars such as Nellie Melba and Dame Clara Butt singing there on a number of occasions (Walsh 571). The kitchens were supplied from Foy's farm below in the Megalong, with produce brought up on a flying-fox. A stable of horses in the Megalong gave guests the chance to explore the valley and Mark Foy's famous fleet of motor-cars (he was a pioneer motorist) took them on more extended trips, particularly to Jenolan Caves (cf. MB 003).

In 1922 the northern part of the hotel, including Belgravia and the picture gallery, was severely damaged in a bush-fire, but the lost buildings were replaced and the hotel recovered. During World War II it was used as a convalescent hospital for American servicemen, who did some violence to the fabric, but it again recovered after the war (Silvey 43).

When Mark Foy finally died in 1950 he stipulated in his will that an extraordinarily lavish tomb be constructed for him at Medlow Bath, but the Equity Court released his family from obligation and he is buried at South Head in Sydney (Walsh 571).

The Hydro has continued as a grand hotel on a fabulous site and has recently been the subject of a Conservation Management Plan and subsequent works of extensive renovation.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|---|-------------|
| 3. Economy-Developing local, regional and national economies | Health-Activities associated with preparing and providing medical assistance and/or promoting or maintaining the well being of humans | (none)- |
| 8. Culture-Developing cultural institutions and ways of life | Leisure-Activities associated with recreation and relaxation | (none)- |
| 9. Phases of Life-Marking the phases of life | Persons-Activities of, and associations with, identifiable individuals, families and communal groups | (none)- |

Assessment of significance

SHR Criteria a)

[Historical significance]

The grandest of the grand hotels in the mountains, the Hydro has state significance as a pioneering spa resort with advanced facilities for the health and pleasure of guests. The century and more of use as a hotel, capitalising on one of the finest situations in the mountains, is also of state significance.

SHR Criteria c)

[Aesthetic significance]

The Hydro Majestic Hotel is a unique overlay of hotel building styles including the pre-fabricated Casino and Federation free-style Reception buildings and the art deco Hargravia, Belgravia and main wings and the federation free classical south wing. The hotel also includes a number of freestanding buildings with a unity of styling and detailing such as the north bunkhouse, toilet block and rear of the Road Bar.

The arrangement of buildings along the ridge parallel to the Great Western Highway with the distinctive street fencing and row of mature radiata pinus trees quickly became, and remains, a significant landmark on the road through the Blue Mountains.

Some individual elements including the Casino and Reception buildings are fine examples of Federation free style architecture.

The tennis courts have a rare quality with their rustic stone walling and location on the edge of the ridge

SHR Criteria d)

[Social significance]

The unusual feature of a prefabricated imported casino which became a showpiece for some of the greatest singers of the Edwardian period, the art collection and the cuisine further enhance the social significance of the Hydro.

SHR Criteria e)

[Research potential]

Technical interest attaches to the remains of the flying fox into the Megalong and the symbiosis between the hotel and valley below has remained a significant element in the Hydro's success.

Integrity/Intactness:

See under further comments

Assessment criteria:

10/14/2020

Mb002 : Hydro Majestic | Heritage NSW

Items are assessed against the  **State Heritage Register (SHR) Criteria** to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|-------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan 2005 | MB002 | 07 Oct 05 | 122 | |
| Local Environmental Plan | Local Environmental Plan 1991 | MB002 | 27 Dec 91 | 183 | |
| Heritage study | | MB002 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB002 | Craft & Associates Pty Ltd & Meredith Walker | | Yes |
| Heritage Study Review, Blue Mountains | 1992 | MB002 | Tropman and Tropman | | Yes |
| Blue Mountains Heritage Register Review | 1999 | MB002 | Jack, R. I. for University of Sydney | RIJ & PH | Yes |
| Technical Audit BM Heritage Register | 2008 | MB002 | Blue Mountains City Council | City Planning Branch | No |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|------------|-------------------------------------|------|---|----------------|
| Written | Hungerford, M. E. and Donald, J. K. | 1982 | Exploring the Blue Mountains | |
| Written | Johns, Fred | 1922 | Who's Who in the Commonwealth of Australia | |
| Photograph | Keldy, Elaine | 1900 | Photograph of Hargrave's House in Medlow 1883 and Now, circa | |
| Written | Keldy, Elaine | 1983 | Medlow 1883 and now | |
| Written | Mitchell, Bruce | 1972 | Hargrave, Edward Hammond (1816-1891), Australian Dictionary of Biography, Vol IV | |
| Written | Silvey, Gwen | 1996 | Happy Days: Blue Mountains Guesthouses Remembered | |
| Written | Walsh, G. P. | 1981 | Foy, Francis (1856? - 1918) and Mark (1865-1950) in Australian Dictionary of Biography, Vol. VIII | |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

<https://apps.environment.nsw.gov.au/dpcheritagesapp/ViewHeritageItemDetails.aspx?ID=1170280>

11/12

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170280

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb008 : Post and Telegraph Store, Former

Item details

Name of item:

Mb008 : Post and Telegraph Store, Former

Other name/s:

Flanagan's Hall; Rice's Hall

Primary address:

1 Railway Parade, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|------------------|-------------|----------------|--------|--------|-----------------|
| 1 Railway Parade | Medlow Bath | Blue Mountains | | | Primary Address |

Statement of significance:

The hall and store have high local significance because of their association with the Hydro Majestic and Mark Foy's touristic entrepreneurship, particularly in catering for the interest in Jenolan Caves. The long-standing association with motor-cars is a particularly significant feature.

It is an unusual example of a Federation gothic shopfront. An unusual form for a post office, the building features crenellated parapet, twisted columns and a decorative shield in the centre of the façade.

The hall had a high local profile as a centre for dances, films and, after World War II, a wide variety of Catholic and community functions, while the store and post-office played their usual key role for the residents and visitors alike.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1903-1910

Physical description:

Large gabled building on east-west axis with skillion on north side and 3 bay parapetted shop front with skillion roof on west side facing street

Three bay shop front is rendered with crenellated parapet to side bays, engaged pilasters between bays and a large rectangular panel with a decorative shield marked with the date

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170284>

1/5

"1910" in the centre bay.

The centre bay has a dentilated coping. The engaged flat pilasters become twisted below a skillion awning over the entry door.

The parapetted shop front returns 1 bay to link to the main gabled building

The shop entry is located in the centre bay of the shop front. An arched window is located on northern bay of west elevation.

4 over 4 pane double hung windows are located on the north and south sides of the parapet wall

Corrugated galvanised steel wall (painted) on south side of main gabled building with pair diagonal boarded ledged and sheeted doors approached by timber entry ramp and 3no. 3 pane windows.

Corrugated steel skillion toilets on east end.

Skillion on north side

Timber boarded linings to main hall

Physical condition and/or

Archaeological potential:

Good

Date condition updated: 08 Feb 00

Modifications and dates:

Entry door and sidelights

Window in shopfront

Additional skillion verandah on north side, semi-enclosed

Gabled corrugated steel shed in southeast corner of yard

Petrol bowser stored in garage

Timber milepost removed

Current use:

Shop/café and residence

Former use:

Post Office; Dance Hall; Catholic Church

History

Historical notes:

The site of the store was originally granted to Isabella Jane Smith in December 1903. By 1905 Mark Foy, who had in 1900 bought what became the Hydro Majestic Hotel, purchased the store-site from Isabella Smith and built a hall, using timber and corrugated iron recycled from the hall in the now abandoned shale village in Nellies Glen. The new hall was known as Flanagan's Hall because Mrs Flanagan ran a boarding-house nearby to the north, on the corner of Atlingworth Parade, and was presumably the manager of the hall where dances were held.

In 1910 Foy's workmen built the front section to create a store, with post office premises attached to the north. After 1918 the store and hall took on a new, highly significant role under Jack Rice, head chauffeur to the car-conscious Mark Foy. The tourist industry developed by Foy entailed a steam Serpollet charabanc and De Dion Bouton and Packard cars to take visitors from the Hydro to Jenolan Caves and other, closer sights. The hall at Medlow Bath was the hub of this operation between the wars; refreshments were served in the hall, now known as Rice's Hall, and the vehicles started their trips from the store, where

10/14/2020

Mb008 : Post and Telegraph Store, Former | Heritage NSW

there was a petrol bowser.

In these inter-war years the hall was also used as a cinema annexe to the Hydro Majestic. During World War II it was made available to American servicemen for recreation.

After 1945 both the hall and what had been Mrs Flanagan's guesthouse were in need of repair and the Hydro sold them separately. The Roman Catholic church purchased the hall in 1952 and used it as a church and church-hall until 1967. During this fifteen-year period, the hall was used for a very wide variety of local social purposes, including the Progress Association, the library, a polling booth, a Centre for Rural Youth and charity sales.

In the 1970s the building deteriorated and demolition was contemplated in 1980. Mr W. Boldiston, however, bought it in 1980 and conserved the hall and store, although the post-office annexe was demolished in 1986. (Boldiston, 16-17, 25)

The building is now open as an antique shop, book-shop and tearoom, run by Peter Koehndorfer under the ownership of Garry Coxen. The highly significant early petrol pump was removed from the front for safety and is secure, with other motoring relics, in the adjacent shed, but the old wooden mile-marker which also stood outside has recently been stolen.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|--|-------------|
| 3. Economy-Developing local, regional and national economies | Commerce-Activities relating to buying, selling and exchanging goods and services | (none)- |
| 4. Settlement-Building settlements, towns and cities | Accommodation-Activities associated with the provision of accommodation, and particular types of accommodation – does not include architectural styles – use the theme of Creative Endeavour for such activities. | (none)- |
| 4. Settlement-Building settlements, towns and cities | Land tenure-Activities and processes for identifying forms of ownership and occupancy of land and water, both Aboriginal and non-Aboriginal | (none)- |
| 7. Governing-Government | Government and Administration-Activities associated with the governance of local areas, regions, the State and the nation, and the administration of public programs - includes both principled and corrupt activities. | (none)- |
| 8. Culture-Developing cultural institutions and ways of life | Creative endeavour-Activities associated with the production and performance of literary, artistic, architectural and other imaginative, interpretive or inventive works; and/or associated with the production and expression of cultural phenomena; and/or environments that have inspired such creative activities. | (none)- |
| 8. Culture-Developing cultural institutions and ways of life | Religion-Activities associated with particular systems of faith and worship | (none)- |
| 9. Phases of Life-Markings the phases of life | Persons-Activities of, and associations with, identifiable individuals, families and communal groups | (none)- |

Assessment of significance

SHR Criteria a)

[Historical significance]

The hall and store have high local significance because of their association with the Hydro Majestic and Mark Foy's touristic entrepreneurship, particularly in catering for the interest in

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170284>

3/5

10/14/2020

Mb008 : Post and Telegraph Store, Former | Heritage NSW

Jenolan Caves. The long-standing association with motor-cars is a particularly significant feature.

SHR Criteria c)

[Aesthetic significance]

The former Post and Telegraph Office and Store is an unusual example of a Federation gothic shopfront. An unusual form for a post office, the building features crenellated parapet, twisted columns and a decorative shield in the centre of the façade.

SHR Criteria d)

[Social significance]

The hall had a high local profile as a centre for dances, films and, after World War II, a wide variety of Catholic and community functions, while the store and post-office played their usual key role for the residents and visitors alike.

Integrity/Intactness:

High

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan2005 | MB008 | 07 Oct 05 | 122 | |
| Heritage study | | MB008 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB008 | Croft & Associates Pty Ltd & Meredith Walker | | Yes |
| Heritage Study Review, Blue Mountains | 1992 | MB008 | Tropman and Tropman | | Yes |
| Blue Mountains Heritage Register Review | 1999 | MB008 | Jack, R. I. for University of Sydney | RJ & PH | Yes |
| Technical Audit BM Heritage Register | 2008 | MB008 | Blue Mountains City Council | City Planning Branch | No |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|---------|--------------|------|------------------------------------|----------------|
| Written | Boldston, W. | | The Post Office Store, Medlow Bath | |

Note: internet links may be to web pages, documents or images.



<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170284>

4/5

10/14/2020

Mb008 : Post and Telegraph Store, Former | Heritage NSW

(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170284

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb010 : St Luke's Anglican Church

Item details

Name of item:

Mb010 : St Luke's Anglican Church

Primary address:

40A Railway Parade, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|--------------------|-------------|----------------|--------|--------|-----------------|
| 40A Railway Parade | Medlow Bath | Blue Mountains | | | Primary Address |

Statement of significance:

The church has had social significance for the Anglican community around Medlow Bath for over eighty years. It is a representative example of a Federation carpenter gothic church built for a small rural village, although it took its present simpler shape only after storm damage in 1920.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1913-1913

Physical description:

2 bay gabled church on east west axis with entry vestibule on west end and apsidal chancel at east end with hipped roof

Corrugated steel roof and exposed rafters to eaves.

Fretwork barge boards with trefoil motif to main building

Rusticated weatherboards at front, chamfered at sides

Framed and sheeted pointed arch doors on south side of porch

Pointed three sash windows to porch

Pointed 2 over 2 pane windows with toplights at sides

Leadlight windows to east and corner windows of chancel, 3 pane at sides

Bell on wrought iron bracket on timber post on north side of porch

Acorn picket fence with diamond capped posts

Physical condition and/or**Archaeological potential:**

Good

10/14/2020

Mb010 : St Luke's Anglican Church | Heritage NSW

Date condition updated:08 Feb 00**Modifications and dates:**

Transepts removed and building shortened 1920

Current use:

Church

Former use:

Church

History

Historical notes:

Medlow Bath was within the Anglican jurisdiction of Blackheath. The early ministers at St Aidan's, Blackheath, conducted services at Medlow Bath at first in a cave, then in a private house. Land in Railway Parade was purchased from Tom Pain by the church in 1908 and finally in 1913 St Luke's was built.

Prime movers in the building of the church were Captain and Mrs Wade-Brown, prominent Anglicans in the village; the Foy's were Catholics and played no particular role.

In 1920 the church, which had been built with a nave, chancel and transept, was damaged in a storm when the chancel was partly destroyed. To remedy the damage, the transept was removed and the chancel was rebuilt to join the nave without a transept; the church was accordingly shortened by some 5 metres and took on its present simple rectangular shape.

Stained glass windows were donated to St Luke's, one at the time of opening in 1913 by 'H.P.P.', the other in memory of a minister, the Revd J. Read (who had not been the local rector).

The church bell is a former ship's bell, rescued from the 'Princess Alexandra' which had been wrecked on the Bellingbar in northern New South Wales in 1874.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|---|-------------|
| 8. Culture-Developing cultural institutions and ways of life | Religion-Activities associated with particular systems of faith and worship | (none)- |

Assessment of significance

SHR Criteria c)

[Aesthetic significance]

St Luke's Church of England, Medlow Bath is a representative example of a Federation carpenter gothic church built for a small rural village

SHR Criteria d)

[Social significance]

The church has had social significance for the Anglican community around Medlow Bath for over eighty years.

Integrity/Intactness:

Intact

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170286>

2/4

10/14/2020

Mb010 : St Luke's Anglican Church | Heritage NSW

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|-------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan 2005 | MB010 | 07 Oct 05 | 122 | |
| Heritage study | | MB010 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB010 | Croft & Associates Pty Ltd & Meredith Walker | | Yes |
| Heritage Study Review, Blue Mountains | 1992 | MB010 | Tropman and Tropman | | Yes |
| Blue Mountains Heritage Register Review | 1999 | MB010 | Jack, R. I. for University of Sydney | RJ & PH | Yes |
| Technical Audit BM Heritage Register | 2008 | MB010 | Blue Mountains City Council | City Planning Branch | No |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|---------|--------------------|------|--|----------------|
| Written | | | | |
| Written | Hodgkinson, Lewis | 1984 | The Centenary of St Alden's Church of England, Blackheath, 1884-1984 | |
| Written | Kaldy, Elaine | 1983 | Medlow 1883 and Now | |
| Written | Yeaman, John (ed.) | 1976 | Historic Blackheath | |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170286>

3/4

REPORT

10/14/2020

Mb010 : St Luke's Anglican Church | Heritage NSW

Local Government

Database number:

1170286

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170286>

4/4

Mb013 : Horse Trough

Item details

Name of item:

Mb013 : Horse Trough

Primary address:

Railway Parade, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|-----------------|-------------|----------------|--------|--------|-------------------|
| Railway Parade | Medlow Bath | Blue Mountains | | | Primary Address |
| Somerset Street | Medlow Bath | Blue Mountains | | | Alternate Address |

Statement of significance:

All the Annis and George Bills horse-troughs have some local significance as evidence of philanthropy towards animals, even though they were erected when the days of the horse on the roads were almost over.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1930-1940

Physical description:

Concrete horse trough of standard design. 8 ft 7 1/2 inches long, 2 ft 7 1/2 inches wide and 3 ft 11 inches high. Majority of the trough is water area, a cistern with an iron lid at one end. Additional small container with water for dogs and cats at ground level on the right side. Pedimented section at rear with inset terrazzo panel bearing cast inscription:

Donated By

Annis and George Bills

Australia

Other examples of Bills troughs occur in the Blue Mountains at Medlow Bath and Blackheath. Horse troughs at Woodford and Lawson are not of the same design. The Lawson Trough (Honour Ave) was donated by Mrs R D Meagher in 1921.

Physical condition and/or**Archaeological potential:**

Good

Date condition updated: 25 Mar 00

Current use:

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170355>

1/3

10/14/2020

Mb013 : Horse Trough | Heritage NSW

Disused

Former use:

Horse Trough and Dog Trough

History

Historical notes:

Annis and George Bills ran a bird exporting business. They were animal lovers who had no children and gave considerable sums to animal societies such as the Dowling Street Dogs Home (£500 p.a) and a shelter for horses in Kingswood/Maroubra. They also donated to the (English) Metropolitan Drinking Fountain and Cattle Troughs Association.

When George died in 1927 he left an estate of £91,000. After various bequests, the remainder of the estate was to be used to construct and pay for horse troughs wherever they may be necessary for the relief of horses or other dumb animals in Australasia, the British Isles or any other part of the world subject to the consent of the proper authorities.

The troughs were erected from 1930 to 1940, providing employment in the Depression years. About 700 troughs were erected in Australia, the majority were in Victoria, and about 50 were erected overseas, including the UK and Dublin. The troughs were usually of reinforced concrete and were of a standard design. Rare examples were occasionally in granite (eg Scone, NSW and Hawthorn, Victoria).

The Medlow Bath example is a characteristic Bills trough, with a small trough for small animals such as dogs to the right.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|--|-------------|
| 3. Economy-Developing local, regional and national economies | Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements | (none) |
| 7. Governing-Governing | Welfare-Activities and process associated with the provision of social services by the state or philanthropic organisations | (none) |

Assessment of significance

SHR Criteria a)

[Historical significance]

All the Annis and George Bills horse-troughs have some local significance as evidence of philanthropy towards animals, even though they were erected when the days of the horse on the roads were almost over.

Integrity/Intactness:

High

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|------------------|---------------|----------------|--------------|----------------|--------------|
|------------------|---------------|----------------|--------------|----------------|--------------|

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170355>

2/3

10/14/2020

Mb013 : Horse Trough | Heritage NSW

| | | | | | |
|--------------------------|-------------------------------|-------|-----------|-----|--|
| Local Environmental Plan | Local Environmental Plan 2005 | MB013 | 07 Oct 05 | 122 | |
| Heritage study | | MB013 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB013 | Craft & Associates Pty Ltd & Meredith Walker | | Yes |
| Heritage Study Review, Blue Mountains | 1992 | MB013 | Tropman and Tropman | | Yes |
| Blue Mountains Heritage Register Review | 1999 | MB013 | Jack, R. I. for University of Sydney | RJ & PH | Yes |
| Technical Audit BM Heritage Register | 2008 | MB013 | Blue Mountains City Council | City Planning Branch | No |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|---------|-------------------|------|---------------|----------------|
| Written | McWilliams, M. E. | | Horse Troughs | |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170355

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb015 : Avenue of Radiata Pines

Item details

Name of item:

Mb015 : Avenue of Radiata Pines

Primary address:

52 Great Western Highway To 33 Station Street, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|---|-------------|----------------|--------|--------|-----------------|
| 52 Great Western Highway To 33 Station Street | Medlow Bath | Blue Mountains | | | Primary Address |

Statement of significance:

The avenue is an integral part of the significance of the Hydro Majestic, telling testimony to the public image of the resort projected by Mark Foy and continuing today. Viewed as part of the whole, aesthetically and historically, the avenue, like the hotel, has state significance.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1904-1904

Physical description:

Radiata pines extend in a single line between the Great Western Highway and the railway line from the eastern end of the Hydro Majestic estate to the over-bridge and then down Station Street as far as no.33, the former gatekeeper's cottage. Some trees have died in the effluxion of time and been replaced, but the avenue is basically intact.

Physical condition and/or**Archaeological potential:**

Good

Date condition updated: 25 Mar 00

Modifications and dates:

Some trees have been replaced.

Further information:

It is not clear whether this avenue is threatened by the impending road widening by RTA nor is it known how many more years the older trees are likely to survive.

Current use:

Avenue

Former use:

Avenue

History

Historical notes:

The avenue of radiata pines opposite the Hydro Majestic but extending beyond the overbridge down Station Street, was planted by Mark Foy on the public land between the main road and the railway. The purpose of the planting was to emphasise how Medlow Bath was the Hydro and the Hydro Medlow Bath, so that a distinctive avenue along the straight portion of main road gave unmistakable signals to all travellers both by rail and by road. The extension of the avenue down Station Street emphasised that Shelaugh Cottage (no.6, MB 019) was also part of the Foy domain.

Some trees have died over the years and been replaced, but the avenue is basically intact.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|---|-------------|
| 3. Economy-Developing local, regional and national economies | Environment - cultural landscape-Activities associated with the interactions between humans, human societies and the shaping of their physical surroundings | (none) |
| 8. Culture-Developing cultural institutions and ways of life | Leisure-Activities associated with recreation and relaxation | (none) |
| 9. Phases of Life-Marking the phases of life | Persons-Activities of, and associations with, identifiable individuals, families and communal groups | (none) |

Assessment of significance

SHR Criteria a)

[Historical significance]

The avenue is an integral part of the significance of the Hydro Majestic, telling testimony to the public image of the resort projected by Mark Foy and continuing today. Viewed as part of the whole, aesthetically and historically, the avenue, like the hotel, has state significance.

SHR Criteria c)

[Aesthetic significance]

The avenue is a highly significant aesthetic feature of Medlow Bath and the Hydro Majestic

Integrity/Intactness:

High

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|-------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan 2005 | MB015 | 07 Oct 05 | 122 | |
| Heritage study | | MB015 | | | |

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|-------------------------------|------|--------|---------------------------------------|--------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB015 | Croft & Associates Pty Ltd & Meredith | | |

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170357>

2/3

10/14/2020

Mb015 : Avenue of Radiata Pines | Heritage NSW

| | | | | | |
|---|------|-------|--------------------------------------|----------------------|-------------|
| | | | Walker | | Y e s |
| Heritage Study Review, Blue Mountains | 1992 | MB015 | Tropman and Tropman | | Y e s |
| Blue Mountains Heritage Register Review | 1999 | MB015 | Jack, R. I. for University of Sydney | RIJ & PH | Y e s |
| Technical Audit BM Heritage Register | 2008 | MB015 | Blue Mountains City Council | City Planning Branch | N o |

References, internet links & images

None

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170357

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb017 : Urunga

Item details

Name of item:

Mb017 : Urunga

Other name/s:

Station-master's Residence

Primary address:

1 Park Street, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|----------------|-------------|----------------|--------|--------|-------------------|
| 1 Park Street | Medlow Bath | Blue Mountains | | | Primary Address |
| Railway Parade | Medlow Bath | Blue Mountains | | | Alternate Address |

Statement of significance:

The association of the house with the railway and the growth of rail traffic at Medlow Bath associated with the Hydro gives the cottage local historical significance.

Date significance updated: 25 Mar 00

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the Department of Premier and Cabinet [copyright](#) and [disclaimer](#).

Description

Construction years:

1916-1916

Physical description:

Symmetrical gabled cottage on east-west axis facing south to Park Street with skillion verandah on south side

2 over 2 pane double hung windows either side of 4 panel door with toplight

Corrugated steel roof and corbelled brick chimneys

Rusticated weatherboard cladding, wider to front (south) elevation.

2 no. stone steps on axis of door

Stop chamfered verandah columns and beam

Gate in Station Street fence leads directly to Medlow Bath railway station.

2 no. large corrugated steel skillion sheds on Railway Parade boundary, 1 with corbelled brick chimney

Corrugated steel outhouse on north boundary with arched roof

Physical condition and/or**Archaeological potential:**

Reasonable

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170290>

1/3

10/14/2020

Mb017 : Urunga | Heritage NSW

Date condition updated:09 Feb 00**Modifications and dates:**

Central verandah columns relocated (date unknown)

Weatherboard skillion on west side (date unknown)

Low pitched gabled weatherboard addition on east side with north slope of roof extending to form rear skillion to main house (date unknown)

Fibro cladding on east wall (date unknown)

Fibro outhouse in northeast corner of yard. (date unknown)

Further information:

The Heritage Register at present shows 2-4 Park Street (MB 009) as the former station-master's house. This is not correct.

Current use:

Residence

Former use:

Stationmaster's Residence

History

Historical notes:

Urunga was built adjacent to the Medlow Bath railway station around 1916 to accommodate the station-master as the station had become busier with the success of Mark Foy's Hydro (MB 002). It remained a railway house until recently when it was sold. It is now a private residence owned since 1998 by Ms C. Holman.

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|--|-------------|
| 3. Economy-Developing local, regional and national economies | Transport-Activities associated with the moving of people and goods from one place to another, and systems for the provision of such movements | (none) |
| 4. Settlement-Building settlements, towns and cities | Land tenure-Activities and processes for identifying forms of ownership and occupancy of land and water, both Aboriginal and non-Aboriginal | (none) |

Assessment of significance

SHR Criteria a)

[Historical significance]

The association of the house with the railway and the growth of rail traffic at Medlow Bath associated with the Hydro gives the cottage local historical significance.

Integrity/Intactness:

Medium

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan2005 | MB017 | 07 Oct 05 | 122 | |
| Heritage study | | MB017 | | | |

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170290>

2/3

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB017 | Croft & Associates Pty Ltd & Meredith Walker | | Y e s |
| Heritage Study Review, Blue Mountains | 1992 | MB017 | Tropman and Tropman | | Y e s |
| Blue Mountains Heritage Register Review | 1999 | MB017 | Jack, R. I. for University of Sydney | RJ & PH | Y e s |
| Technical Audit BM Heritage Register | 2008 | MB017 | Blue Mountains City Council | City Planning Branch | N o |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|--------------|---|------|-------------------------|----------------|
| Oral History | Holman, Ms. C., owner | 1999 | Interview, 20 September | |
| Oral History | Macnab, Dr. Ken, son of station-master, Medlow Bath | 2000 | Interview, 23 March | |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

Database number:

1170290

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage

Item details

Name of item:

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage

Other name/s:

No 2 Station Street: Melbourne House, No 4: Cosy Cot; No 6: Shelaugh Cottage.

Primary address:

2, 4 & 6 Station Street, Medlow Bath, NSW 2780

Local govt. area:

Blue Mountains

All addresses

| Street Address | Suburb/town | LGA | Parish | County | Type |
|-------------------------|-------------|----------------|--------|--------|-----------------|
| 2, 4 & 6 Station Street | Medlow Bath | Blue Mountains | | | Primary Address |

Description

Construction years:

1900-1910

Physical description:

Lot 1 GWH

Brick single storey shop facing east to highway with attached single storey residence on south side.

Large battened and shingled steeply pitched gable roof on east-west axis to shop with wide skillion roof over shop front. Roof behind shop is lower pitched gable with corrugated steel skillion shed attached.

House has low pitched gabled roof on north-south axis with battened gable. Verandah on east side of house facing street supported on timber posts and beam with tapered end.

Corrugated steel roofs. Corbelled brick chimney

Double hung windows to side of house, casements to front verandah, with concrete intels and brick sills

Panelled front door with octagonal glazing

Vacant Block

Melbourne House (2 Station Street)

Single storey cottage facing east to Station Street.

Intersecting gables with north wing on east-west axis and central wing on north-south axis with east and west verandahs. Later south gabled wing on east-west axis extends beyond the verandahs of the central wing.

Corrugated steel roofs and corbelled brick chimneys

North wing has corrugated steel cladding, central wing has rusticated weatherboards. South wing has stretcher bond brickwork to west elevation and west end of south elevation,

10/14/2020

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage | Heritage NSW

corrugated steel to remainder of south elevation
Rear corrugated galvanised steel skillion to north wing
East verandah has stop chamfered timber posts, timber beam and brackets
East gable of south wing has pressed metal with roughcast pattern and battens
4 panel doors open from north and south wings to east verandah, 2 pairs french doors open from central wing to east verandah
2 over 2 pane double hung windows to north wing
Beaded boards to verandah soffit

Brick garage with diagonal boarded doors on north side of site

Cosy Cot: 4 Station Street

Single storey house facing east to Station Street with battened gable at south end, hipped roof at north end and skillion verandah on east side
Projecting gable at north end facing Station Street has fretwork barge and projecting pair of double hung windows with metal skillion roof with wide eave supported on timber brackets. Sides of window projection have no windows
Corrugated steel roof and corbelled brick chimneys with terracotta pots
Rusticated weatherboard cladding
Diagonal brackets and timber posts to verandah
Low pitched gable facing north off north wing with bracketed awning roof over paired double hung windows on east side
6 panel 1/2 glazed front door
Paired double hung widows to verandah
Framed and boarded skillion on north side

Sheleagh Cottage (6 Station Street)

Single storey hipped roof house facing east to Station Street with projecting gable on north end of east elevation and west end of south elevation. Roof extends over verandah on east and south sides
Unglazed terracotta tiled roof with orb finials
Rendered chimneys with brick corbels
Bay window on southeast corner projects into verandah.
Double hung windows
Verandah has turned verandah posts and timber balustrade combining cross bracing and turned timber balusters.
Rusticated weatherboard cladding
Sandstone entry stair and sub-floor walls
Panelled front door with art nouveau leadlight glazing, sidelights and toplight
Timber boarding to verandah soffit
North gable has projecting pair of double hung windows with shingled spandrel below and frieze over. Sides of projection are clad with rusticated weatherboards with shingled frieze.
Catslide roof over entry porch on north side

Garden with low stone walls, stone benches and sundial
Mature trees

2 storey low-pitch gabled garage with 2 storey wings on north and south sides. Side wings

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170292>

2/6

10/14/2020

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage | Heritage NSW

have low pitched roof broken back to main roof.

Gable is battened fibro, walls have splayed weatherboard cladding and corrugated steel cladding to ground floor of skillions

Double hung windows to upper floor

Gabled slab shed in rear garden

Physical condition and/or

Archaeological potential:

Good

Date condition updated: 09 Feb 00

Modifications and dates:

Melbourne House (2 Station Street)

East wall of north wing clad in hardiplank

Flat awning over windows on south side with tapered rafters

Ripple iron infill with sliding 4 pane windows to rear verandah

Pergola on north side, partly infilled

Cosy Cot (4 Station Street)

South end of verandah enclosed

Rear (west) skillion

Sheleagh (6 Station Street)

Pergola on north side

Shingle cladding to gable in north bay

Glazed infill to south verandah

Skillion roof cottage in rear yard clad with

aluminium siding

Further information:

Street Addresses: Country Kitchen (Lot 1) Great Western Highway

Melbourne House: 2 Station Street,

Cosy Cot: 4 Station Street

Shelaugh Cottage: 6 Station Street.

Current use:

Lot 1 GWH: Shop and Residence; Vacant Block: Unused; 2-4 Station St: Residences

Former use:

Lot 1 GWH: Shop and Residence; Vacant Block: Unused; 2-4 Station St: Residences

History

Historical notes:

The four buildings extending from Lot 1 Great Western Highway northwards to 2,4 and 6 Station Street, have no particular historical coherence beyond their closeness of date in the first decade of the twentieth century.

Lot 1 on the western side of the highway is a house and shop characteristic of the early years of the century. It is still run as a shop and cafe called The Country Kitchen by Mr and Mrs A. Athanasi, who reside in the cottage section.

10/14/2020

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage | Heritage NSW

After a vacant block, the next building to the north is Melbourne House, 2 Station Street. This cottage was built in 1908 or soon afterwards by a Mrs Saunders. The owner for half a century after c.1940 was Miss Rose Pritz (Kaldy 53).

Cosy Cot, 4 Station Street, probably belongs to the same decade, 1900 to 1910. It was the mountain retreat of Henry Hartley, better known as Hartley Sargent (1878 - 1924), who, with his mother and step-father, ran a famous chain of bakeries and refreshment rooms in New South Wales and Victoria. The firm is still remembered for Sargent's Pies. Hartley's health was impaired by a period as prisoner-of-war after being injured in battle in 1917 and he spent most of his remaining years at Cosy Cot. He died there in 1924 after falling from the cliff edge immediately beyond his own grounds. The house has remained in the hands of the Sargent family: Hartley had two daughters and three sons (MacCulloch XI 522; Kaldy 56).

Shelaugh Cottage, 6 Station Street, is intimately connected with the Hydro, unlike Melbourne House and Cosy Cot. Immediately after Mark Foy acquired what became the Hydro, he commissioned Sir Herbert Ross to design a summer residence for himself and the house was erected between 1901 and 1903. It was named Shelaugh Cottage after Foy's third daughter. The Foy family sold the house to a stonemason called Bagley in c.1958; Bagley is responsible for the stone benches, walling and sundial which adorn the house today. In c.1978 the cottage was acquired by Wilmot-Farden, who in turn sold it in 1995 to the present owner, Mrs Helen Craig. The outhouse known as 'The Loft' belongs to the Mark Foy period; the shed built of sleepers and known as 'The Pub' is probably a later erection, reusing materials (Kaldy 37; Craig interview).

Historic themes

| Australian theme (abbrev) | New South Wales theme | Local theme |
|--|---|-------------|
| 3. Economy-Developing local, regional and national economies | Commerce-Activities relating to buying, selling and exchanging goods and services | (none)- |
| 4. Settlement-Building settlements, towns and cities | Land tenure-Activities and processes for identifying forms of ownership and occupancy of land and water, both Aboriginal and non-Aboriginal | (none)- |

Assessment of significance

Integrity/Intactness:

Lot 1 GWH: Reasonable

Victoria House (2 Station Street): High

Cosy Cot (4 Station Street): High

Sheleagh Cottage (6 Station Street): High

Assessment criteria:

Items are assessed against the  [State Heritage Register \(SHR\) Criteria](#) to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Listings

| Heritage Listing | Listing Title | Listing Number | Gazette Date | Gazette Number | Gazette Page |
|--------------------------|-------------------------------|----------------|--------------|----------------|--------------|
| Local Environmental Plan | Local Environmental Plan 2005 | MB019 | 07 Oct 05 | 122 | |
| Heritage study | | MB019 | | | |

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170292>

4/6

10/14/2020

Mb019 : Melbourne House, Cozy Cot, Sheleagh Cottage | Heritage NSW

Study details

| Title | Year | Number | Author | Inspected by | Guidelines used |
|---|------|--------|--|----------------------|-----------------|
| Blue Mountains Heritage Study | 1983 | MB019 | Craft & Associates Pty Ltd & Meredith Walker | | Yes |
| Heritage Study Review, Blue Mountains | 1992 | MB019 | Tropman and Tropman | | Yes |
| Blue Mountains Heritage Register Review | 1999 | MB019 | Jack, R. I. for University of Sydney | RJ & PH | Yes |
| Technical Audit BM Heritage Register | 2008 | MB019 | Blue Mountains City Council | City Planning Branch | No |

References, internet links & images

| Type | Author | Year | Title | Internet Links |
|--------------|--------------------------------------|------|--|----------------|
| Oral History | Craig, Helen, owner of 6 Station St, | 1999 | Interview, 20 September | |
| Written | Kaldy, Elaine | 1983 | Medlow 1883 and Now | |
| Written | MacCulloch, J. | 1988 | Sargent, George (1859-1921) and Charlotte (1856-1934), Australian Dictionary of Biography, Vol. XI | |

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name:

Local Government

<https://apps.environment.nsw.gov.au/dpcheritagesapp/ViewHeritageItemDetails.aspx?ID=1170292>

5/6

REPORT

10/14/2020

Mb019 : Melbourne House, Cosy Cot, Sheleagh Cottage | Heritage NSW

Database number:

1170292

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the [Database Manager](#).

All information and pictures on this page are the copyright of Heritage NSW or respective copyright owners.

<https://apps.environment.nsw.gov.au/dpcheritageapp/ViewHeritageItemDetails.aspx?ID=1170292>

6/6

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

Address: 52-88 Great Western Highway

DUAP Region: Sydney West

Suburb / Nearest Town: Medlow Bath 2780

Historic region: Sydney

Local Govt Area: Blue Mountains

Parish: Megalong

State: NSW

County: Cook

Other/Former Names:

Area/Group/Complex: Hydro Majestic - original walking track complex

Group ID: 1173065

Aboriginal Area:

Curtilage/Boundary: Portions 56, 134, 57, 91, 132, 35, 46

Item Type: Landscape

Group: Landscape - Cultural

Category: Historic Landscape

Owner: Private - Corporate

Admin Codes:

Code 2:

Code 3:

Current Use: Tracks used by experienced bushwalkers

Former Uses: Tourist tracks

Assessed Significance: **State**

Endorsed Significance:

Statement of Significance: (Note: This Statement of Significance applies to the whole of the former Hydro Majestic walking track system, not only that portion remaining within the present hotel grounds. However, lots on adjacent lands have been excluded from the listing boundary at this stage.)

The Hydro Majestic Hotel walking track complex has State Significance as the most extensive privately constructed walking track complex in Australia and for its association with Australia's only hydropathical resort developed on the European model.

The Hydro Majestic walking track complex has State Significance for its association with entrepreneur Mark Foy junior who made a fortune in retailing and invested most of it in developing the Hydro Majestic hydropathic resort. The track complex has local significance for its association with William Hargraves, Chief Clerk in Equity of NSW and Blackheath pioneer.

The Hydro Majestic walking track complex has aesthetic significance at the State level for the design values in its construction which demonstrate superb integration of natural and constructed features.

As nearly all of the track complex is intact, it offers an opportunity to research late 19th and early 20th century walking track design and construction techniques, significant at the local level.

The Hydro Majestic walking track network is a rare example of a very extensive privately constructed walking track complex, mostly over 100 years old, which has survived virtually

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 1

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

fully intact to the present time.

Historical Notes or Provenance: The Hydro Majestic walking tracks are only one component of a complex cultural landscape begun by William Hargraves and greatly elaborated by Mark Foy. The following draft Statement of Significance for this cultural landscape was prepared for the Hydro Majestic Walking Track Conservation Management Plan (1998) and provides relevant historical information for these walking tracks.

Sydney retailer and hotelier Mark Foy created, in a spectacular escarpment setting, on about 900ha of land at Medlow Bath and in the adjoining Megalong Valley, a cultural landscape with a remarkably diverse range of components. The 1904 Hydro Majestic Hotel, Australia's only hydropathic resort, is surrounded by formal gardens and street plantings reflecting late Victorian and early Edwardian landscaping principles. The Hotel achieved a high level of self sufficiency with its own farm, terraced vegetable gardens, water supply, electricity plant, septic system, coal mine, garages, stables and a rare example of an aerial ropeway transport system. There were a wide range of sporting facilities including shooting, golf, horseracing and croquet lawns. Cultural attractions included an early example of a regional art gallery, a museum and performances by artists of international stature in the Casino.

Foy created land subdivisions within this landscape and built an "Ideal Home" in an attempt to put into practice his ideas on town planning. The land contained Foy's country residence, Sheleagh Cottage and his intended mausoleum.

The varied features of this cultural landscape were linked by Australia's most extensive privately owned walking track system, totalling some 18km in length. These tracks were made to the highest standards of construction and embody significant aesthetic values in the way that they blend with the natural landscape features. They contain the most extensive dry stone walling features in the region.

Despite this wide range of constructed features created by Foy, over 80% of the bushland in which they are located has remained in a natural condition. This bushland contains a wide variety of vegetation types, on soils derived from both sandstone and granite, including heathlands, closed forest, open forests and sedgeland.

Foy's cultural landscape reflects aspects of 18 of the State's historical themes including strong associations with the themes of Environment, Leisure, Health, Sport and Persons. The historical association with hydropathy is rare at the National level as is the extent of development of the private walking track system. The survival of an early Edwardian self contained resort hotel (with later additions) and its extensive grounds in a relatively intact condition is rare at the State level. The landscaping of the grounds is considered to have aesthetic values that are rare at the State level. The survival of so much physical evidence and documentation of the components of this cultural landscape offers opportunities for research that are rare at the State level. At present the social significance of the cultural landscape is representative at the local level.

The present grounds of the Hydro Majestic are only about 90ha or about 10% of the original

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 2

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

area. Many parts of Foy's cultural landscape and the walking track network now lie outside the hotel grounds. The following history of the walking track complex is divided into two sections covering the tracks built by Hargraves separately to those developed by Foy.

Due to the absence of early maps or records it is not possible to say in which order the tracks were developed by Hargraves. Due to this inability to date them more accurately than from the period 1893 to 1902 and the fairly uniform quality and style of work over the complex, these tracks are here considered as one heritage unit.

Mr William H. Hargraves built his home "Tullumbar", on portion 56, Parish of Kanimbla, at Medlow in about mid 1893. His caretaker and gardener, as listed in the 1892-93 and 1894 electoral rolls was Murdo McLennan. By late 1894 there were said to be "upwards of six miles of paths, zig zag in shape, with steps here and there". These were said to "have been completed on a plan of Mr Hargraves own". Mark Foy said that all the paths were made by McLennan. McLennan is known to have also constructed at least three stone houses in Medlow around 1900. The paths included one down the cliffs accessing the Megalong Valley part of the portion. A 1900 article stated there were "more than seven miles length of safe and substantial pathways". They were "freely open to the public at all reasonable times". Mark Foy purchased Hargraves' property soon after and acquired the adjacent land as well. He continued to employ McLennan. Soon after the opening of the consolidated Hydro Majestic property in 1904, it was stated that there were twelve miles of tracks in the grounds. Guidebooks for guests issued through the 1930s and 1940s stated that there were thirteen miles of paths. It is assumed that the central core of tracks on Portion 56 were constructed by McLennan for Hargraves between 1893 and 1902. This core includes the track to the "Coliseum" via the "Sunbath" area, the Sunbath area to hotel track via the clifftops, and the connections between them. Hargraves probably also had constructed the dense network of garden and walking terraces to the south and south west of the hotel. Between 1902 and 1912 the track network was practically doubled in length by Mark Foy to include the Mark's Tomb, Wonderland, Swimming Pool, Glen Rosa, Mt Mark, Three Brothers, Lovers, Flying Fox and Belgravia St connections tracks. An important outlier of the Mark Foy tracks lies in the Blue Mountains National Park. In 1907 Foy organised the construction of a carriage road along the Point Pilcher Road. From its end he had built an elaborate and meticulously constructed track in the McLennan/Hydro Majestic style to link into the Grand Canyon track. The integrity of the Hydro Majestic tracks is threatened by the break up of the Foy estate, which has led to the track system being divided among a number of landowners. Hargraves and Foy between them created a cultural landscape that is unrivalled in the region for the diversity of its components. It included, as well as the luxury hotel, a working farm to supply produce for it. The hotel and farm were linked by a flying fox up the cliffs. There was also a shooting lodge, working coal mine and racecourse in the Megalong Valley. On the clifftops were garden terraces tended by Chinese men, terraces for clay pigeon shooting, a bush swimming pool, the "Sunbath", "Marks Tomb" (a cave where he intended to be buried), a bust of Mel Ward (who ran a museum on the property) and a cave within which church services were held in the 1890s. All of these features were linked by the walking track system. While Foy advertised the Hydro Majestic as a "Palace in a Wilderness" it was actually a palace in a carefully designed recreational cultural landscape.

The actual extent of additions by Mark Foy to the Hydro Majestic track network after his purchase of the Hargraves estate can be inferred from the fact that a 1900 article stated that

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 3

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountain]**

there were 7 miles of paths on the Hargraves estate (T. & C.J. 15/12/1900) whereas an article published just before the opening of the consolidated Hydro Majestic property stated that there were 12 miles of paths (M., 21/6/1904). An undated annotation by Mark Foy on a 1890s photograph states that McLennan 'made all the 13 miles of paths'. These lengths have not been verified by actual measurement. It is not clear whether the 13 mile figure includes the tracks to Lake Medlow, Walls Cave and Point Pilcher.

Some tracks such as the Wonderland, Mount Mark and Mt Maxine tracks, were associated with Foy's attempts to develop real estate subdivisions. It is of interest that the 'official' map of the Hydro Majestic tracks issued from about 1932 does not show all of the tracks such as the cliff top tracks south of the sunbath or the Coliseum to Murphy's track connection. These had become disused. As Foy's tracks can not be dated accurately individually and have relatively uniform structural quality they are treated here as a single heritage unit. The date for the beginning of the Foy era is the date of his purchase of Hargraves estate (leased from 12/4/1902, purchased 4/5/1903). The date of 1912 for the end of the Foy era of track development is speculative. It seems quite likely that McLennan was still employed by Foy while the Pt Pilcher track was being made in 1907.

| Themes: | National Theme | State Theme | Local Theme |
|---------|----------------|---------------------------------|-------------|
| | 1. Environment | Environment - naturally evolved | (none) |
| | 3. Economy | Environment - cultural land | (none) |
| | 8. Culture | Leisure | (none) |

Designer: William Hargraves; Mark Foy and Murdo McLennan

Maker / Builder: Murdo McLennan

Year Started: 1893 Year Completed: 1912 Circa: No

Physical Description: The central core of Hydro Majestic tracks on Portion 56 includes:

1. Hotel to "Sunbath" track
2. Cliff top track between Sunbath area and Hotel via (later) septic tanks
3. Connections between 1 & 2 including Glen Rosa
4. Megalong Valley Tracks - including Colosseum & connection to Megalong Valley Road
5. Garden and short walk terraces to west and south west of hotel

Additions by Mark Foy

1. Wonderland/Marks Tomb Tracks
2. Hotel to Swimming Pool Track
3. Three Brothers track
4. Lovers Track

5. Flying Fox Track and Access Road

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 4

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountain]**

6. Belgravia Street Connections
7. Sunbath Road
8. Murphys Track
9. McLennans Track
10. Maxine Bower Track
11. Prisoners Glen Track
12. Numerous connection tracks

Physical Condition: Original stonework and benching nearly all intact apart from a few landslips, many sections are overgrown and some are eroding.

Modification Dates: Some restoration by John Webb in early 1980s. Construction of septic tank on Sunbath Track in early 2000s.

Recommended Management: List on Blue Mountains Local Environment Plan 2015.

Management:

Further Comments: The Statement of Significance on this form applies to the whole of the former Hydro Majestic walking track network, which is now under multiple private ownership. The Statement of Significance should not be necessarily taken to apply only to the parts of the walking track system now owned by the Hydro Majestic Hotel. The following list shows those parts of the track network outside the Hotel grounds which are included in this assessment.

Parts of the original Hydro Majestic Walking Track network outside the present hotel grounds.

1. Portion 57 (owned by Blue Mountains City Council as community land)

- Northern section of Wonderland track
- Northern loop of Murphy's track
- Valley service track
- Coal mine track

2. Portion 91

- Western end of Murphys Track
- Lower Flying Fox station access road

3. Portions 132 and 35

- Parts of the Lovers track

4. Portion 36

- Northern end of Lovers' track
- Part of swimming pool track
- Parts of Flying Fox access cart track

5. Portion 46

- Part of Sunbath track
- Belgravia Street connection

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 5

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

- Sunbath to Marks Tomb track
- Southern end of Wonderland track
- Connecting to Gatehouse, Belgravia Street and Sheleagh Cottage

6. Lot 1 DP526758

- An easement through this lot appears to be intended to provide access to the northern end of the Wonderland Track.

7. Portion 133

- Part of the Lovers' track.

It should be noted that the Hydro Majestic Hotel is currently listed as an item of local heritage significance on Blue Mountains City Council's LEP. It is not currently (2016) listed on the State Heritage Register. No individual walking track or walking track complex in the Blue Mountains Region is listed individually on the State Heritage Register. The current listing on the State Heritage Register is for "Blue Mountains Walking Tracks" with the location given as "Blue Mountains National Park". At the suggestion of Jim Smith an indicative list of the region's most culturally significant walking tracks and track complexes, both within and outside the Blue Mountains National Park was included within the description of this item. This list does not include the Hydro Majestic tracks. This indicative list arose out of the Blue Mountains Walking Track Heritage Study Stage 3. The report is entitled "Tracks into History" (2006) and is a "Conservation Management Plan" for Walking Tracks of State Heritage Significance in the Blue Mountains. This report assessed 37 tracks and track complexes as potentially of State Heritage Significance. However none of the management authorities for these tracks have ever submitted the completed SHI forms for these items for endorsement by the State Heritage office and none are individually listed on the State Heritage Register. As there is no precedent for any walking track or track complex in the region having been endorsed as of State Heritage Significance this assessment of the significance of the Hydro Majestic walking tracks is indicative only.

Criteria a) Note: This Statement of Significance applies to the whole of the former Hydro Majestic walking track system, not that portion remaining within the present hotel grounds.

The Hydro Majestic Hotel walking track complex has State Significance as the most extensive privately constructed walking track complex in Australia and for its association with Australia's only hydropathical resort developed on the European model.

Criteria b) The Hydro Majestic walking track complex has State Significance for its association with entrepreneur Mark Foy junior who made a fortune in retailing and invested most of it in developing the Hydro Majestic hydropathic resort. The track complex has local significance for its association with William Hargraves, Chief Clerk in Equity of NSW and Blackheath pioneer.

Criteria c) The Hydro Majestic walking track complex has aesthetic significance at the State level for the design values in its construction which demonstrate superb integration of natural and constructed features.

Criteria d)

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 6

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number
1173065

Study Number
HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountains]**

Criteria e) As nearly all of the track complex is intact, it offers an opportunity to research late 19th and early 20th century walking track design and construction techniques, significant at the local level.

Criteria f) The Hydro Majestic walking track network is a rare example of a very extensive privately constructed walking track complex, mostly over 100 years old, which has survived virtually fully intact to the present time.

Criteria g)

Integrity / Intactness: Nearly all of the track fabric is intact.

| References: | Author | Title | Year |
|-------------|--|--|------|
| | Anon., | Untitled Map of Hydro Majestic Tracks | 1990 |
| | Anon., | Medlow Bath Hydropathic Establishment, Mountaineer (21 June 1904) | 1904 |
| | Anon., | Tullumbar, A Blue Mountains Beauty Spot, Town & Country Journal (15 December 1900) | 1900 |
| | Anon., | A Mountain Pioneer, Mountaineer (23 November 1894) | 1894 |
| | Anon., | The Paths of Medlow Bath NP N.D. [1930s] | |
| | Anon., [J. Webb] | Untitled Map of Hydro Majestic Tracks, N.D. [early 1980s] | |
| | E. Kaldy | Medlow 1883 & Now | 1983 |
| | J. Macken | Blue Mountains at Medlow Bath Including Paths of Medlow Bath, Hydro Majestic | |
| | Jim Smith | Hydro Majestic Walking Tracks Conservation Plan | 1999 |
| | Jim Smith | Medlow Bath / Hydro Majestic Map 12 in How to See the Blue Mountains (2nd edition) | 1986 |
| | Jim Smith, Chris Betteridge & David Beaver | Tracks into History | 2006 |
| | N. Melville | Map & detailed descriptions of Walking Tracks on the Blue Mountains between Wentworth Falls & Bell | 1939 |
| | V. Henry | Locality Plan on lithograph Mount Mark & Wonderland Park Estates | 1925 |
| | W. Cranney | Plan, Showing the Paths of Medlow Bath, in Anon., The Paths of Medlow Bath, N.P. N.D. [1930s] | |

| Studies: | Author | Title | Number | Year |
|----------|-----------------------------|------------------------------------|--------|------|
| | Jim Smith | Walking Track Heritage Study, NPWS | HF1 | 1998 |
| | Blue Mountains City Council | LEP 2015 Heritage Review | | 2016 |
| | Jim Smith | LEP 1991 Consultant Review | | 2010 |

| Parcels: | Parcel Code | LotNumber | Section | Plan Code | Plan Number |
|----------|-------------|-----------|---------|-----------|-------------|
| | LOT | 1 | | DP | 133407 |
| | LOT | 2 | | DP | 133410 |

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 7

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

| | | | |
|-----|-----|----|--------|
| LOT | 15 | DP | 2450 |
| LOT | 8 | DP | 2450 |
| LOT | 9 | DP | 2450 |
| LOT | 10 | DP | 2450 |
| LOT | 11 | DP | 2450 |
| LOT | 12 | DP | 2450 |
| LOT | 14 | DP | 2450 |
| LOT | 16 | DP | 2450 |
| LOT | 17 | DP | 2450 |
| LOT | 18 | DP | 2450 |
| LOT | 19 | DP | 2450 |
| LOT | 20 | DP | 2450 |
| LOT | 21 | DP | 2450 |
| LOT | 22 | DP | 2450 |
| LOT | 13 | DP | 2450 |
| LOT | 20 | DP | 25570 |
| LOT | 2 | DP | 334630 |
| LOT | 134 | DP | 751657 |

Latitude:

Longitude:

Location validity:

Spatial Accuracy:

Map Name:

Map Scale:

AMG Zone:

Easting:

Northing:

Listings:

Name:

Title:

Number:

Date:

Potential Heritage Item

LEP 2015 Am5

MB026

Local Instrument: LEP 2005 (Note: Deferred from LEP 2015 as in Amendment 1- R1 zone; inclusion subject to resolution of Amendment 1)

Local Identifier: MB026

Draft Instrument:

Technical Comment:

Update Notes: (i) New listing with added data from NPWS SHI 3900201 (Hydro Majestic - Hargraves Complex) and SHI 3900202 (Hydro Majestic - Mark Foy Additions). Supercedes Council SHI forms 1170984 (Track Meadow Bath Climbers Passes MB025); 1170980 (Glen Rosa Track MB021); 1170981 (Track Hotel to Sunbath to Hotel MB022); 1170979 (Track to Three

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 8

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountai**

Brothers MB020); 1170983 (Track Valley Farm and Colosseum MB024); 1170982 (Track Wonderland Track and Marks Tomb MB023). Jim Smith 2010.

(ii) Heritage Review 2016 - address updated, additions to reflect some items not included in new listing / deleted from Council's inventory as on adjacent land, (1170982 (Track Wonderland Track and Marks Tomb MB023) not deleted), Aboriginal statement added (SR/Oct2016)

Aboriginal Heritage: The Blue Mountains Local Government Area is within the Country of the Darug and Gundungurra peoples and Blue Mountains City Council respects their cultural heritage and deep ongoing connection to this Country.
This inventory sheet documents the European occupation of the site only, and the heritage significance of the place in this context (assessed against the heritage criteria set by the NSW Heritage Council).
This does not therefore, represent a complete history of the place, or represent the perspectives of Darug and Gundungurra Traditional Owners in relation to the colonial impacts on this site. Consultation with Traditional Owners and other Aboriginal stakeholders is required, before Aboriginal Cultural Heritage that may be associated with this place, can be recorded.

Data Entry:

Date First Entered: 25/07/2010

Date Updated: 04/11/2016

Status: Basic

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 9

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.

Blue Mountains Heritage Register Review

Heritage Inventory - 2016 Heritage Review

SHI Number

1173065

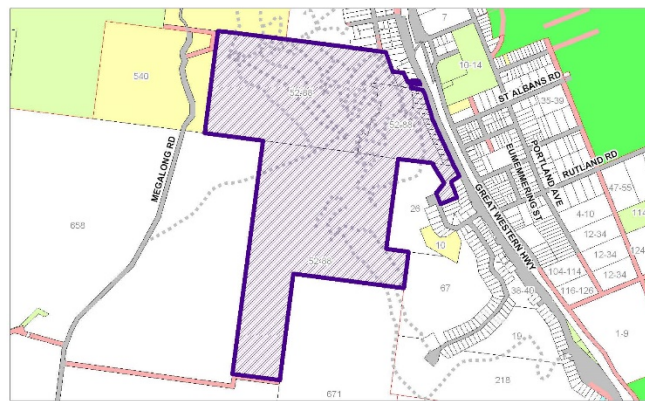
Study Number

HF1

Item Name: **Hydro Majestic - original walking track complex**

Location: **52-88 Great Western Highway, Medlow Bath [Blue Mountain]**

Image:



Caption: Location Map

Copyright: City Planning

Image by: City Planning

Image Date: 16/03/2011

Image Number:

Image Path:

Image File: 1173065b00.jpg

Thumb Nail Path:

Thumb Nail File:

Heritage Inventory - 2016 Heritage Review

Date: 04/11/2016

Full Report with Images

Page 10

This report was produced using the Heritage Database Software provided by the Heritage Branch, NSW Department of Planning.